

DM20 – A5 Active & Sustainable Transport Assessment: Rural

Description

The identification and assessment of potential active & sustainable transport opportunities in rural areas along the A5, Northern Ireland. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 – New Buildings to Aughnacloy - Terms of Reference*” document (Mouchel Ref. 1058654/RP/001).

This memo should be read in conjunction with drawings: 718736-ASASTA-0800-D-053 to 065 ‘Assessments of Rural Areas’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic on the A5.

A. New Buildings to Strabane (See drawing D-053 to D-055)

- i. A potential 3.0m wide greenway route has been identified during the stakeholder consultation exercise which could run adjacent to the existing A5's southbound carriageway through New Buildings. The construction of the 3.0m wide greenway is a result of utilising existing footways / greenways, widening existing footways, repurposing existing carriageway, land acquisition and partially constructing a brand new greenway.

Links to this potential greenway have not been assessed as it utilises the existing A5.

- ii. **Opportunity No.1:** A potential 3.0m wide greenway route has been identified which utilises the disused railway line. Access to this potential greenway could be provided via the National Cycle Network Route No.93.



Figure 1 - Opportunity No.1 – Access to Greenway via NCN 93. Red box represents potential access at Greenway and NCN crossing point. Green box represents potential access at the 'New Buildings Community and Environmental Association'.

B. Ballymagorry (See Drawing D-055)

- i. **Opportunity No.2:** A potential 3.0m wide greenway route has been identified which utilises the disused railway line. The route passes close to the southern end of Ballymagorry. Access to this potential greenway could be provided via the existing footway along Woodend Road, where the route passes approximately 150m from the existing A5.

C. Strabane (See Drawing D-055 & D-056)

- i. **Opportunity No.3:** A potential 3.0m wide greenway route has been identified which utilises grass areas on the southern side the Mourne River and also the eastern side of the River Finn. Access to this potential greenway could be provided via the existing footways at the southern end of the existing A5 Bradley Way bridge over the Mourne River, where the greenway intersects the existing A5. Footways at this location are approximately 3.5m wide.



Figure 2 - Opportunity No.3 – Access to Greenway at A5 Bradley Way Bridge (Red Square)

- ii. **Opportunity No.4:** The greenway adjacent the Mourne River continues until it meets the disused railway line close to the south of Strabane. At this point the potential greenway route is approximately 100m away from the existing A5.

The greenway at this location can be extended by 100m to meet the existing A5 utilising the disused railway line.



Figure 3 - Opportunity No.4 – Access to Greenway utilising disused railway line (Red Square)

D. Sion Mills (See Drawing D-056)

- i. **Opportunity No.5:** A potential greenway route has been identified to the east of Sion Mills. It utilises the disused railway line and runs parallel to the existing A5 which runs through the town. As this greenway route is relatively close to the existing A5 (circa. 300m offset), the requirement to widen the existing footways in Sion Mills to provide a Shared Use NMU Facility is negated.

Beside the Baptist Church in Sion Mills, directly opposite the Daisy Park junction, there is an NMU-only link to the disused railway line. As this railway line has been identified as a potential greenway route, it is proposed to upgrade this NMU-only route by ensuring 3.0m width is achieved throughout its length. Appropriate shared-used facility signage should be installed. Length of facility is approx. 270m.



Photograph 1 - Opportunity No.5 - Upgrade of 'Willows' NMU-only route to Greenway

Opportunity No.6: Access to this section of the greenway can be granted via Willows. It is proposed to improve access to this section of the line. The northern footway along Willows could be widened to incorporate a shared use NMU facility. Additional land (circa. 2m x 300m) is likely to be required and 4no. street lights will need to be relocated in order for shared use facility. Length of facility is approx. 300m.

E. Victoria Bridge (See Drawing D-057)

- i. **Opportunity No.7:** A potential 3.0m wide greenway route has been identified which utilises the disused railway line. The route passes close to the northern end of Victoria Bridge. Access to this potential greenway could be provided via the existing 2.5m wide footway along Fyfin Road, where the route passes approximately 330m from the existing A5.



Figure 4 - Opportunity No.7 – Access to Greenway via Fyfin Road (Red Square)

F. Newtownstewart (See Drawing D-058)

- i. **Opportunity No.8(i) & 8(ii):** A potential 3.0m wide greenway route has been highlighted in the area of Newtownstewart running relatively parallel to the existing A5, utilising the disused railway line. Note; the break in the dashed green line on the drawings represents a length where the disused railway line is not viable as a greenway route as the existing A5 utilises this alignment.

Access to this potential greenway could be provided via the National Cycle Network Route No.92 and a potential shared use NMU facility highlighted in 'DM12 - A5 - Active & Sustainable Transport Assessment: Newtownstewart'.

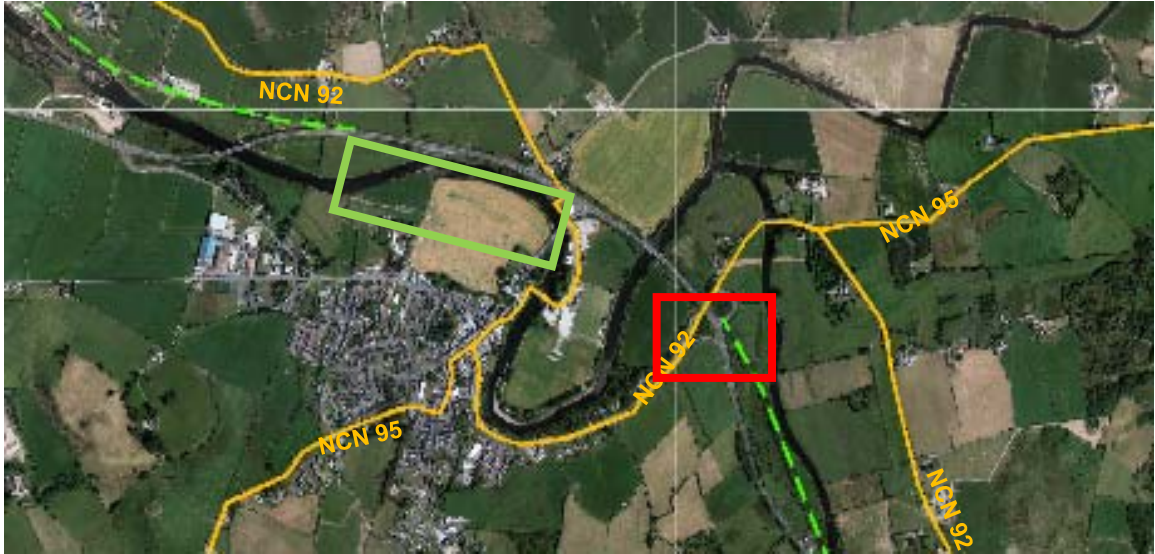


Figure 5 - Opportunity No.8(i) – Access via NCN 92 and potential Shared used facility (Green Square) &

Opportunity No.8(ii) - Access to Greenway via NCN 92 (Red Square)

G. Ulster Way (See Drawing D-059)

- i. **Opportunity No.9:** A potential 3.0m wide greenway route has been highlighted in the area of where the Ulster Way (an existing recognised walking route) crosses the existing A5, north of the Ulster American folk park. The greenway route at this point is approximately 250m from the existing A5 and it utilises the disused railway line. Access to this potential greenway could be provided via the Ulster Way.

H. Mountjoy (See Drawing D-059 & D-060)

- i. **Opportunity No.10:** A potential 3.0m wide greenway route has been identified which utilises the disused railway line. The route passes to the east of Mountjoy.

Access to this potential greenway could be provided via a potential 2.0m wide footway close to the Folk Park, as outlined in 'DM13 - A5 - Active & Sustainable Transport Assessment: Mountjoy'.

Opportunity No.11: Access to this potential greenway could be provided via a potential 2.0m wide footway adjacent Mountjoy Avenue, as outlined in 'DM13 - A5 - Active & Sustainable Transport Assessment: Mountjoy'

Opportunity No.12: Access to this potential greenway could be provided via a potential 3.0m wide shared use NMU facility along Castletown Road, as outlined in 'DM13 - A5 - Active & Sustainable Transport Assessment: Mountjoy'

I. Omagh (See Drawing D-061)

Opportunity No.13: A potential greenway route which utilises the disused railway line has been identified along Crevenagh Road, south of Omagh. NMU

access to this greenway can be granted via Crevenagh Road Roundabout, as outlined in “DM14 - A5 - Active & Sustainable Transport Assessment: Omagh”. This greenway route continues in the direction of Dungannon.

J. Ballygawley (See Drawing D-064)

- i. **Opportunity No.14:** A potential greenway route which utilises the disused railway line has been identified which runs parallel to Tullybryan Road, to the south of Ballygawley. NMU access to this greenway can be provided via Tullybryan Road.

K. Aughnacloy (See Drawing D-065)

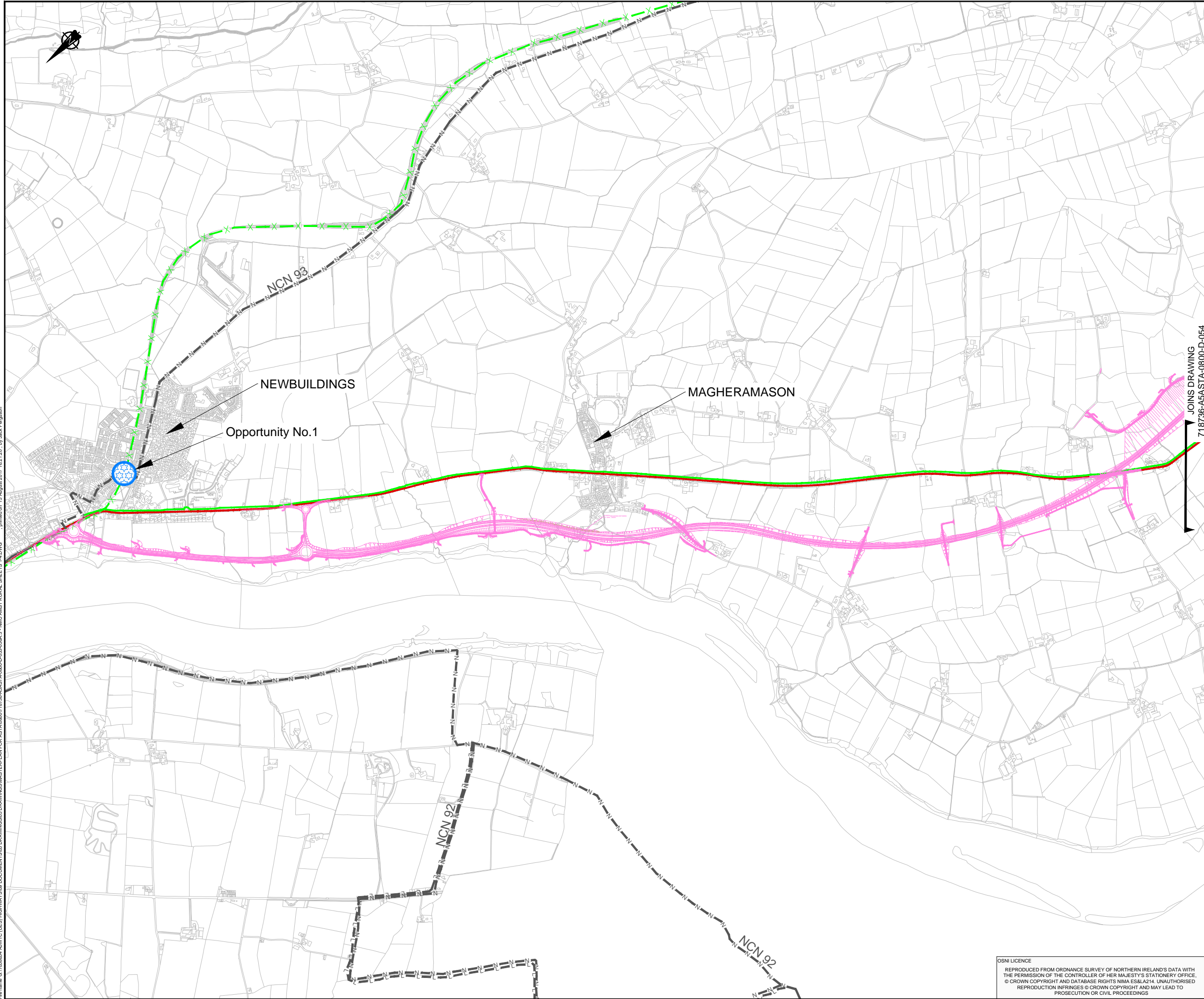
- i. **Opportunity No.15:** A potential 3.0m wide greenway route has been identified which utilises the disused railway line. The route starts to the north of Aughnacloy and heading east. Access to this potential greenway could be provided via a potential 3.0m wide shared use NMU facility along the southbound carriageway of A5 Tullyvar Road. The area where the greenway begins will need to be cleared of vegetation.

Opportunity No.16: Access to this potential greenway could also be provided via Tramline Way. At this location, the greenway route would be approximately 100m away from the existing A5.



Photograph 2 - Opportunity No. 16 – Access to Greenway via Tramline Way

Opportunity No.17: Access to this potential greenway could also be provided via a potential 3.0m wide shared use NMU facility along the southbound of B35 Carnteel Road, which is also a part of the Ulster Way.



Notes

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- This Drawing should be read in conjunction with "DM17 - A5 - Active & Sustainable Transport Assessments: Rural" which will explain each identified opportunity.

Key

Existing Asset Features

- LOCAL CYCLE ROUTE - EXISTING
- NATIONAL CYCLE ROUTE - EXISTING
- REGIONAL CYCLE ROUTE - EXISTING
- ULSTER WAY - EXISTING
- EXISTING A5

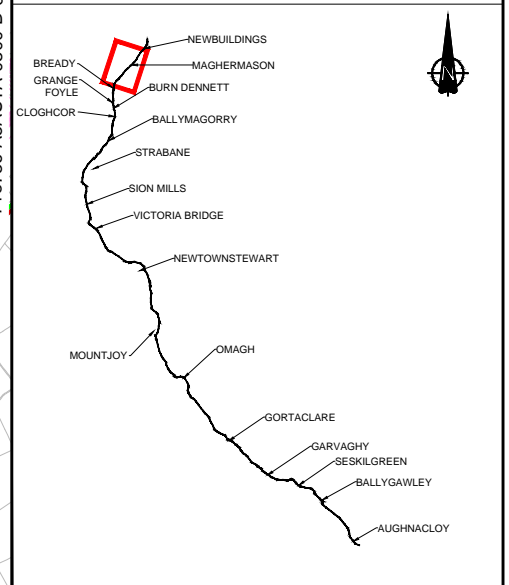
Proposed Improvements

- GREENWAY - POTENTIAL
- GREENWAY - POTENTIAL (DISUSED RAILWAY)
- GREENWAY - POTENTIAL (EXISTING TOWPATH)

Potential Greenways are aspirational lines only. They do not reflect the exact features on the ground.

Other Features

- PROPOSED A5WTC SCHEME
- OPPORTUNITY DESCRIBED IN MEMO



KEY PLAN

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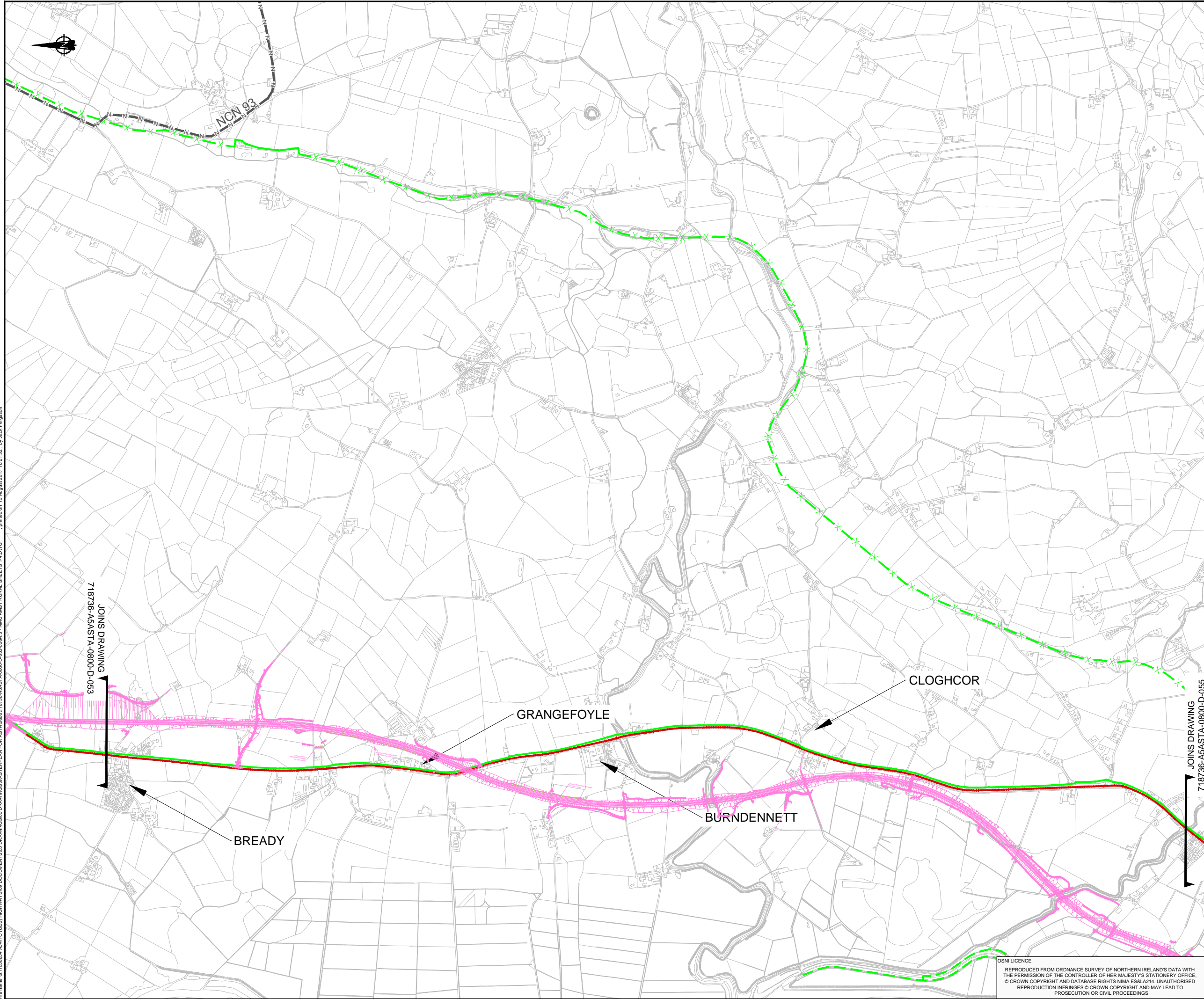
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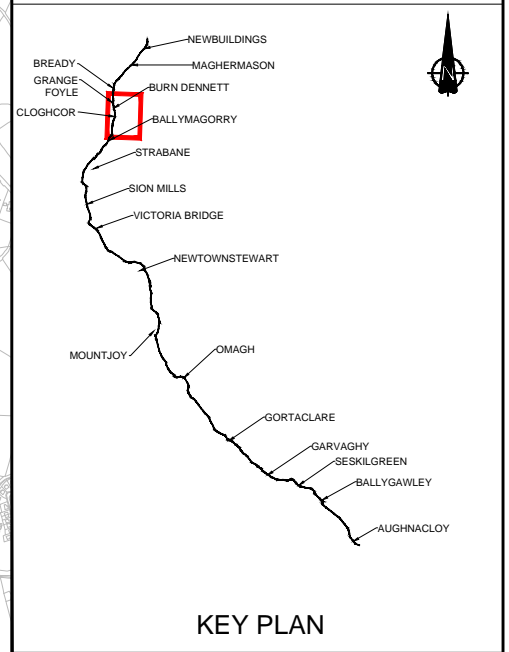
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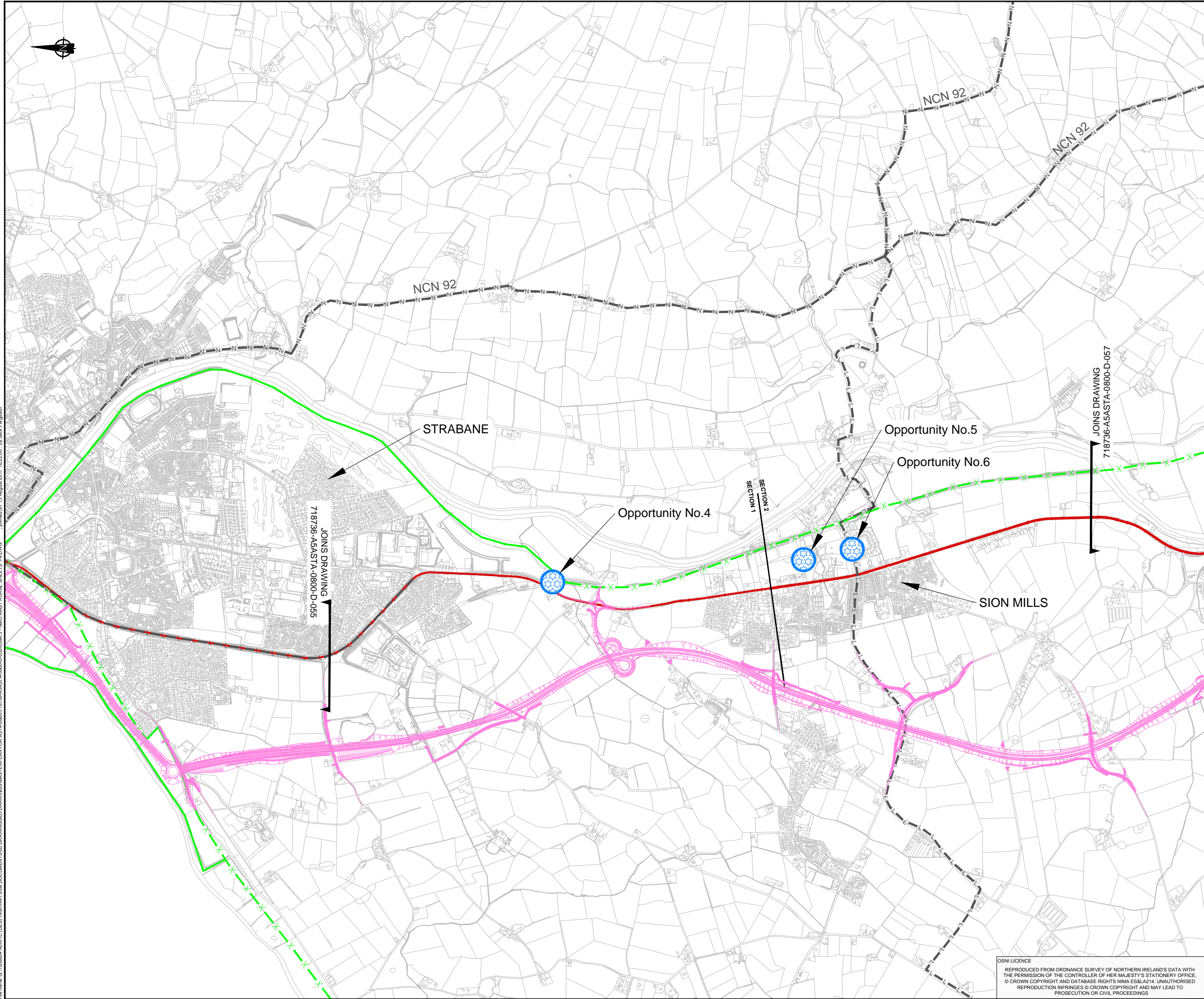
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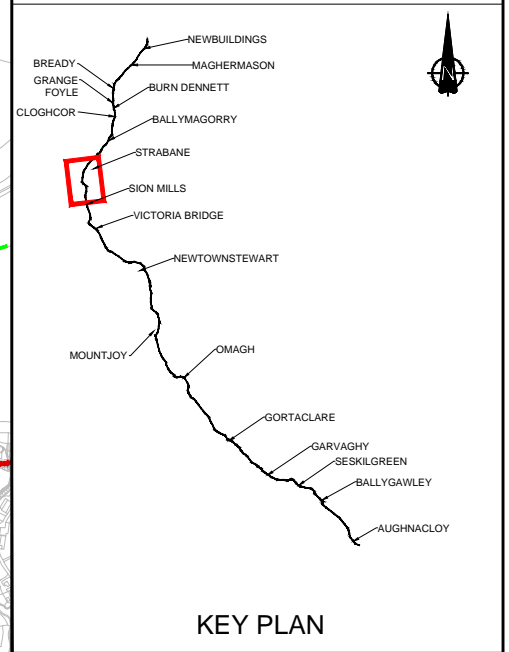
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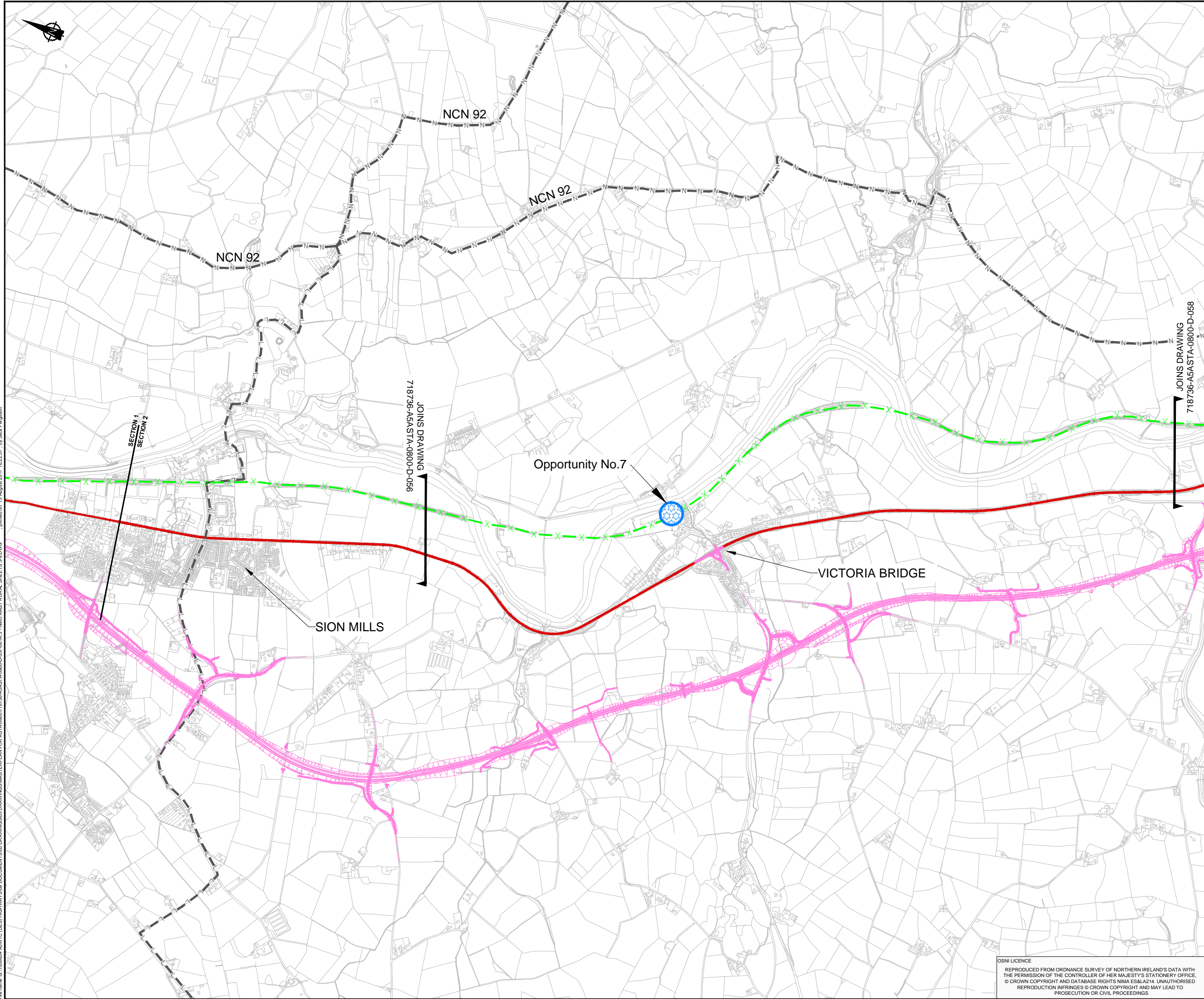
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SHEET 4 OF 13

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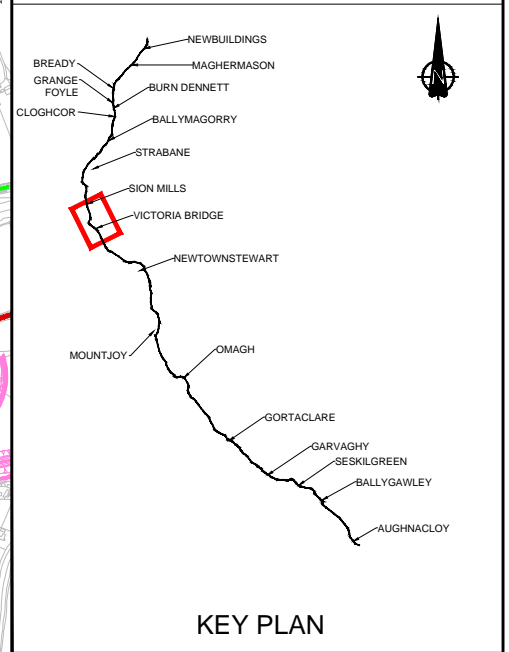
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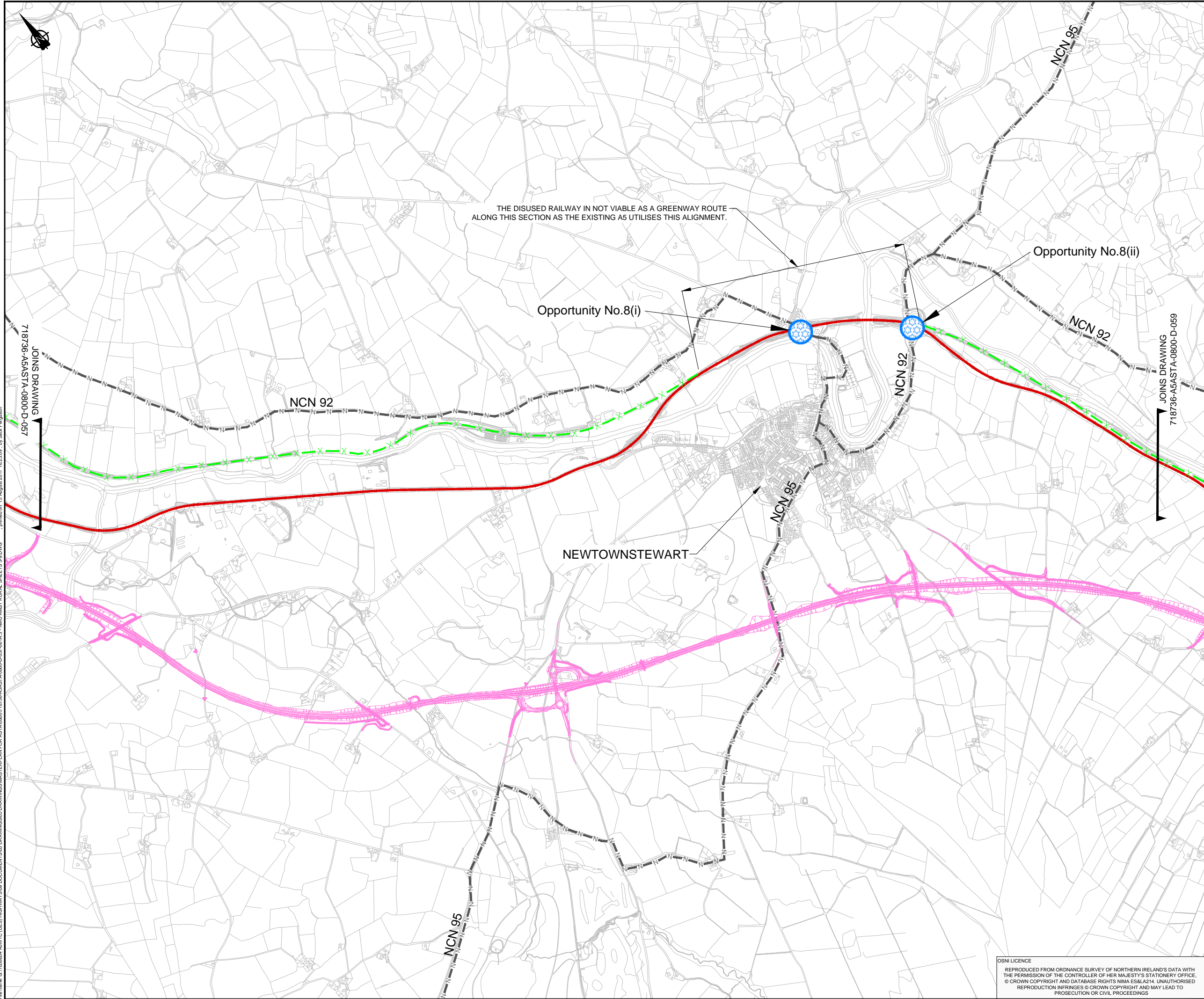
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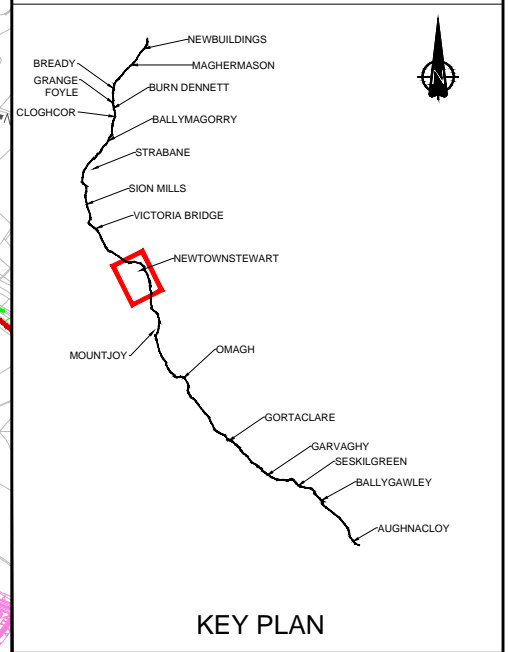
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A5WTC
Active & Sustainable Transport

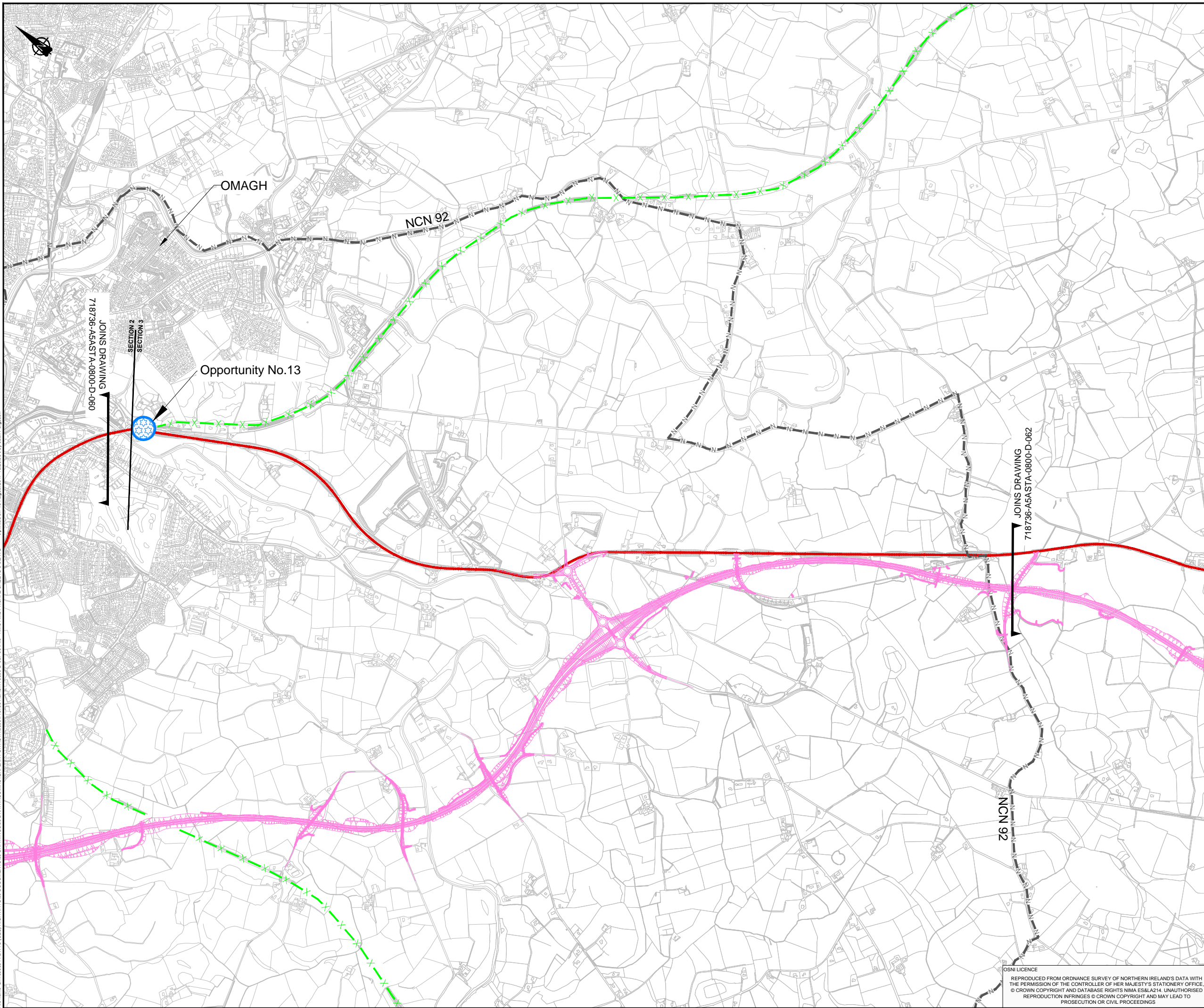
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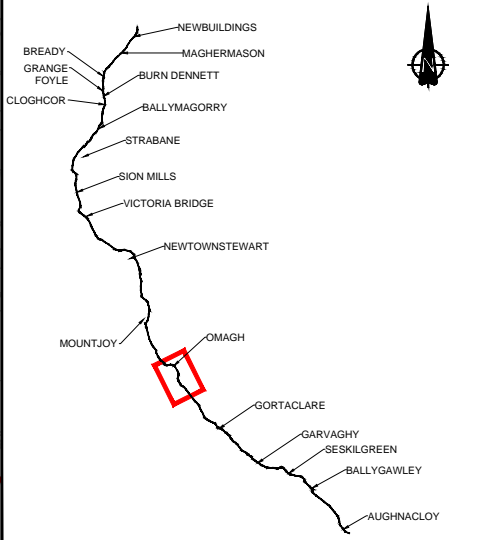


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SHEET 9 OF 13

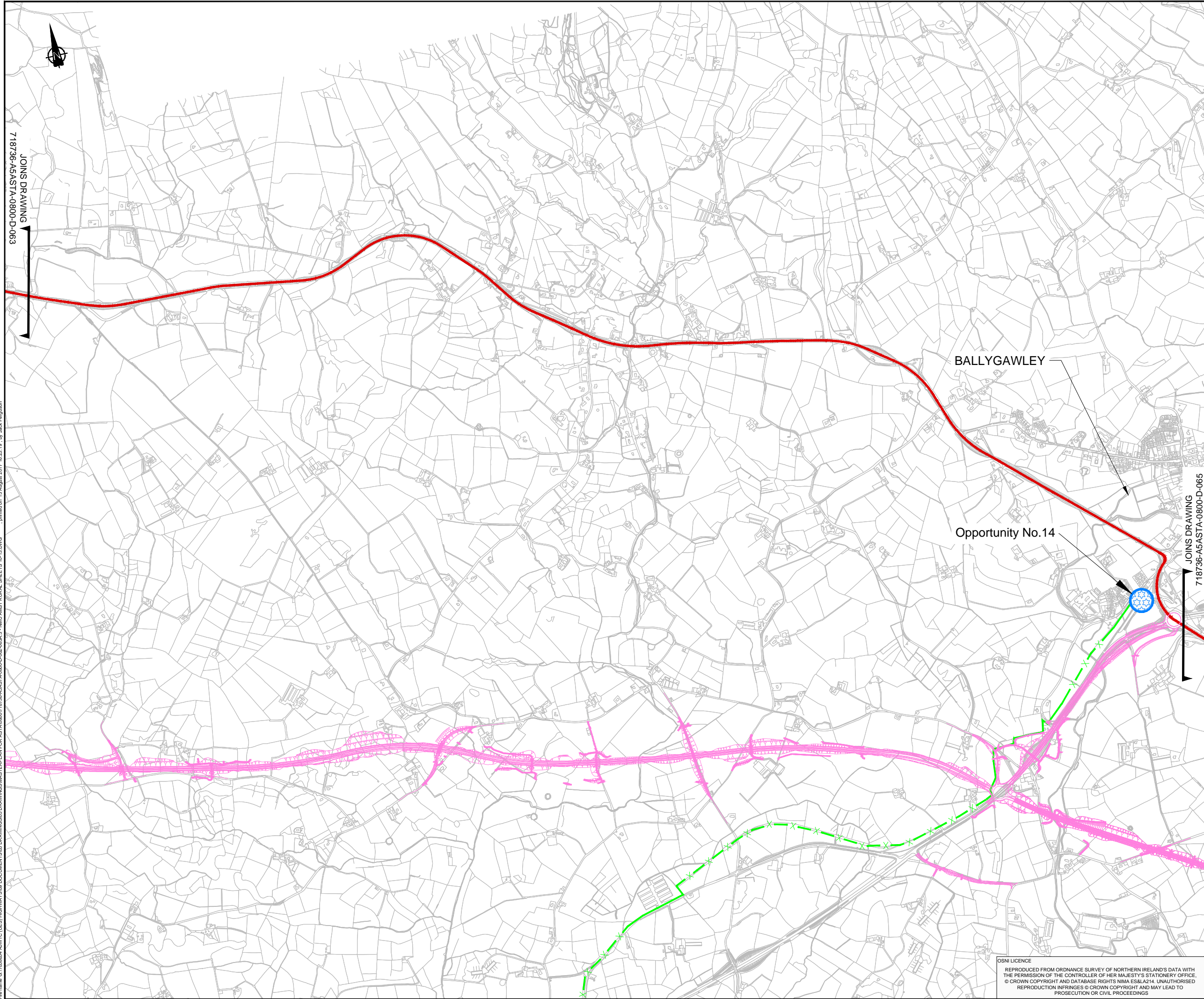
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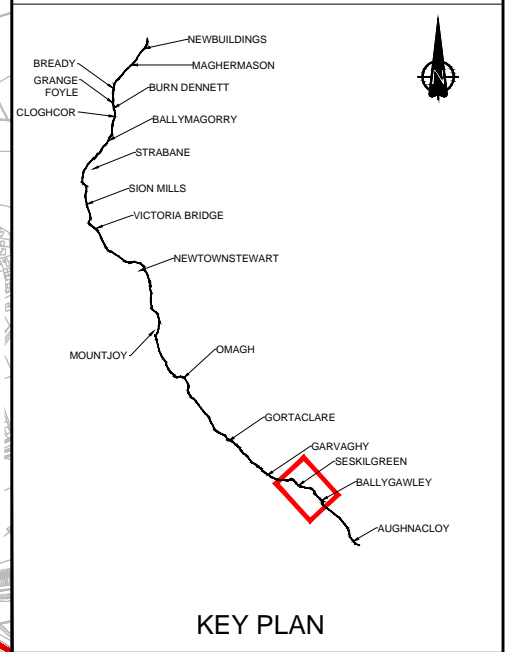
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 - GREENWAY - POTENTIAL (DISUSED RAILWAY)
 - GREENWAY - POTENTIAL (EXISTING TOWPATH)
- Potential Greenways are aspirational lines only. They do not reflect the exact features on the ground.
- Other Features**
- PROPOSED A5WTC SCHEME
 - OPPORTUNITY DESCRIBED IN MEMO



DRAWING STATUS: **FOR COMMENT**

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STEP/PROJECT: **Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy**

TITLE: **ASSESSMENTS OF RURAL AREAS PROPOSED NMU FACILITIES SHEET 12 OF 13**

SCALE @ A1: 1:10,000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DATE: 14/08/2017

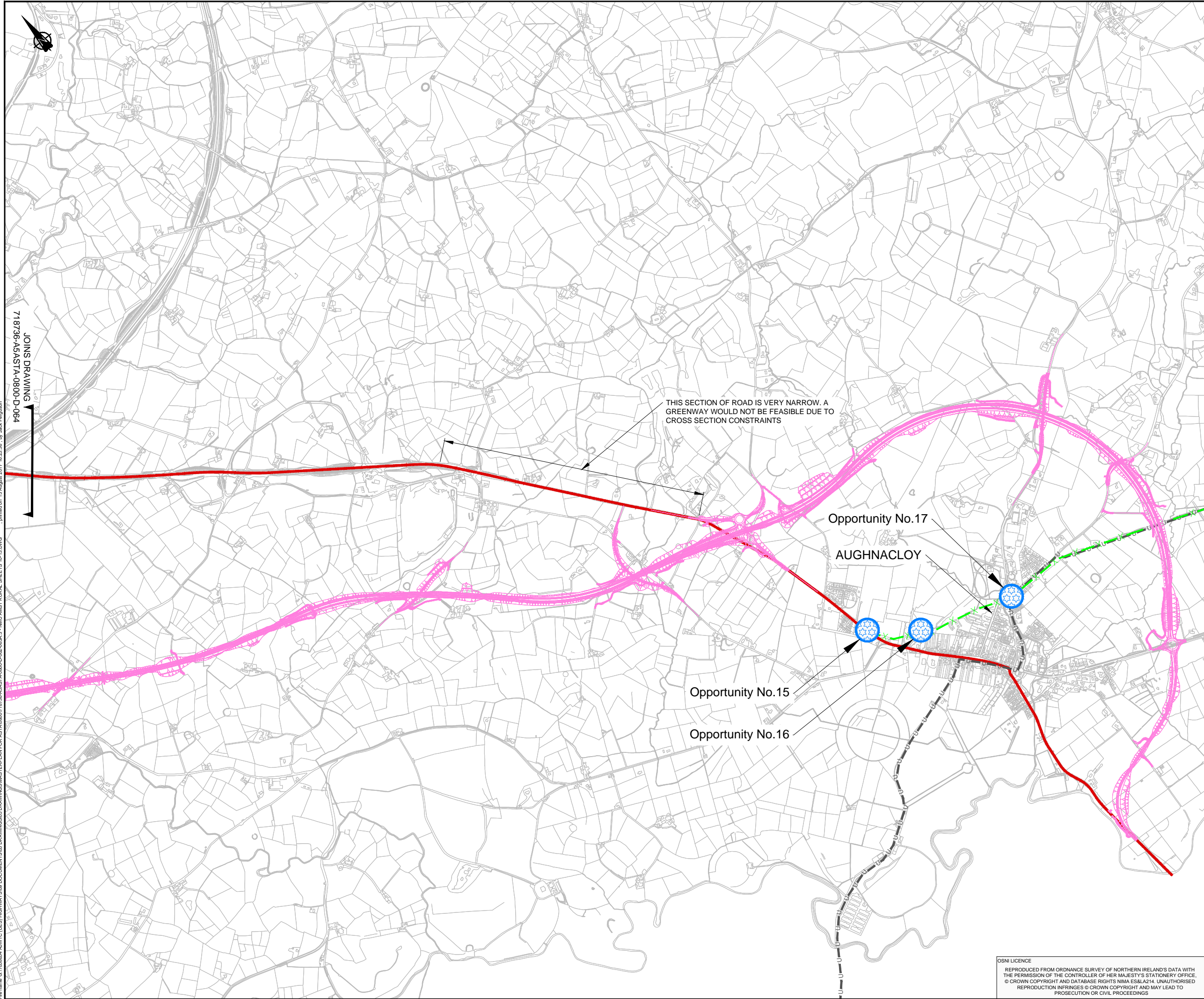
DRAWING NO: **718736-A5ASTA-0800-D-064** REV: **A.3**

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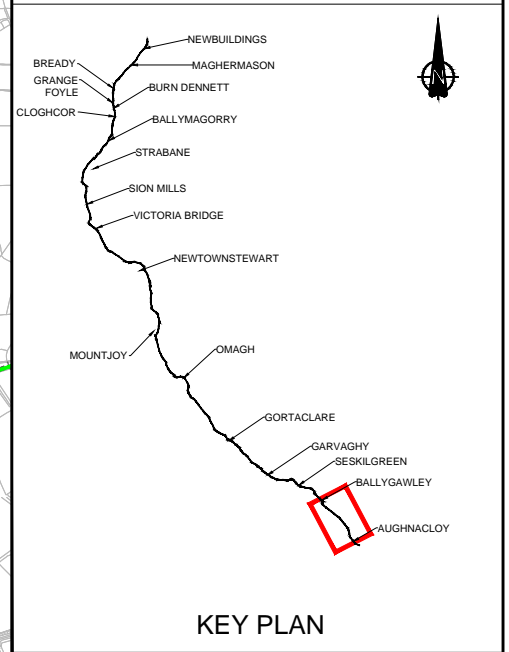
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Notes

- The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
- This Drawing should be read in conjunction with "DM17 - A5 - Active & Sustainable Transport Assessments: Rural" which will explain each identified opportunity.

- Key**
- Existing Asset Features**
- LOCAL CYCLE ROUTE - EXISTING
 - NATIONAL CYCLE ROUTE - EXISTING
 - REGIONAL CYCLE ROUTE - EXISTING
 - ULSTER WAY - EXISTING
 - EXISTING A5
- Proposed Improvements**
- GREENWAY - POTENTIAL
 - GREENWAY - POTENTIAL (DISUSED RAILWAY)
 - GREENWAY - POTENTIAL (EXISTING TOWPATH)
- Potential Greenways are aspirational lines only. They do not reflect the exact features on the ground.
- Other Features**
- PROPOSED A5WTC SCHEME
 - OPPORTUNITY DESCRIBED IN MEMO



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STEP/PROJECT: **Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy**

TITLE: **ASSESSMENTS OF RURAL AREAS PROPOSED NMU FACILITIES SHEET 13 OF 13**

SCALE @ A1: 1:10,000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DATE: 14/08/2017

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