

DM04 – A5 Active & Sustainable Transport Assessment: New Buildings

Description

The identification and assessment of potential active & sustainable transport opportunities in New Buildings, County Londonderry. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo should be read in conjunction with drawings: 718736-A5ASTA-0800-D-001 to 002 ‘Assessments of Urban Areas, Location: New Buildings, Co. Londonderry’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic.

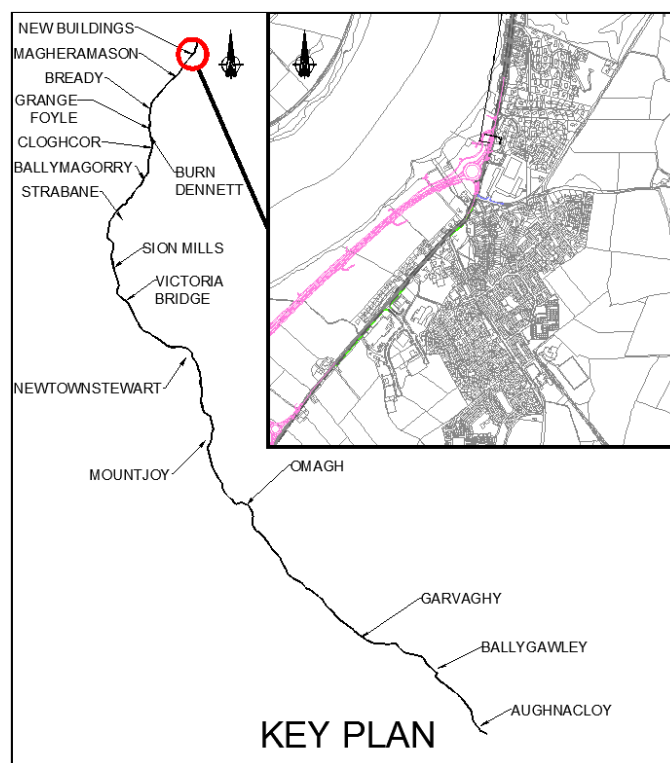


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 through New Buildings are of a relatively good standard i.e. an existing footway is present on both sides of the existing A5 through New Buildings (except for a short length of approximately 120m adjacent to the northbound direction of travel at the south end of New Buildings); moreover, considerable lengths of the footways on both sides are segregated from the existing A5 by a grass verge.

Existing Cycling Facilities/ NI Greenway Networks (see drawings D-001 and D-002)

There is an existing Greenway of approximate length 1km located at the northern end of New Buildings, the majority of which (i.e. circa 0.75 km) is located adjacent to the northbound direction of travel.



Photograph 1: Southbound Entrance to New Buildings

Existing Parking Facilities (see drawings D-001 and D-002)

There are limited parking opportunities along the existing A5 Victoria Road through New Buildings. However, a lay-by is present to the rear of the existing footway adjacent to the northbound direction of travel between the Dunhugh Park and Stoneypath estates which includes twelve parking spaces. With the exception of New Buildings Primary School,

there are few attractors along the A5 Victoria Road which would likely require online parking.

There is limited opportunity to provide formalised parking on the existing A5 Victoria Road outside New Buildings Primary School. This is due to the presence of domestic properties on both sides of the road which would prohibit widening the carriageway on either side to accommodate on-line parking.



Photograph 2: Southbound on A5 Victoria Road at the entrance to New Buildings Primary School (see drawings D-001 and D-002)

Existing NMU Crossing Facilities (see drawings D-001 and D-002)

There are two existing NMU crossing facilities that traverse the existing A5 Victoria Road through New Buildings. There is a pedestrian refuge island approximately 125m south of Stoneypath junction with the existing A5. The second is a pelican crossing at the Duncastle Road junction with the existing A5.

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this design memo discuss potential improvements to the two gateway entrances to New Buildings on the existing A5, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5, potential Greenway facilities that could be introduced, potential improvements to existing bus stop facilities on the existing A5, and the potential reduction in speed limit on the existing A5 outside New Buildings Primary School.

Opportunity 1 (see drawings D-001 and D-002): Village Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. See the first image on the following page for an example of these potential gateway improvements; the two images below illustrate the existing gateway features.



Photograph 3: Northbound entrance to New Buildings (see drawing D-002)



Photograph 4: Southbound entrance to New Buildings (see drawing D-001)



Photograph 5: Example potential gateway improvements

Opportunity 2 (see drawings D-001 and D-002): NMU Crossing facilities

Opportunity 2(a)

Provide an uncontrolled NMU crossing across the entrance to Dunhugh Park.

Opportunity 2(b)

Provide an uncontrolled NMU crossing across the entrance to the Stoneypath estate.



Photograph 6: Northbound on A5 Victoria Road at the entrance to Stoneypath Estate (see drawing D-001)

Opportunity 2(c)

Provide an uncontrolled NMU crossing (with pedestrian refuge island) across the mouth of the Woodside Road junction with the A5 Victoria Road.



Photograph 7: Southbound on A5 Victoria Road at Woodside Road junction (see drawing D-001)

Opportunity 2(d)

Provide an uncontrolled NMU crossing at the bus stop immediately to the south of the Woodside Road junction with the A5 Victoria Road.



Photograph 8: Southbound on A5 Victoria Road north of the Duncastle Rd. junction (see drawing D-001)

Opportunity 2(e)

Provide an uncontrolled NMU crossing across the mouth of the Rock Park junction with the A5 Victoria Road.



Photograph 9: Northbound on A5 Victoria Road at Rock Rd. junction (see drawing D-001)

Opportunity 2(f)

Provide an uncontrolled NMU crossing across the mouth of the Ballyore Grove junction with the A5 Victoria Road.



Photograph 10: Ballyore Grove junction with the A5 Victoria Road (see drawing D-002)

Opportunity 2(g)

Provide a controlled signalised (pelican) NMU crossing outside the New Buildings Primary School to cater for school children crossing the A5, Victoria Street to and from the bus stop opposite the school (it appears that a school crossing patrol is currently in operation).



Photograph 11: Southbound on A5 Victoria Road at the entrance to Ballyore Grove (see drawings D-001 and D-002)

Opportunity 2(h)

Provide an uncontrolled crossing across the mouth of the Carrowreagh Park junction with the A5 Victoria Road.



Photograph 12: Southbound on A5 Victoria Road at the entrance to Carrowreagh Park (see drawing D-002)

Opportunity 2(i)

Provide an uncontrolled NMU crossing at the mouth of the Edgewater junction with the A5 Victoria Road. Provide a connection from the A5 Victoria Road footway to the internal footway network of the Edgewater estate.

Opportunity 2(j):

Improve the NMU crossing at the entrance to Ballyore Retail Park with the addition of tactile paving.



Photograph 13: Southbound on A5 Victoria Road at the entrance to Ballyore Retail Park (see drawing D-002)

Opportunity 3 (see drawing D-001): Park & Ride Facilities

Translink was consulted during this assessment work and indicated that a Park & Ride (P&R) facility to the east of the existing A5 Victoria Road (at the location of the proposed roundabout in New Buildings that will form part of the A5WTC) is desirable. More detailed assessment work will be required to ascertain if this is the optimum location for this facility in New Buildings.

Opportunities 4(a) and 4 (b) (see drawing D-001): NI Greenway Network

A potential Greenway (the majority of which would be 3m wide) has been identified during the stakeholder consultation exercise. As mentioned on page 3 there is an existing Greenway of approximate length 1km located at the northern end of New Buildings, the majority of which (i.e. circa 0.8km) is located adjacent to the northbound direction of travel. The potential Greenway would extend the existing Greenway by an overall length of approximately 0.65km to the south by widening the existing footway provision adjacent to the southbound direction of travel over two separate lengths i.e. a 0.2km length

immediately to the south of the southern end of the existing Greenway (Opportunity 4(a) as shown in drawing D-001) and a 0.45km length at the southern extent of the New Buildings assessment (Opportunity 4(b) as shown in drawing D-002). Between these two lengths there is an existing section of footway of approximate length 0.25km that is heavily constrained (by New Buildings Primary School and domestic dwellings beyond the rear of the existing footway) requiring land acquisition from the school and domestic gardens.

Opportunity 5 (see drawings D-001 and D-002): Northern Ireland Road Safety Strategy to 2020

The Northern Ireland Road Safety Strategy to 2020 identified “...*considering the applicability of urban speed reduction initiatives and assessing the potential for wider introduction of 20mph limits in residential areas and other urban areas where there is a significant presence of vulnerable road users.*”

The entrance to New Buildings Primary School is via the A5 Victoria Road. The current posted speed limit along the A5 Victoria Road outside the school is 30mph. There are currently no “School Ahead” warning signs on the A5 Victoria Road on either approach to the school. Furthermore, the increased carriageway width outside the school due to the presence of right turn lanes may lead to increased vehicle speeds. Moreover, there are no formal parking facilities outside the school; therefore, road users dropping pupils off at the school in private cars may park on either side of the A5 Victoria Road, potentially reducing driver visibility for pedestrians crossing the A5 Victoria Road. For these reasons the A5 Victoria Road outside the New Buildings Primary School may be an appropriate location for a 20mph speed limit to be implemented.

It should be noted that proposals to reduce speed limits at any location along the existing A5, would be subject to the appropriate assessments carried out by the Departments’ Traffic Management Section and Network Development Section, along with the PSNI Traffic Section.

Opportunity 6 (see drawing D-002): Bus Stop Facilities

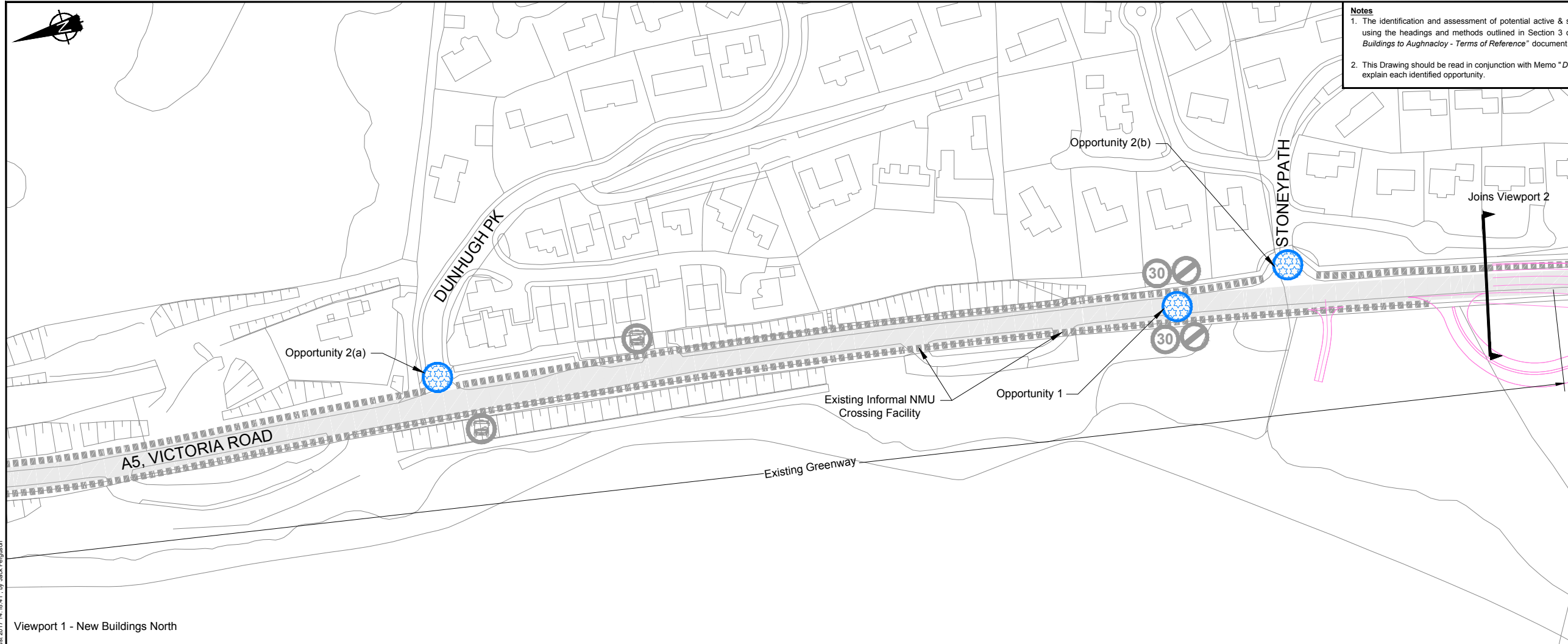
Bus stops have been provided outside the New Buildings Primary School. The southbound bus stop is online. To alert drivers' attention to the presence of the online bus stop red bus bay road markings could be provided. An offline bus stop lay-by complete with a bus shelter has been provided on the northbound side of the carriageway.



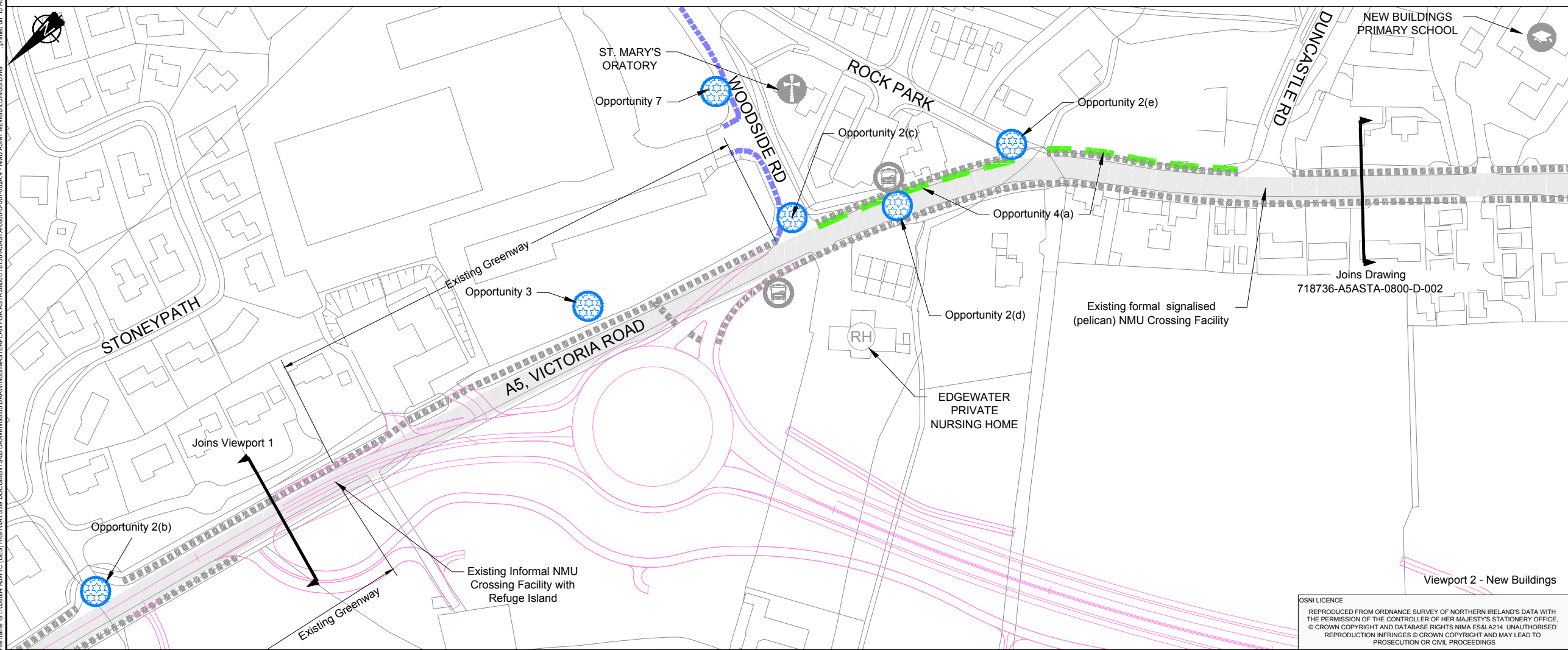
Photograph 14: Southbound on A5 Victoria Road north of New Buildings Primary School (see drawing D-002)

Opportunity 7 (see drawing D-001): Footway Improvements

A new 2m wide shared use facility is proposed along Woodside Road, it is proposed to tie into the existing footway along the A5, Victoria Road. This opportunity could be provided in the verge along the north side of Woodside Road.



Viewport 1 - New Buildings North



Viewport 2 - New Buildings

Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM04 - A5 - Active & Sustainable Transport Assessment: New Buildings" which will explain each identified opportunity.

Key

Existing Asset Features

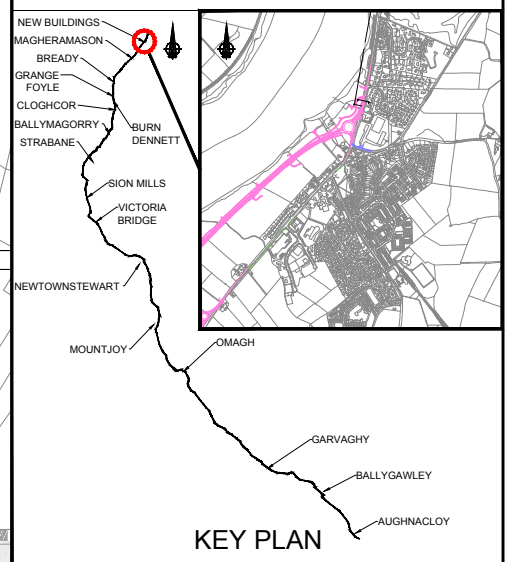
- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme



DRAWING STATUS: FOR ISSUE

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SITE PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy

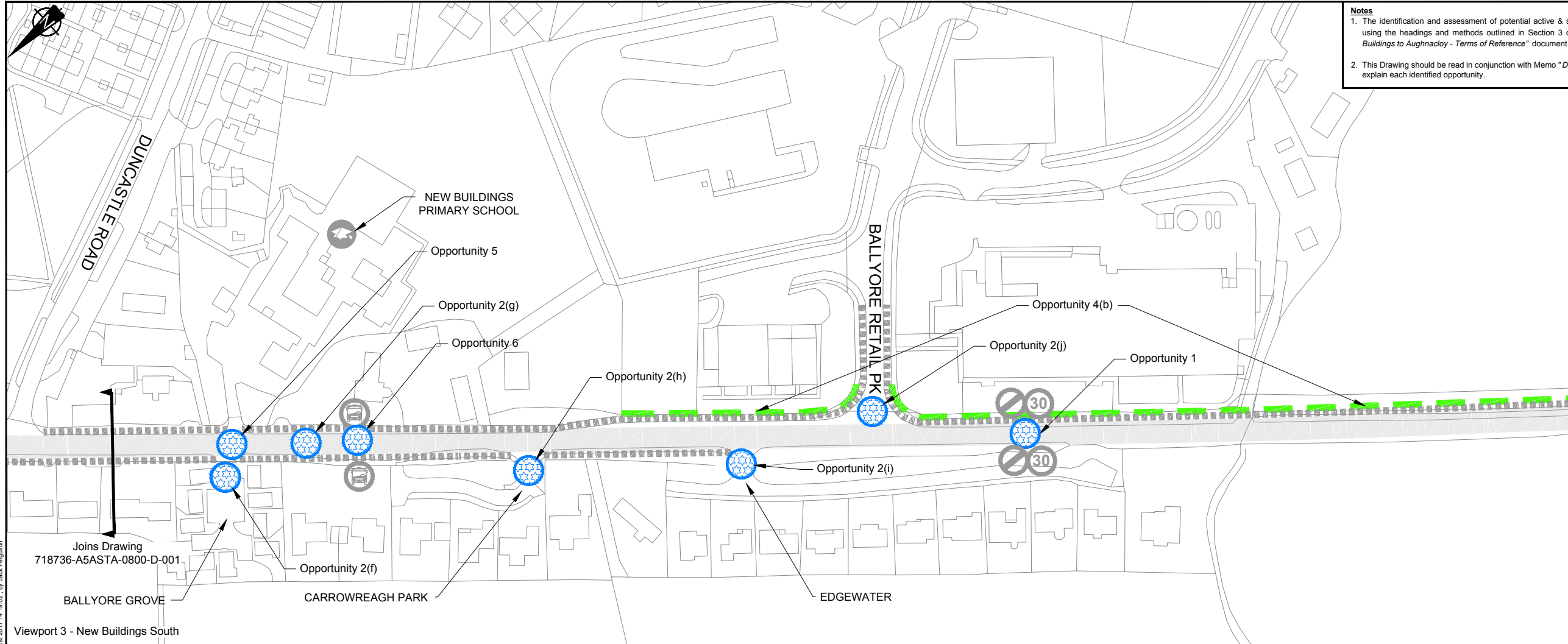
TITLE: ASSESSMENTS OF URBAN AREAS
 LOCATION: NEW BUILDINGS, CO. L'DERRY
 SHEET 1 OF 2

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: DM	DATE: 14/08/2017

DRAWING NO: 718736-A5ASTA-0800-D-001	REV: A
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Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM04 - A5 - Active & Sustainable Transport Assessment: New Buildings" which will explain each identified opportunity.

Key

Existing Asset Features

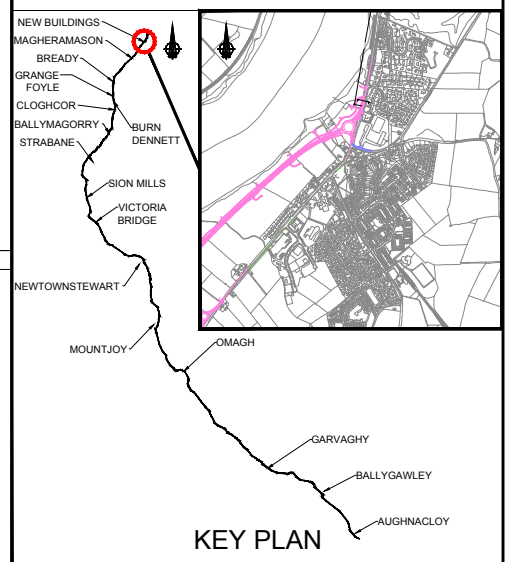
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- Signage: [Icons: 50, 40, 30]
- School: [Icon]
- Residential Care Homes (RH): [Icon]
- Bus Stop: [Icon]
- Sports / Cricket Club: [Icon]
- Church: [Icon]
- Health Care (HC): [Icon]

Proposed Improvements

- 2m Wide NMU Facility: [Icon]
- 3m Wide (Shared Use) NMU Facility: [Icon]
- Existing NMU Facility to be Widened: [Icon]
- Existing NMU Facility to be Widened (via Potential Greenway): [Icon]
- Potential Greenway Route: [Icon]
- Highway Realignment with new Shared Use NMU Facility: [Icon]
- Opportunity Described in Memo: [Icon]

Other Features

- Proposed A5WTC Scheme: [Icon]



Joins Drawing
 718736-A5ASTA-0800-D-001
 BALLYORE GROVE
 CARROWREAGH PARK
 Edgewater
 Viewport 3 - New Buildings South

DRAWING STATUS: FOR ISSUE

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SITE/PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy **A5WTC**

TITLE: ASSESSMENTS OF URBAN AREAS
 LOCATION: NEW BUILDINGS, CO. L'DERRY
 SHEET 2 OF 2

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: DM	DATE: 14/08/2017

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DM05 – A5 Active & Sustainable Transport Assessment: Magheramason

Description

The identification and assessment of potential active & sustainable transport opportunities in Magheramason, County Londonderry. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo should be read in conjunction with Drawing: 718736-A5ASTA-0800-D-003 ‘Assessments of Urban Areas, Location: Magheramason, Co. Londonderry’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic

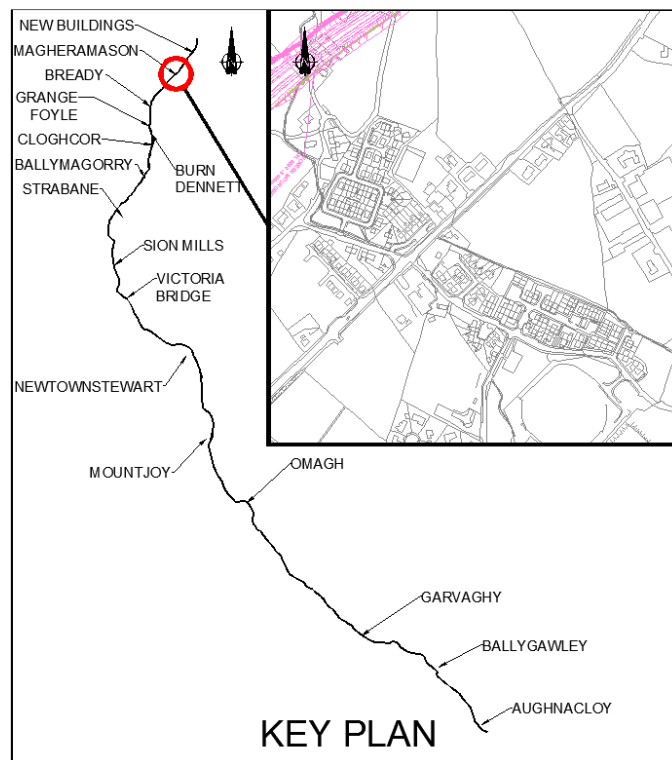


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 through Magheramason are of a relatively good standard i.e. an existing footway is present on both sides of the existing A5 through Magheramason. Moreover, considerable lengths of the footways on both sides are segregated from the existing A5 by a grass verge.

Existing NMU Crossing Facilities (See Drawing D-003)

There appears to be a number of controlled NMU crossing facilities located in Magheramason. Their locations are as follows;

NMU crossing with refuge island located approximately 60m south of the Clampernow Road junction with the existing A5 (see drawing D-003).

A Pelican Crossing has been provided within 100m of the Magheramason Presbyterian Church (North).



Photograph 1: Existing Pelican Crossing: Approx. 100m North of Magheramason Presbyterian Church (See Drawing D-003)

An uncontrolled pedestrian crossing has been provided outside the Magheramason Presbyterian Church (South).



Photograph 2: NMU Crossing with Refuge Island: Outside Magheramason Presbyterian Church (See Drawing D-003)



Photograph 3: Existing Uncontrolled Pedestrian Crossing: Victoria Meadows junction with the A5 (See Drawing D-003)

An uncontrolled crossing point with pedestrian guardrails have been provided at the entrance to the Victoria Meadows residential development off the A5, Victoria Road.



Photograph 4: NMU Crossing with Refuge Island: Approx. 40m south of Victoria Meadows junction with the existing A5 (See Drawing D-003)

An uncontrolled pedestrian crossing point has been provided across the mouth of the junction of the Mason Road with the A5, Victoria Road. This crossing is associated with Opportunity 3(b).

Uncontrolled pedestrian crossings have been provided across the mouth of the junction of the Dunalong Road with the A5, Victoria Road and the entrance to Brookfield off the Dunalong Road. This crossing is associated with Opportunity 3(c).

Existing Facilities – Offline Bus Stops (See Drawing D-003)

2No. bus lay-bys have been provided south of the entrance to the Victoria Meadows residential development. Both bus stops are served by the refuge island crossing mentioned above.

Existing Parking Facilities (See Drawing D-003)

There are limited parking opportunities along the existing A5, Victoria Road through Magheramason. With the exception of the Magheramason Presbyterian Church there are few attractors along the A5, Victoria Road which would likely generate / require online parking. The Magheramason Presbyterian Church has its' own car park and due to the proximity of junctions with Mason Road and Dunnalong Road there is limited opportunity to provide formalised parking for the church on the A5, Victoria Road.



Photograph 5: Existing (Private) Parking facilities at Magheramason Presbyterian Church (See Drawing D-003)

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Magheramason on the existing A5, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5, potential Greenway facilities that could be introduced and potential improvements to existing bus stop facilities.

Opportunity 1 (See Drawing D-003): Village Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built

up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements; the two images below illustrate the existing gateway features.



Photograph 6: Northbound entrance to Magheramason (See Drawing D-003)



Photograph 7: Southbound entrance to Magheramason (See Drawing D-003)

Opportunity 2 (See Drawing D-003): NMU Routes & Greenways

There are no formal cycling facilities through Magheramason. With the exception of discrete sections where minor alterations to carriageway would be required the southbound footway could be widened to create a shared use facility. This section of the A5, Victoria Road has been identified in the Northern Ireland Greenway Network Plan for upgrading and/or widening to accommodate cyclists.



Photograph 8: Example of discrete sections where a shared use NMU facility could be incorporated (See Drawing D-003)

Special consideration surrounding the movement of cyclist through the Mason Road / A5, Victoria Road would be required.

Opportunity 3 (See Drawing D-003): Footway Improvements

Opportunity 3(a)

Provide tactile paving at the footway terminus on the Clampernow Road. Clampernow Road is an unclassified local road serving a number of farms and private dwellings.



Photograph 9: Clampernow Road junction with A5 Victoria Road (See Drawing D-003)

Opportunity 3(b)

The northbound footway of the Mason Road appears to be less than the Absolute Minimum 2m recommended by TA 90/05 of the DMRB. A pedestrian guardrail has been provided around the outside of the footway. The proximity of the guardrail to the carriageway edge presents a hazard to cyclists. The type and alignment of the guardrail may also obscure a road user's view of pedestrians approaching the crossing. The boundary wall and guardrail terminus also creates a pinch point in the working width of the footway.



Photograph 10: Mason Road approach to A5 Victoria Road (See Drawing D-003)

Realigning the boundary wall and extending the footway (circa. 1m) into the church carpark would result in a footway of a more suitable width. However, due to the complexity and the involved costs of acquiring land, a more feasible option would be to widen the existing footway into Mason Road's carriageway by approximately 1m. An assessment as to whether a replacement pedestrian guardrail which conforms to Department for Infrastructure's requirements should be undertaken

Opportunity 3(c)

The eastbound footway of the Dunalong Road appears to be less than the Absolute Minimum 2m recommended by TA 90/05 of the DMRB. A pedestrian guardrail has been provided around the outside of the footway. The proximity of the guardrail to the carriageway edge presents a hazard to cyclists. The type and alignment of the guardrail may also obscure a road user's view of pedestrians approaching the crossing. The Give Way sign at the back of the footway and guardrail terminus also creates a pinch point in the working width of the footway.

Realigning the timber fence / wall and extending the hardstanding (circa. 1m) into the grassed area at the back of the footway would result in a footway of a more suitable width. Due to the width of the junction (estimated 10-15m) the option exists for the footway to be extended into the carriageway in order to facilitate the expansion to reduce pre-existing hazards whilst removing the pinch point. An assessment as to whether a replacement pedestrian guardrail which conforms to Department for Infrastructure's requirements should be undertaken.

Opportunity 4 (See Drawing D-003): Speed Limit Reduction

This Opportunity relates to the potential speed limit reduction through the village of Magheramason from 40mph to 30mph and associated traffic calming along the length of the village and adjacent roads to coincide with the proposed speed limit reduction. This may consist of build-outs / narrowings, chicanes, road humps, raised junctions, textured surfacing etc.

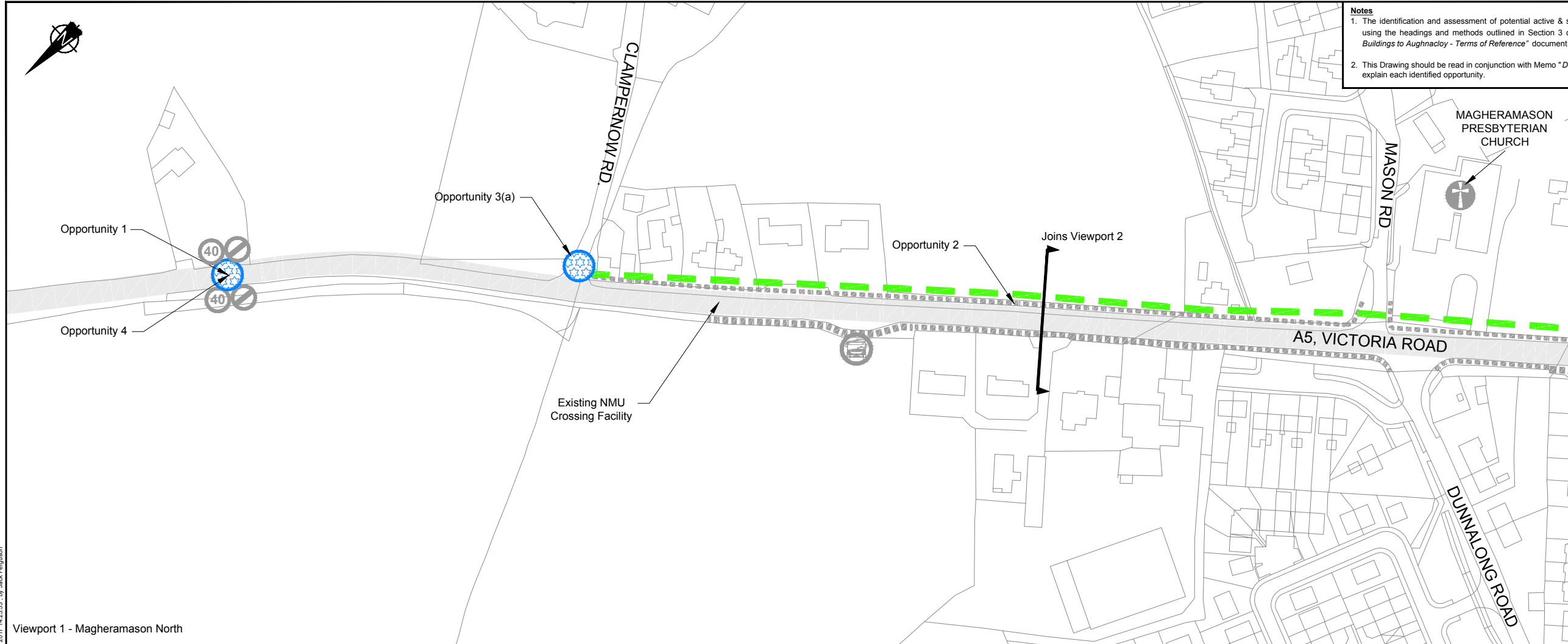
The Design Manual for Roads and Bridges (DMRB) recommends that where appropriate, the use of 30mph limits in villages is encouraged. In such villages, traffic calming or traffic management measures are likely to be needed to help enforce the limit. Most traffic calming schemes will consist of a combination of physical and non-physical measures and as far as practicable schemes with speed reductions introduced should be self-enforcing. Such schemes would be subject to detailed assessments carried out by the Departments Traffic Management and Network Development section and the Police Service of Northern Ireland (PSNI) traffic section.

Park and Ride Facilities

During the course of this assessment Translink have been consulted and have not identified Magheramason as a desirable location for a Park and Ride Facility.



Notes
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 2. This Drawing should be read in conjunction with Memo "DM05 - A5 - Active & Sustainable Transport Assessment: Magheramason" which will explain each identified opportunity.



Key

Existing Asset Features

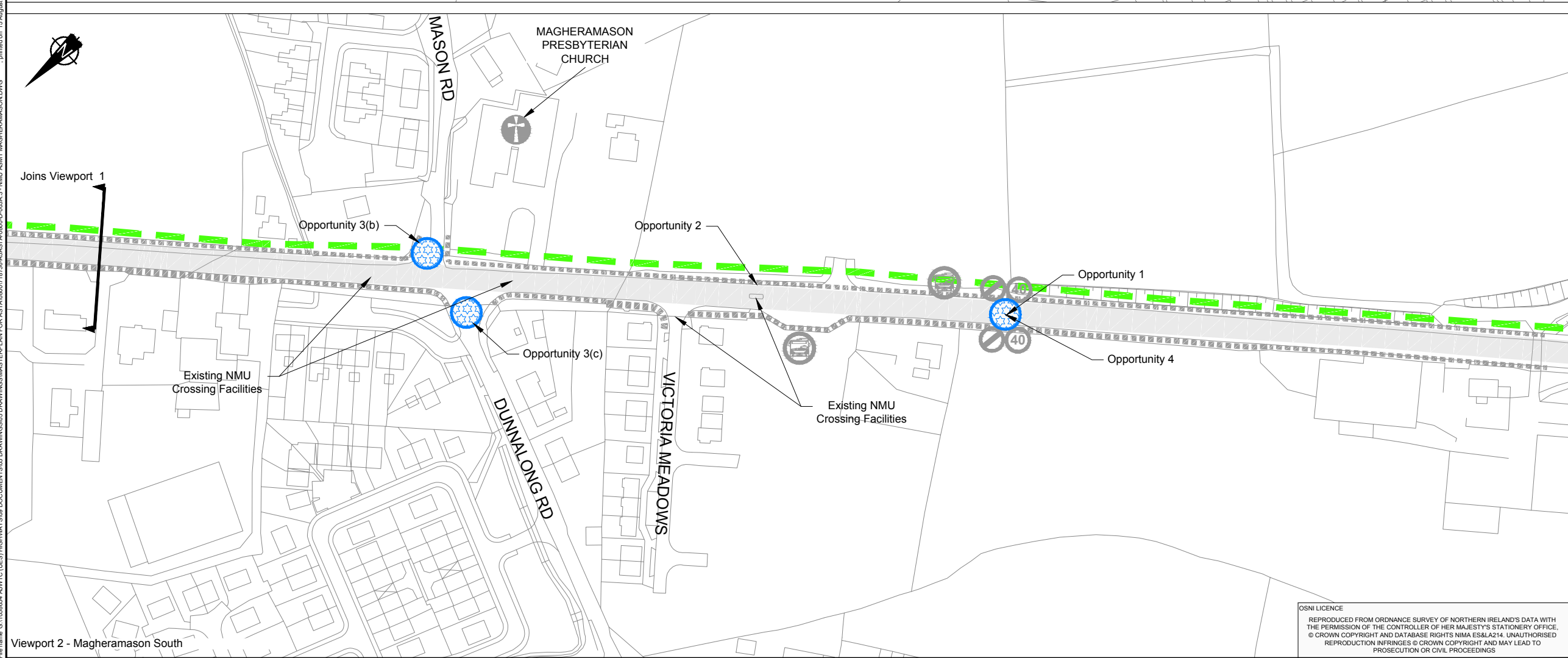
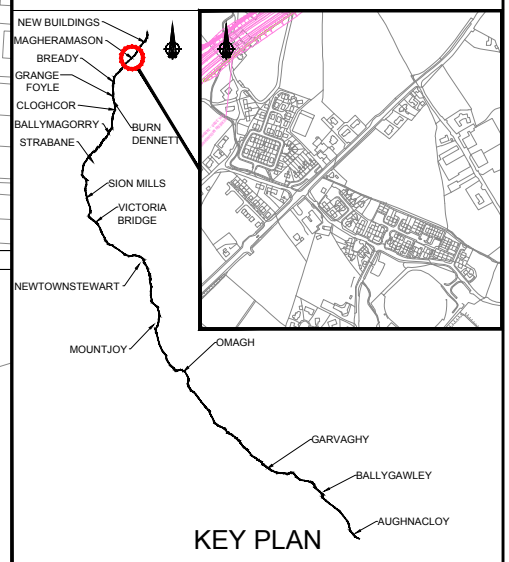
- NMU Facility: [Icon]
- Signage: [Icons for 50, 40, 30]
- School: [Icon]
- Residential Care Homes (RH): [Icon]
- Bus Stop: [Icon]
- Sports / Cricket Club: [Icon]
- Church: [Icon]
- Health Care (HC): [Icon]

Proposed Improvements

- 2m Wide NMU Facility: [Icon]
- 3m Wide (Shared Use) NMU Facility: [Icon]
- Existing NMU Facility to be Widened: [Icon]
- Existing NMU Facility to be Widened (via Potential Greenway): [Icon]
- Potential Greenway Route: [Icon]
- Highway Realignment with new Shared Use NMU Facility: [Icon]
- Opportunity Described in Memo: [Icon]

Other Features

- Proposed A5WTC Scheme: [Icon]



DRAWING STATUS
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SITE/PROJECT:
Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy **A5WTC**

TITLE:
ASSESSMENTS OF URBAN AREAS
LOCATION: MAGHERAMASON, CO. TYRONE
SHEET 1 OF 1

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: DM	DATE: 14/08/2017
DRAWING NO: 718736-A5ASTA-0800-D-003		REV: A

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DM06 – A5 Active & Sustainable Transport Assessment: Bready

Description

The identification and assessment of potential active & sustainable transport opportunities in Bready, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 – New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo should be read in conjunction with Drawing: 718736-A5ASTA-0800-D-004 ‘Assessment of Urban Areas, Location: Bready, Co. Tyrone’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic.

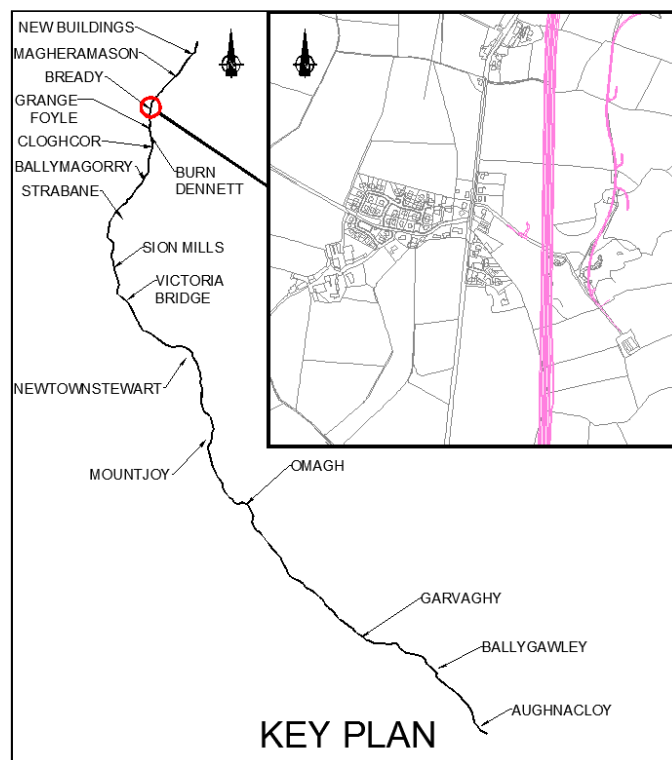


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 through Bready are inadequate, with limited crossing points both controlled and uncontrolled.

Existing Footways (See Drawing D-004)

A footway is currently provided at discrete locations adjacent to either side of the northbound and southbound carriageway of the existing A5.

There are no other NMU attractors along on the northbound side of the carriageway. Apart from Dunnalong Parish Church and two bus stops, with the exception of localised improvements it is not proposed to provide a footway continuous on the northbound side of the carriageway.

Existing Cycling Facilities / NI Greenway Networks (See Drawing D-004)

There are no formal cycling facilities through Bready. This section of the A5, Victoria Road has been identified in the Northern Ireland Greenway Network Plan for upgrading to accommodate cyclists.

Existing Parking Facilities

There are limited parking opportunities along the existing A5, Victoria Road through Bready. With the exception of the Dunnalong Parish Church there are few attractors along the A5, Victoria Road which would likely generate / require online parking. The Dunnalong Parish Church does not appear to have its own off street parking and it is unclear whether patrons typically park along the A5, Victoria Road or on the unclassified Dunnalong Road.

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Bready, the potential addition of pedestrian crossing facilities and on roads that intersect the existing A5, potential Greenway facilities that could be introduced and potential improvements to existing bus stop facilities.

The proposal would include for the provision of a 3m wide shared use facility adjacent to the southbound carriageway (via the greenway proposals). Land acquisition including church grounds and the demolition of an existing outbuilding, would be necessary if realigning part of the existing carriageway, although this is not deemed to be a desirable option. Properties adjacent to the southbound side of the existing carriageway located at the southern end of Bready would prevent widening the existing footway.

To the northern end of Bready existing ghost island junctions at Cloghboy Road and Bready Reformed Presbyterian Church could potentially allow the existing southbound footway to be extended into the carriageway if deemed necessary.

As part of the A5WTC scheme, a new footway kerb provision is to be constructed along the existing A5 where the new road carriageway passes over kerbs located within 2m of the carriageway are to be half battered kerbs with a 100mm upstand.

Opportunity 1 (See Drawing D-004): Village Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements; the two images below illustrate the existing gateway features.



Photograph 1: Northbound entrance to Bready (See Drawing D-004)



Photograph 2: Southbound entrance to Bready (See Drawing D-004)

Opportunity 2 (See Drawing D-004): Relocation of Speed Limit Signage

This Opportunity relates to the potential relocation of an existing speed limit change circa 100m north at the northern entrance to Bready. This Opportunity is related to Opportunity 3(a) the introduction of an uncontrolled NMU crossing facility with a refuge island. This could not be implemented without a Departure from Standard at its proposed location shown on drawing D-004 without the relocation of the commencement of the 40mph speed limit zone, as TA91/05 'Provision for Non-Motorised Users' states in clause 6.18 "Any island on a road with a speed limit greater than 40mph, that is not part of a single lane dualling design, requires 'Departure from Standards' approval.

Opportunity 3 (See Drawing D-004): Pedestrian Crossing Facilities

Opportunity 3(a) & 3(b)

Immediately north of the northern gateway to Bready village there are two offline bus laybys which appear to service the nearby school off the Cloghboy Road.

The footway adjacent to the northbound carriageway which leads from the northbound bus stop currently terminates with no tactile paving at a location where the carriageway is circa. 10m wide. School children who disembark the southbound bus don't appear to have a designated point to cross the A5, Victoria Road. The distance to be crossed and

the absence of a controlled crossing may present problems to pedestrians with impaired mobility.

A controlled pedestrian crossing with a pedestrian refuge island could be provided to allow pedestrians who disembark the northbound bus at the bus stop and need to walk into Bready. This crossing point would need to be located at a suitable distance from where the realigned Cloghboy Road ties into the existing A5 (as part of the A5WTC Scheme) – approx. 50m south of new junction (Opportunity 3(a)).

An uncontrolled pedestrian crossing could also be installed across the junction where the realigned Cloghboy Road ties into the existing A5. Dropped kerbs and tactile paving could be installed along the footways desire line. Crossing distance is approximately 6m. (Opportunity 3(b)).



Photograph 3: Southbound bus facility on the A5 Victoria Road entering Bready (See Drawing D-004)

Opportunity 4 (See Drawing D-004): NMU Routes & Greenways

Opportunity 4(a)

The southbound footway outside the Bready Reformed Presbyterian Church appears to be less than the Absolute Minimum 2m recommended by TA 90/05 of the DMRB. Realigning the boundary wall and extending the footway (circa. 2m) into the church carpark or extending into the road and utilising the hatched area would result in a shared NMU facility of a more suitable width.



Photograph 4: Bready Reformed Presbyterian Church (See Drawing D-004)

Opportunity 4(b)

To provide a 3m wide shared NMU facility at this location, the outbuilding on the inside of the bend would need to be removed. Removing this building would also increase southbound forward visibility and would allow for the horizontal alignment of the carriageway to be improved reducing the risk of errant vehicle crossing the centreline and leaving the carriageway at this location.

This approach would likely be quite costly and difficult to justify without establishing a clear need for such a facility.

Opportunity 4(c)

Following on from Opportunity 4(b), over the next 250m no footway is provided on either side of the carriageway. The existing grass verge and over the edge drainage system along the southbound edge of the carriageway could be replaced with a 3m wide shared NMU facility on either side and an alternative DMRB compliant drainage system could be provided. Such an approach would likely involve the purchase of a strip of agricultural land (Circa. 4m x 250m) and the replacement of the field boundary adjacent to the carriageway. At least two utility poles and one road sign would also require re-location.



Photograph 5: Northbound on the A5 Victoria Road south of the Bready Reformed Presbyterian Church (See Drawing D-004)

Opportunity 4(d)

The extension of the existing footway along Victoria Road could result in the provision of a 3m shared NMU facility through the village. The costs associated with this proposal may be prohibitive and difficult to justify without establishing a clear need for such a facility.

Opportunity 4(e)

To provide a 3m wide shared NMU facility the carriageway would have to be realigned to the west into the northbound verge.

Opportunity 5 (See Drawing D-004): Bus Stop Facilities

A paved area and shelter could be provided for the bus stop currently in the northbound verge. This would likely require the purchase of agricultural land and the extension of the existing carriageway embankment to ensure the shelter is at suitable level with respect to the carriageway.

Opportunity 6 (See Drawing D-004): Pedestrian Crossing Facilities

Provide an uncontrolled pedestrian crossing across the junction of Dunnalong Road and the A5, Victoria Road.

Opportunity 7 (See Drawing D-004): Pedestrian Crossing Facilities

Before the terminus of the footway on Dunalong road, provide an uncontrolled crossing point across the A5, Victoria road to cater for NMU's travelling to and from Dunalong Road.



Photograph 6: Dunalong Road approach to A5 Victoria Road (See Drawing D-004)

Opportunity 8 (See Drawing D-004): Pedestrian Crossing Facilities

A bus shelter and offline bus bay has been provided next to Dunnalong Parish Church. A uncontrolled crossing point across the A5, Victoria Road should be provided for mobility impaired pedestrians to cross to and from this bus stop.



Photograph 7: A5 Victoria Road south of the Dunnalong Road exit (See Drawing D-004)

Opportunity 9 (See Drawing D-004): Footway Improvements

The existing bus stop at the southern end of the village would need to be relocated to facilitate the widening of the footway to the 3m (min.) required for the shared NMU facility. The proximity of the existing bus stop to the entrance to the cul de sac means that a standard layby could not be provided without significantly impacting on the entrance and garden of the private dwelling to the north of the existing bus stop. It should also be noted that at least 2No. mini pillars at this location would also need to be relocated if a 3m shared NMU facility was to be provided.

Opportunity 10 (See Drawing D-004): Pedestrian Crossing Facilities

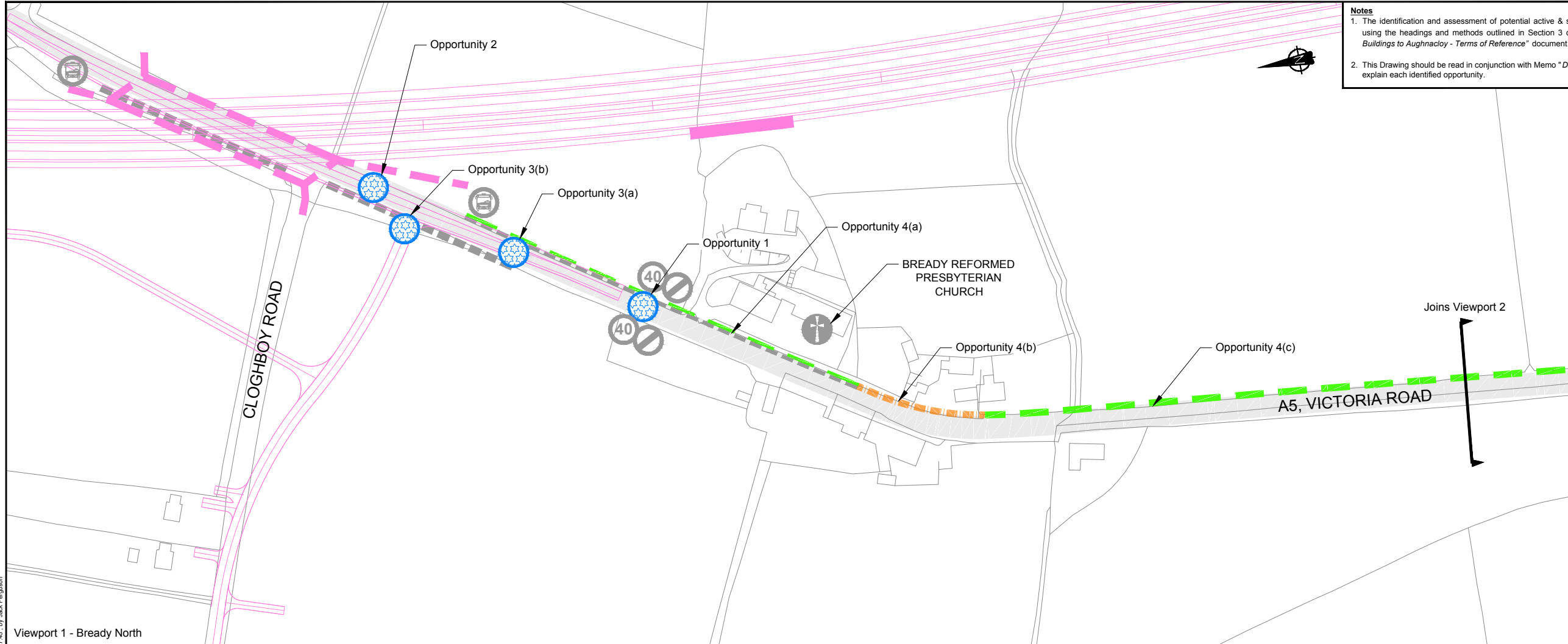
Provide an uncontrolled pedestrian crossing across the entrance to cul-de-sac.



Photograph 8: Cul-de-sac entrance south of southbound bus facility on the A5 Victoria Road exiting Bready (See Drawing D-004)

Park and Ride Facilities

During the course of this assessment Translink have been consulted and have not identified Bready as a location for a Park and Ride Facility.



Viewport 1 - Bredy North



Viewport 2 - Bredy South

Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM06 - A5ASTA - Active & Sustainable Transport Assessment: Bredy" which will explain each identified opportunity.

Key

Existing Asset Features

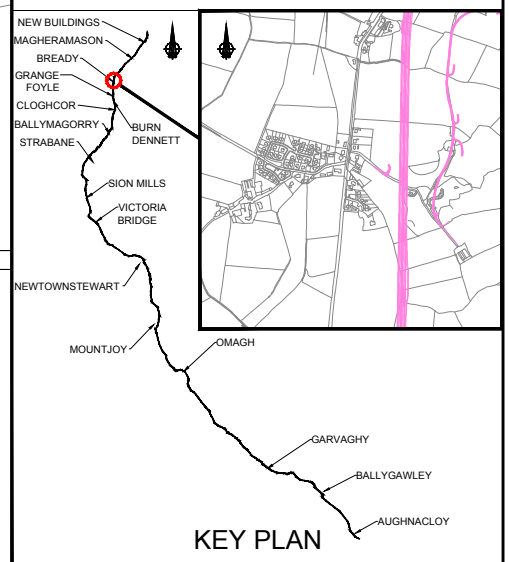
- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

Proposed A5WTC Scheme



KEY PLAN

DRAWING STATUS: FOR ISSUE

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STEP/PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy

TITLE: ASSESSMENTS OF URBAN AREAS
 LOCATION: BREDY, CO. TYRONE
 SHEET 1 OF 1

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: DM	DATE: 14/08/2017

DRAWING NO: 718736-A5ASTA-0800-D-004	REV: A
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File name: G:\1058654_A5WTC (UES) HIGHWAYS\08 DOCUMENTS\05 DRAWINGS\05 DRAWINGS\MASTERPLAN FOR ASTA\0800\718736-A5ASTA-0800-D-004-3 - NMU ASMT BREDY.DWG . printed on: 15 August 2017 14:27:45 by Jack Ferguson

DM07 – A5 Active & Sustainable Transport Assessment: Grangefoyle Burndennett

Description

The identification and assessment of potential active & sustainable transport opportunities in Grangefoyle / Burndennett, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo should be read in conjunction with drawings: 718736-A5ASTA-0800-D-031 to 032 ‘Assessments of Urban Areas, Location: Grangefoyle / Burndennett, Co. Tyrone’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic.

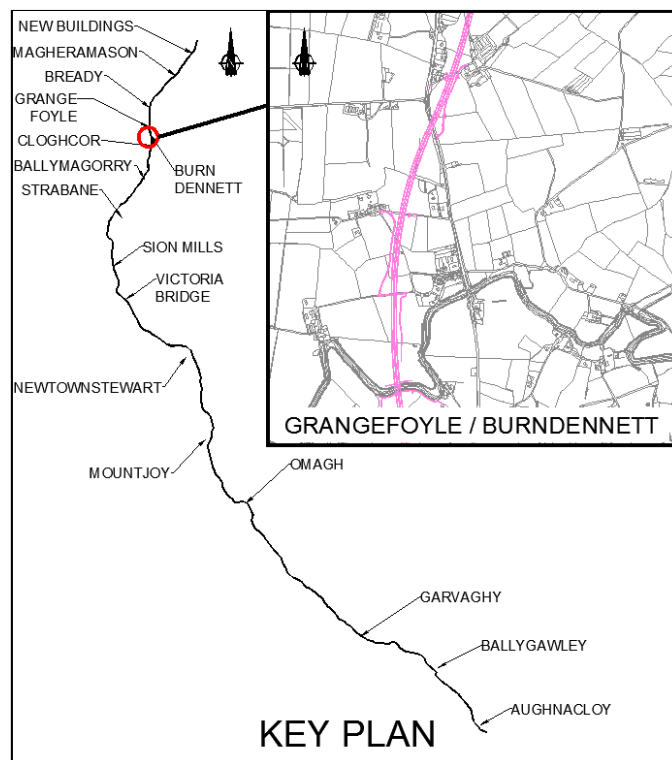


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 through Grangefoyle and Burdennett are relatively limited. There are two short stretches of footway adjacent the southbound carriageway (approximate lengths 350m and 135m), and a further two stretches adjacent the northbound carriageway (approximate lengths 135m and 115m). These footways are largely segregated from the existing A5 by a wide grass verge.

Existing Parking Facilities (see Drawings D-032)

There are limited parking opportunities along the existing A5 Victoria Road through Grangefoyle and Burdennett. However, a lay-by is present on the Burdennett overbridge structure adjacent to the southbound bus stop, which includes five parallel parking spaces. With the exception of Burdennett Cricket Club (which has its own parking facilities), there appears to be no other attractors along this section of the A5 Victoria Road which would require on-line parking.



Photograph 1: Existing parking facilities on Burdennett Overbridge Structure (see drawings D-032)

Existing NMU Crossing Facilities (see Drawings D-031 and D-032)

There is one existing NMU crossing facility that traverses the existing A5 Victoria Road at the Burdennett overbridge structure. Drop kerbs appear to be the only NMU crossing attributes. See Opportunity 2(g) for potential improvements to this existing NMU crossing facility.



Photograph 2: Uncontrolled NMU Crossing (see drawing D-032): Burndennett Overbridge Structure

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Grangefoyle and Burndennett on the existing A5, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5, potential Greenway facilities that could be introduced, potential improvements to existing bus stop facilities on the existing A5, and the potential reduction in speed limit on the Drummenny Road (the main route from the existing A5 to the Burndennett Cricket Club).

Opportunity 1 (see Drawings D-031 and D-032): Village Gateway signage, road markings, etc.

Opportunity 1(a): Grangefoyle

Gateway signage could be added to modify driver behaviour and reduce mean speeds.

Consideration should be given to reducing the speed limit over the length of the village.

This potential improvement consists of adding enhanced gateway signage, where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements; the two images below illustrate the existing gateway features.



Photograph 3: Southbound entrance to Grangefoyle (see drawing D-031)



Photograph 4: Northbound entrance to Grangefoyle (see drawing D-031)

Opportunity 1(b): Burndennett

Gateway signage could be added to modify driver behaviour and reduce mean speeds.

This potential improvement consists of adding enhanced gateway signage, where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements; the two images below illustrate the existing gateway features.

Consideration should be given to reducing the speed limit over the length of the village.



Photograph 5: Southbound entrance to Burndennett (see drawing D-031)



Photograph 6: Northbound entrance to Burndennett (see drawing D-032)

Opportunity 2 (see Drawings D-031 and D-032): Pedestrian Crossing facilities

Opportunity 2(a)

Provide an uncontrolled NMU crossing across the Existing A5 approximately 65m north of Willow Road's junction with the Existing A5 (with drop kerbs, tactile paving and potentially a pedestrian refuge island located in the central reserve hatching). This crossing facility will cater the existing northbound and southbound bus stops.

This crossing with Central Island, would be provided in combination with the reduction in speed limit outlined in Opportunity 1.



Photograph 7: Southbound on A5 Victoria Road, North of Willow Road (see drawing D-031)

Opportunity 2(b)

Provide an uncontrolled NMU crossing across the junction where Willow Road meets the Existing A5. This NMU crossing will form part of the potential greenway route identified in Opportunity 3.

Opportunity 2(c)

Provide an uncontrolled NMU crossing across the junction where Grangefoyle Road meets the Existing A5.

Opportunity 2(d)

Provide an uncontrolled NMU crossing across the Existing A5 approximately 20m south of Grangefoyle Road's junction with the Existing A5 (with drop kerbs, tactile paving and potentially a pedestrian refuge island located in the central reserve hatching).

This crossing with Central Island, would be provided in combination with the reduction in speed limit outlined in Opportunity 1.

Opportunity 2(e)

Provide an uncontrolled NMU crossing across the junction where Drumenny Road meets the Existing A5.

Opportunity 2(f)

Provide an uncontrolled NMU crossing across the junction where Lisdivin Road meets the Existing A5. This NMU crossing will form part of the potential greenway route identified in Opportunity 3.



*Photograph 8: Southbound on A5 Victoria Road at the Lisdivin Road Junction
(see drawing D-032)*

Opportunity 2(g)

Upgrade the existing NMU crossing across at the Burdennett overbridge structure. Drop kerbs, tactile paving and a pedestrian refuge island located in the central reserve hatching could potentially be added. This crossing will cater the existing southbound bus stop.

This crossing with Central Island, would be provided in combination with the reduction in speed limit outlined in Opportunity 1.



Photograph 9: Southbound on A5 Victoria Road, existing NMU crossing facility upgrade (see drawing D-032)

Opportunity 3 (see Drawings D-031 and D-032): NI Greenway Network

A potential 3m wide Greenway has been identified during the stakeholder consultation exercise. The potential Greenway would be located adjacent the southbound carriageway of the existing A5, utilising the grass verge for throughout, with the exception of widening two lengths of existing footway to 30m (approximately 0.5km in length).

Opportunity 4 (see Drawings D-031 and D-032): Bus Stop Facilities

Opportunity 4(a)

A shelter could potentially be installed at the southbound bus stop. The southbound bus stop is also on-line; to alert drivers' attention to the presence of the on-line bus stop red bus bay road markings could be provided.



Photograph 10: Southbound on A5 Victoria Road north of Willow Road junction (see drawing D-031)

Opportunity 4(b)

In the image above, the northbound bus stop is on-line; to alert drivers' attention to the presence of the on-line bus stop red bus bay road markings could be provided.

Opportunity 4(c)

The northbound off-line bus stop's shelter could potentially undergo improvement works.



*Photograph 11: Southbound on A5 Victoria Road north of Drumenny Road junction
(see drawing D-032)*

Opportunity 4(d)

A shelter could potentially be installed at the southbound bus stop.

Opportunity 5 (see Drawings D-031 and D-032): Footway Improvements

Opportunity 5(a)

A 20m long, 2.0m wide NMU facility could potentially be provided linking the Grangefoyle Road crossing points identified in Opportunity 2(c) to the crossing point across the existing A5 Opportunity 2(d). This NMU facility will utilise the existing wide grass verge adjacent the northbound carriageway.

Opportunity 5(b)

A 125m long, 2.0m wide NMU facility could potentially be provided linking Ash Avenue to the existing NMU facility further south. This NMU facility will utilise the existing wide grass verge adjacent the northbound carriageway.



Photograph 12: Southbound on A5 Victoria Road south of Ash Avenue junction (see drawing D-031 and D-032)

Opportunity 5(c)

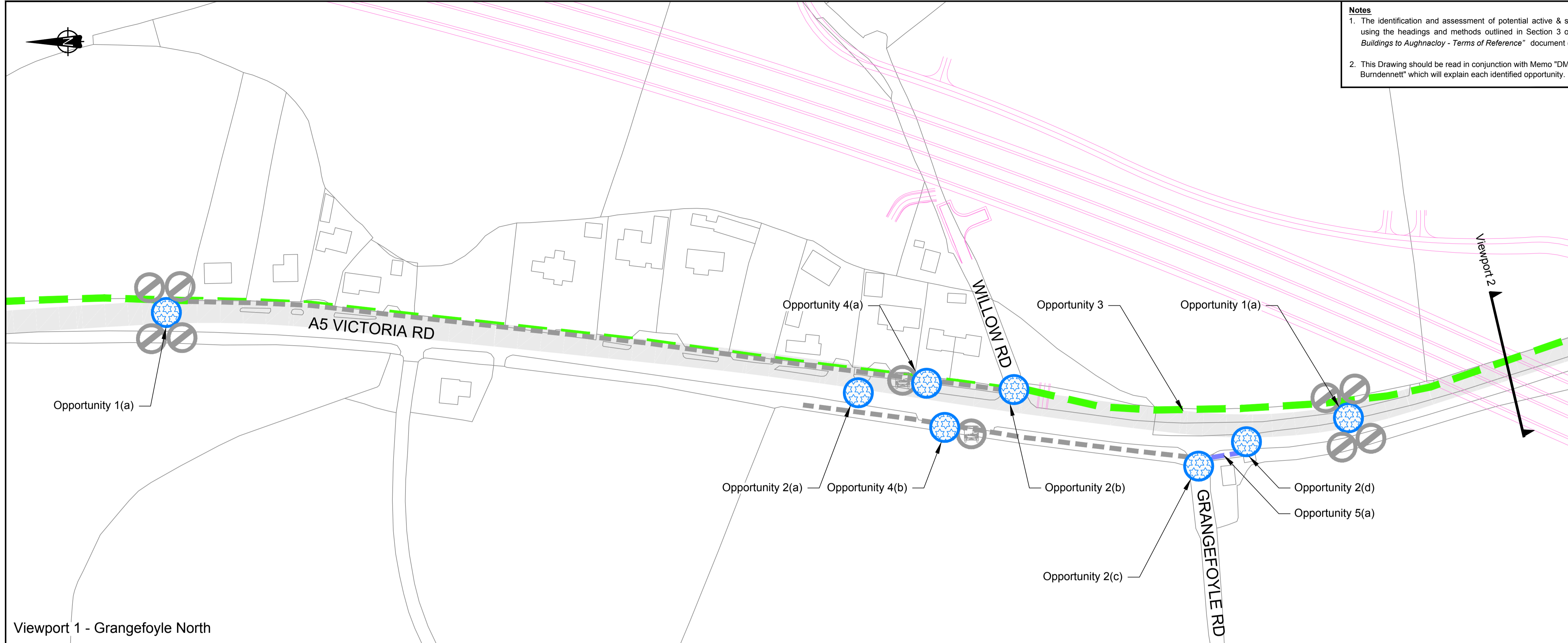
A 125m long, 2.0m wide NMU facility could potentially be provided along the eastbound carriageway of Drumenny Road. This NMU facility will utilise Drumenny Road's wide carriageway and will link four residential properties to the existing A5. 2no. overhead poles will potentially need repositioned.

Opportunity 6 (see Drawings D-032): Reduction in Speed Limit along Drumenny Road (From NSL to 30mph)

As Drumenny Road is the main route to the Burdennett Cricket Club, NMUs regularly use this road. A reduction in speed limit from National Speed Limit to 30mph may benefit the safety of NMUs.

Drumenny Road has two 90 degree bends along the Cricket Club's perimeter, limiting the driving forward visibility at these corners. This reduced 30mph speed limit may promote safety at these bends for motorised users.

The Design Manual for Roads and Bridges (DMRB) recommends that where appropriate, the use of 30mph limits in villages is encouraged. In such villages, traffic calming or traffic management measures are likely to be needed to help enforce the limit. Most traffic calming schemes will consist of a combination of physical and non-physical measures and as far as practicable schemes with speed reductions introduced should be self-enforcing. Such schemes would be subject to detailed assessments carried out by the Departments Traffic Management and Network Development section and the Police Service of Northern Ireland (PSNI) traffic section.



Viewport 1 - Grangefoyle North



Viewport 2 - Grangefoyle South

Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM07 - A5 - Active & Sustainable Transport Assessment: Grangefoyle Burndennett" which will explain each identified opportunity.

Key

Existing Asset Features

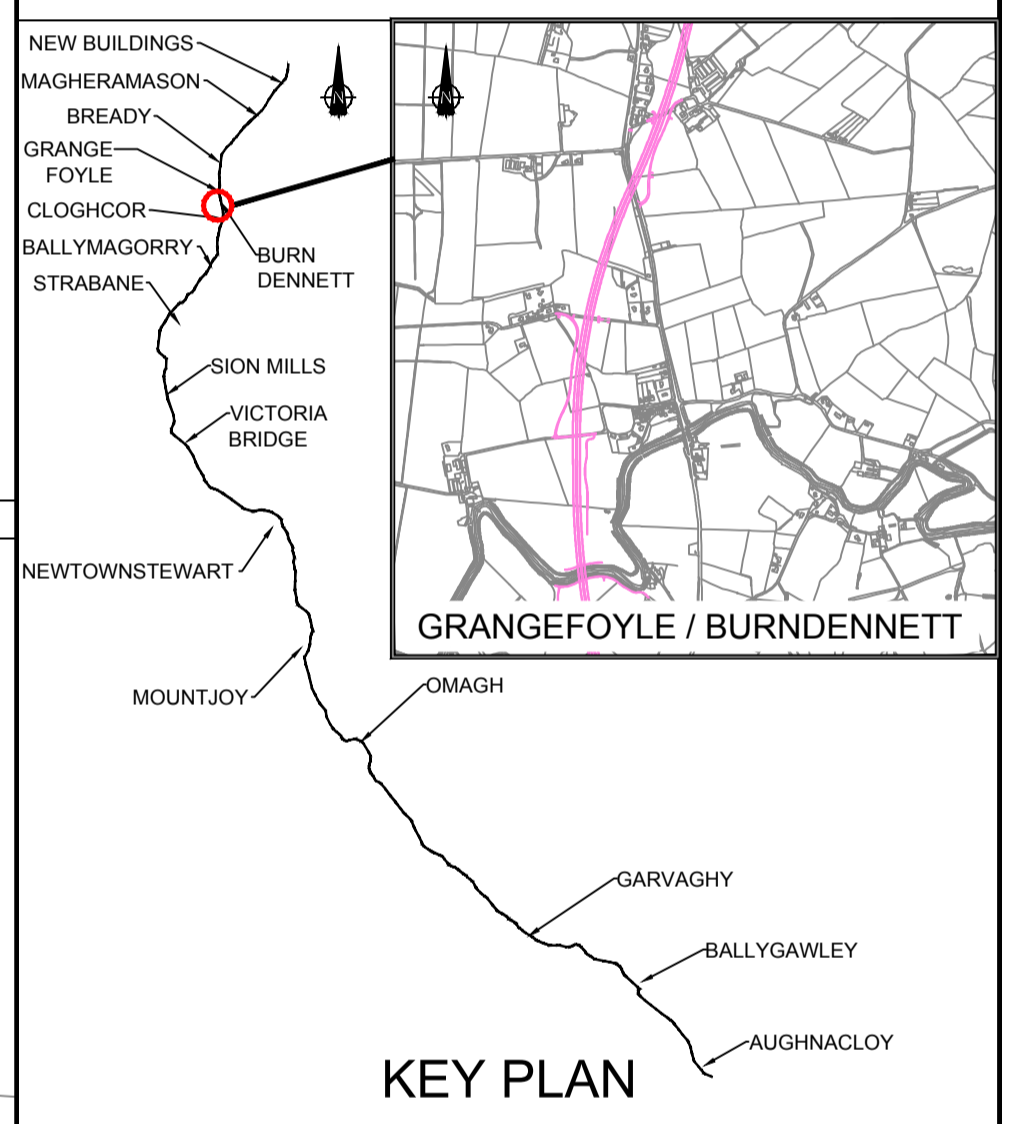
- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme



DRAWING STATUS: FOR ISSUE

wsp

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 www.infrastructure-ni.gov.uk

SITE PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy **A5 WTC**

TITLE: ASSESSMENTS OF URBAN AREAS
 LOCATION: GRANGEFOYLE / BURNDENNETT
 CO. TYRONE SHEET 1 OF 2

SCALE @ A1: 1:1000	CHECKED: xx	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DATE: 14/08/2017
DRAWING NO: 718736-A5ASTA-0800-D-031	REV: A	

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Notes

1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
2. This Drawing should be read in conjunction with Memo "DM07 - A5 - Active & Sustainable Transport Assessment: Grangefoyle Burndennett" which will explain each identified opportunity.

Key

Existing Asset Features

- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme

KEY PLAN

DRAWING STATUS: **FOR ISSUE**

wsp

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CLIENT: Department for Infrastructure
Bonnegair
An Roinn
www.infrastructure-ni.gov.uk

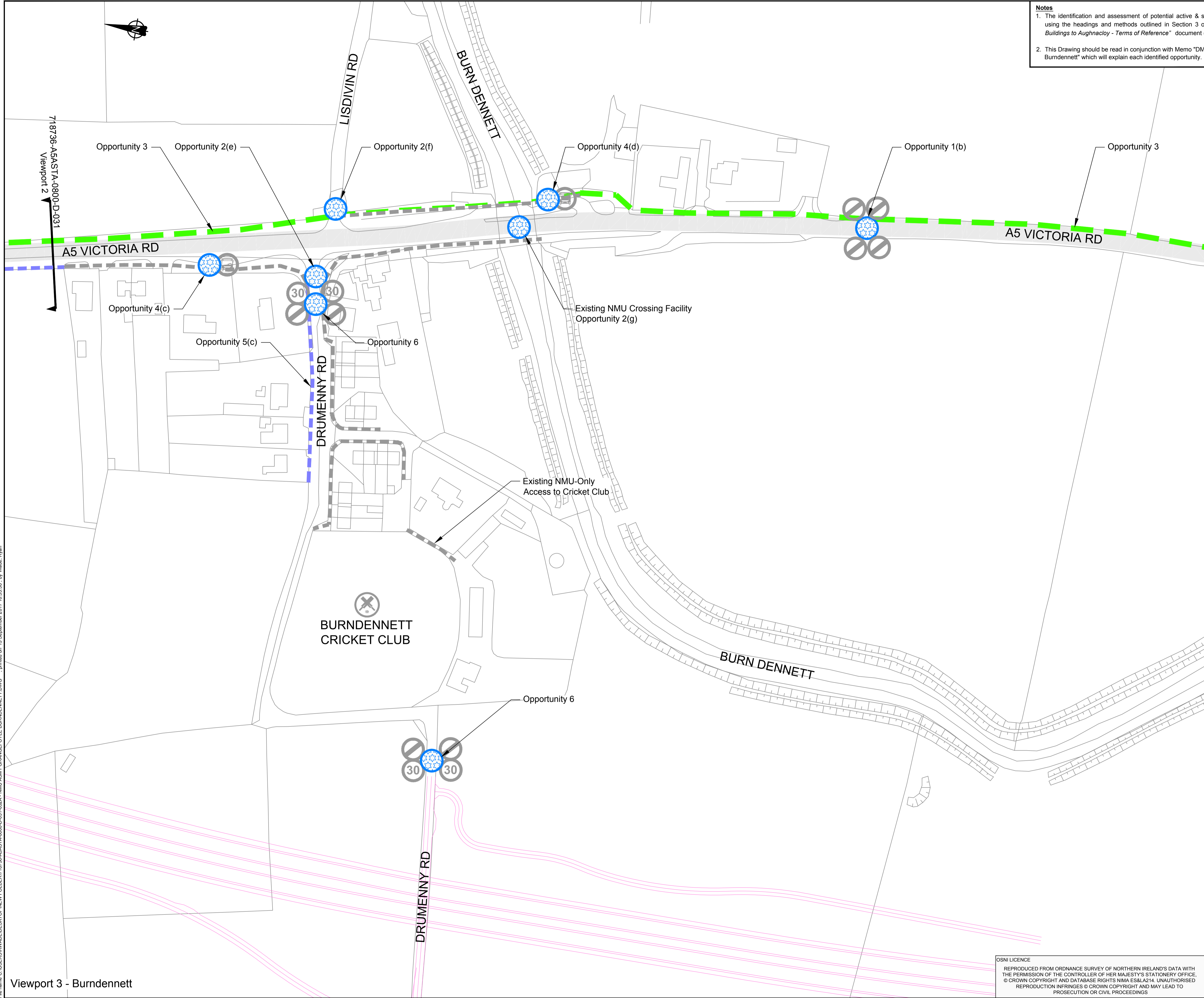
SITE/PROJECT: **Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy**

A5 WTC
Wasteless Transport Corridor

TITLE: ASSESSMENTS OF URBAN AREAS
LOCATION: GRANGEFOYLE / BURNDENNETT
CO. TYRONE SHEET 2 OF 2

SCALE @ A1: 1:1000	CHECKED: xx	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DRAWN: PM
		DATE: 14/08/2017

DRAWING NO: 718736-A5ASTA-0800-D-032	REV: A
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718736-A5ASTA-0800-D-031
Viewport 2

Viewport 3 - Burndennett

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DM08 – A5 Active & Sustainable Transport Assessment: Cloghcor

Description

The identification and assessment of potential active & sustainable transport opportunities in Cloghcor, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 – New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo should be read in conjunction with Drawing: 718736-A5ASTA-0800-D-005 ‘Assessments of Urban Areas, Location: Cloghcor, Co. Tyrone.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic.

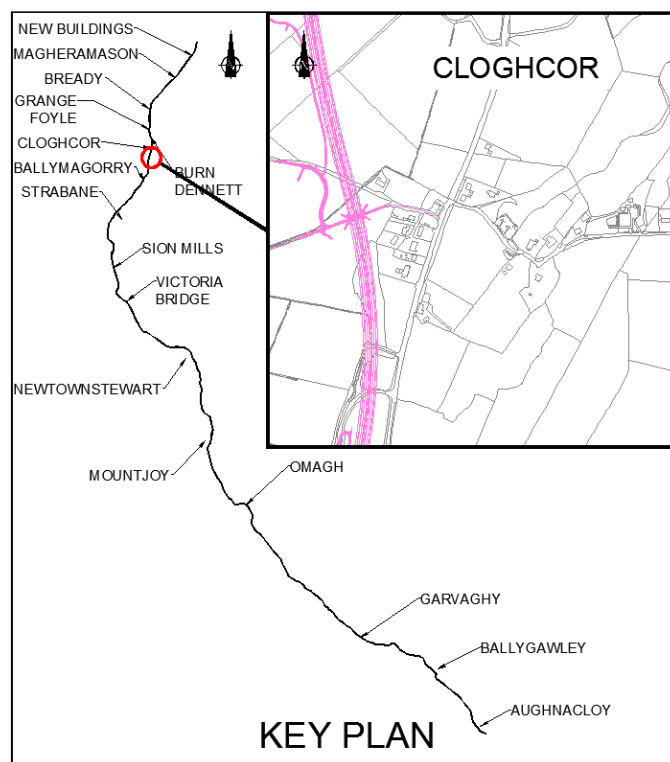


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that there is currently no provision for non-motorised users (NMUs) on and around the existing A5 through Cloghcor. There are only a discrete few sections of footway along Cloghcor Road at present.

This footway is in the vicinity of the church and includes tactile paving where it crosses the entrance to the car park.

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Cloghcor on the existing A5, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5 and potential Greenway facilities that could be introduced.

The proposals include the provision of a 3m wide shared use NMU facility adjacent to the southbound carriageway (via the greenway proposals). In order to provide such facilities, land acquisition would impact land belonging to 3 domestic properties north of Cloghcor Road and the front garden (with a possibility of the building) of a fourth domestic property. These land acquisitions would be necessary due to existing minimal carriageway widths.

Opportunity 1 (See Drawing D-005): Village Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements; the image below illustrates the existing gateway feature.



Photograph 1: Southbound entrance to Cloghcor (See Drawing D-005)

Opportunity 2 (See Drawing D-005): NMU Routes & Greenways

A potential 3m wide Greenway through Cloghcor has been identified during the stakeholder consultation exercise. Due to the constraints of 4No. properties and a pinch point adjacent the northbound carriageway, the Greenway could potentially be constructed along the wide southbound verge of the existing A5.

Opportunity 3 (See Drawing D-005): Bus Stop Facilities

Existing bus shelter to be upgraded to an adequate facility on southbound and northbound carriageway.



Photograph 2: Northbound and Southbound Bus Stops highlighted in red (See Drawing D-005)

Opportunity 4 (See drawing D-005): Pedestrian Crossing Facilities

Opportunity 4(a)

St Mary's Primary School is located approx. 430m from the existing A5 along Cloghcor Road. The 2no. bus stops highlighted in Opportunity 3 serve this school.

There are no formal crossing points on the existing A5 in the vicinity of the school to assist NMUs crossing. An uncontrolled crossing with dropped kerbs and tactile paving could to be provided.



Photograph 3: Uncontrolled Pedestrian Crossing highlighted in red (See Drawing D-005)

Opportunity 4(b)

If the greenway (Opportunity No.2) is constructed, an uncontrolled pedestrian crossing across the junction of Cloghcor Road and the existing A5, Victoria Road, could be provided.

Drop kerbs and tactile paving could be installed along the footway's desire line. The crossing distance is approximately 8.0m.



Photograph 4: Uncontrolled Pedestrian Crossing Cloghcor Road junction with the Existing A5 (See Drawing D-005)

Opportunity 4(c)

An uncontrolled pedestrian crossing could be constructed outside the church (Opportunity 5(b)) to link the existing footway outside the church to the footway described in Opportunity No.8. Drop kerbs and tactile paving could be installed. The crossing distance is approximately 6.0m.

Opportunity 4(d)

An uncontrolled pedestrian crossing could be constructed outside the primary school to link the existing footway outside the school to the footway described in Opportunity No.8. Drop kerbs and tactile paving could be installed. The crossing distance is approximately 6.0m.

Opportunity 5 (See Drawing D-005): Footway Improvements

Opportunity 5(a)

NMUs travelling to the church along Cloghcor Road from the existing A5, need to use the road's carriageway or narrow verge.

A 2.0m wide footway could be constructed along Cloghcor Road's northern verge. The footway length would be approximately 90m. The footway can be fitted into Cloghcor Road's verge in some areas, however some vesting would be required – approximately

an area of 40m x 2m. The verge, for the remaining 51m of footway, would need to be cleared of trees and shrubs.

Appropriate street lighting could be incorporated along this stretch of footway – approximately 3no. lighting columns would be required to provide lighting from the existing A5 to the church.

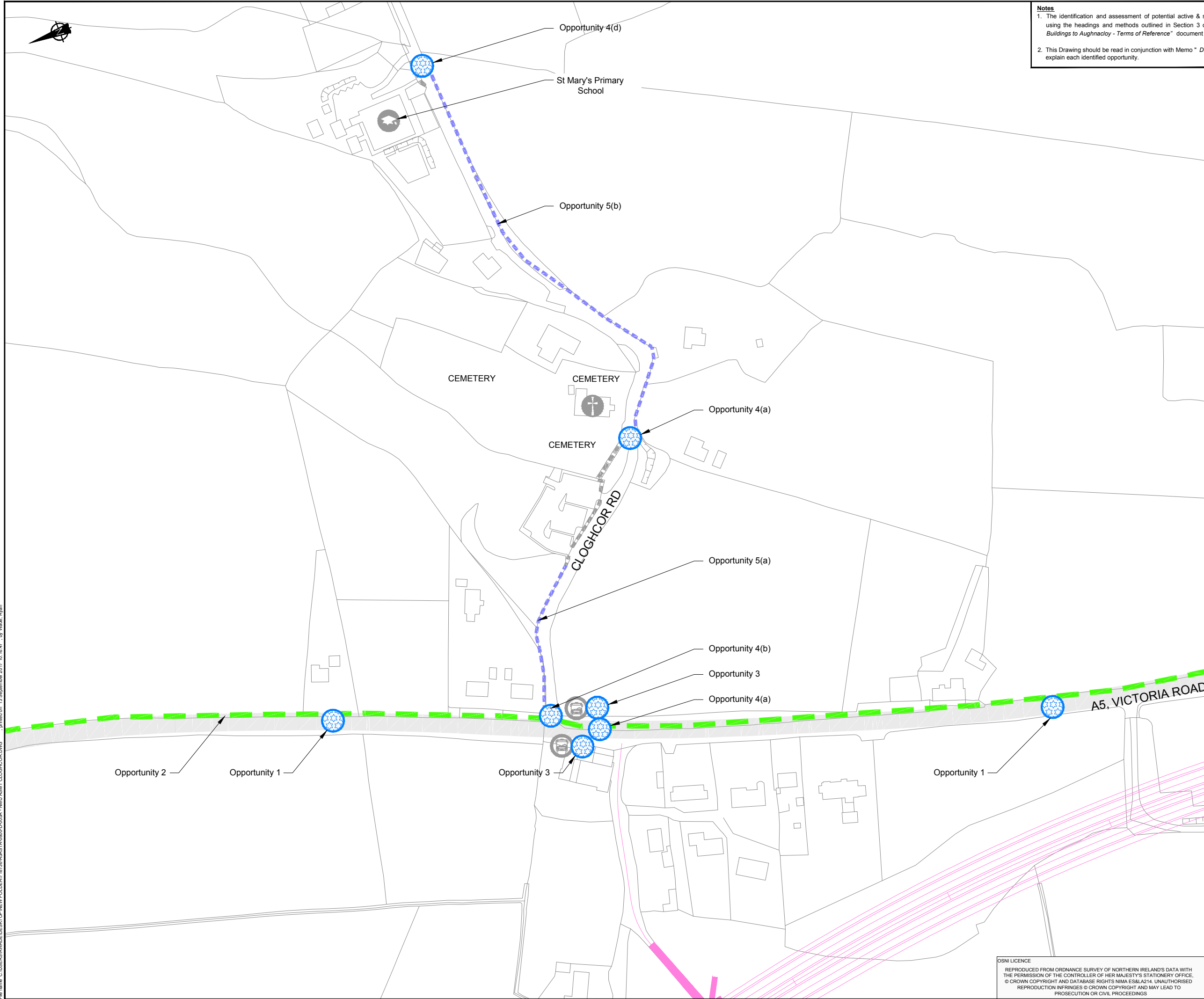
Opportunity 5(b)

At the point where the stairway ends, St. Mary's Primary School is a further 250m climb along Cloghcor Road. Currently this is no provision for footways.

Due to the constraints of the cemetery adjacent the church, it is suggested that a 2.0m wide footway could be constructed on the southern side of Cloghcor Road (opposite the church) – Opportunity No.8. The footway length would be approximately 270m. The footway could be fitted into Cloghcor Road's verge in some areas, however the majority of the section would need to be cleared of trees and shrubs (approx. 150m).

Park and Ride Facilities

During the course of this assessment Translink has been consulted and have not identified Cloghcor as a location for a Park and Ride Facility.



Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM08 - A5 - Active & Sustainable Transport Assessment: Cloghcor" which will explain each identified opportunity.

Key

Existing Asset Features

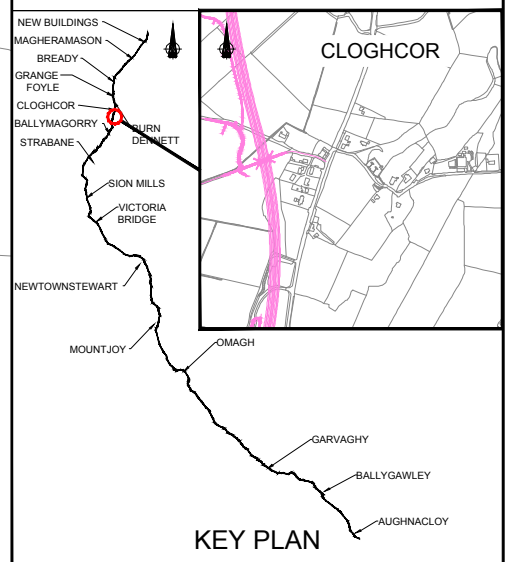
- NMU Facility: [Icon]
- Signage: [Icons: 50, 40, 30]
- School: [Icon]
- Residential Care Homes: [Icon: RH]
- Bus Stop: [Icon]
- Sports / Cricket Club: [Icon]
- Church: [Icon]
- Health Care: [Icon: HC]

Proposed Improvements

- 2m Wide NMU Facility: [Icon]
- 3m Wide (Shared Use) NMU Facility: [Icon]
- Existing NMU Facility to be Widened: [Icon]
- Existing NMU Facility to be Widened (via Potential Greenway): [Icon]
- Potential Greenway Route: [Icon]
- Highway Realignment with new Shared Use NMU Facility: [Icon]
- Opportunity Described in Memo: [Icon]

Other Features

- Proposed A5WTC Scheme: [Icon]



DRAWING STATUS: FOR ISSUE

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STEP/PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy

TITLE: ASSESSMENTS OF URBAN AREAS
 LOCATION: CLOGHCOR, CO. TYRONE
 SHEET 1 OF 1

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: OFK	DRAWN: OFK
		DATE: 14/08/2017

DRAWING NO: 718736-A5ASTA-0800-D-005

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File name: C:\USERS\RWAD\DESKTOP\NEW FOLDER\718736-A5ASTA-0800-D-005A - NMU ASMT CLOGHCOR.DWG, created on: 13 September 2017 10:16:47, by Wade, Ryan

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DM09 – A5 Active & Sustainable Transport Assessment: Ballymagorry

Description

The identification and assessment of potential active & sustainable transport opportunities in Ballymagorry, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 – New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo should be read in conjunction with Drawing: 718736-A5ASTA-0800-D-006 ‘Assessments of Urban Areas, Location: Ballymagorry, Co. Tyrone.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic.

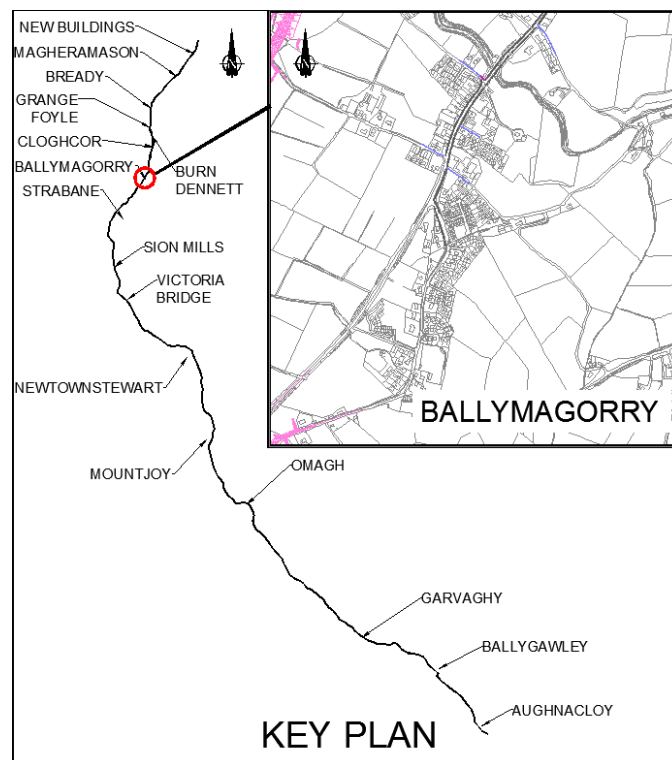


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 through Ballymagorry are of a relatively good standard i.e. an existing footway is present on both sides of the existing A5 through Ballymagorry.

Existing Parking Facilities

There are limited parking opportunities along the existing A5, Victoria Road through Ballymagorry. A lay-by (4No. spaces) is present outside the Ballymagorry Arms. There are limited attractors along the A5, Victoria Road which would likely generate / require on-line parking. Therefore, no additional parking provision has been identified as potential opportunities.



Photograph 1: Existing parking opportunities along the A5 (See Drawing D-006)

Existing NMU Crossing Facilities

There are 2no. existing uncontrolled pedestrian crossings with refuge islands at the northern end of Ballymagorry. These crossing points have drop kerbs and tactile paving. The crossing points serve the northbound and the southbound bus stops and are located within the ghost island hatching associated with a right turn pocket.

Image on following page.



Photograph 2: Existing uncontrolled pedestrian crossing point (See drawing D-006)

There is an existing pedestrian controlled crossing approximately 85m south of the junction where Abbey View meets the A5. This controlled crossing aids access to the petrol filling station, the 2no. bus stops north of the A5 Victoria Road/Abbey View junction, other commercial properties and domestic properties.



Photograph 3: Existing Pedestrian Controlled Crossing (See Drawing D-006)



Photograph 4: Existing uncontrolled pedestrian crossing with a refuge island at the southern end of Ballymagorry (See Drawing D-006)

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Ballymagorry on the existing A5, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5, potential Greenway facilities that could be introduced, and potential improvements to existing bus stop facilities.

Proposals include for the provision of a 3m wide shared use NMU facility, which is anticipated to be achieved by widening the existing southbound footway into the carriageway (via the greenway plans).

Opportunity 1 (See Drawing D-006): Village Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built

up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements; the two images below illustrate the existing gateway features.



Photograph 5: Northern entrance (Southbound) to Ballymagorry (See Drawing D-006)



Photograph 6: Southern entrance (Northbound) to Ballymagorry (See Drawing D-006)

Opportunity 2 (see drawing D-006): NMU Routes & Greenways

A potential 3m wide Greenway through Ballymagorry has been identified during the stakeholder consultation exercise.

The 3m width of the Greenway would be achieved by increasing the width of the existing southbound footway into the carriageway of the existing A5 by approx. 0.5m to 1m. By widening into the carriageway (i.e. reducing the existing A5's carriageways width), land acquisition would not be required – resulting in a more practical and economical option.

Both northbound and southbound footways have constraints in terms of widening, due commercial and domestic properties. However, the Greenway could potentially be constructed along the southbound footway as the footway is wider than the northbound on average.

Opportunity 3 (See Drawing D-006): Pedestrian Crossing Facilities

Opportunity 3(a)

foxlodge Cricket Club is located approximately 55m south of Maple Road junction with the A5 adjacent the northbound carriageway of the existing A5. Currently, there are no points in the vicinity of this sports club. An uncontrolled crossing point with dropped kerb and tactile paving could be incorporated at this point.



Photograph 7: Uncontrolled Pedestrian Crossing (See Drawing D-006)

Note: Crossing Point highlighted with Red Rectangle

Opportunity 3(b)

Provide an uncontrolled pedestrian crossing across the junction of Greenlaw Road and the A5, Victoria Road. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 10m.



Photograph 8: Uncontrolled Pedestrian Crossing Point (See Drawing D-006)

Opportunity 3(c)

Dropped kerbs have been provided for the existing footways on Woodend Road. Tactile paving could be incorporated at both sides of the crossing.

Opportunity 4 (See Drawing D-006): Bus Stop Facilities

Opportunity 4(a)



Photograph 9: Addition of Bus Shelters (See Drawing D-006)

Note: Bus Stops highlighted with Red Circles

Bus stop shelters could be added to the main northbound and southbound bus stops in the town. See image above. These are the main bus stops in Ballymagorry and serve many domestic and commercial properties. The closest controlled crossing point is located 120m south of the bus stops.

Opportunity 4(b)

The bus shelter could be modernised. If the verge is utilised for the shared use NMU facility. 4no. street lighting poles and signage would need to be moved.

As previously mentioned, a potential 3m wide Greenway facility has been identified through Ballymagorry along the southbound verge of the A5. The unobstructed 3m width of this greenway should be unaffected due to this bus stop shelter as the potential greenway will utilise the grass verges at this location.

This bus stop will mainly serve domestic properties along Woodend Road, which shares a junction with the A5 Victoria Road approx. 80m south of the bus stop.



Photograph 10: Bus Shelter Relocation and Refurbishment (See Drawing D-006)

Opportunity 5 (See Drawing D-006): Footway Improvements

Opportunity 5(a)

A potential 2m wide NMU facility could be introduced along Maple Road. Land acquisition would be required to provide this. Due to the residential properties to the south side of the road, the north side would be the appropriate location.

Opportunity 5(b)

A potential 2m wide NMU facility could be introduced along Greenlaw Road. Land acquisition would be required to provide this. There are residential properties on both sides of the east end of Greenlaw Road. As the properties are more on the northern side, the southern side would be considered as the most appropriate location.

Opportunity 5(c)

A potential 2m wide NMU facility could be introduced along Abbey Road. Land acquisition would be required to provide this. There is a verge on the southern side of the road that would lend itself into a footway.

Opportunity 5(d)

A potential 2m wide NMU facility could be introduced along Park Road as per Greenlaw Road.

Opportunity 5(e)

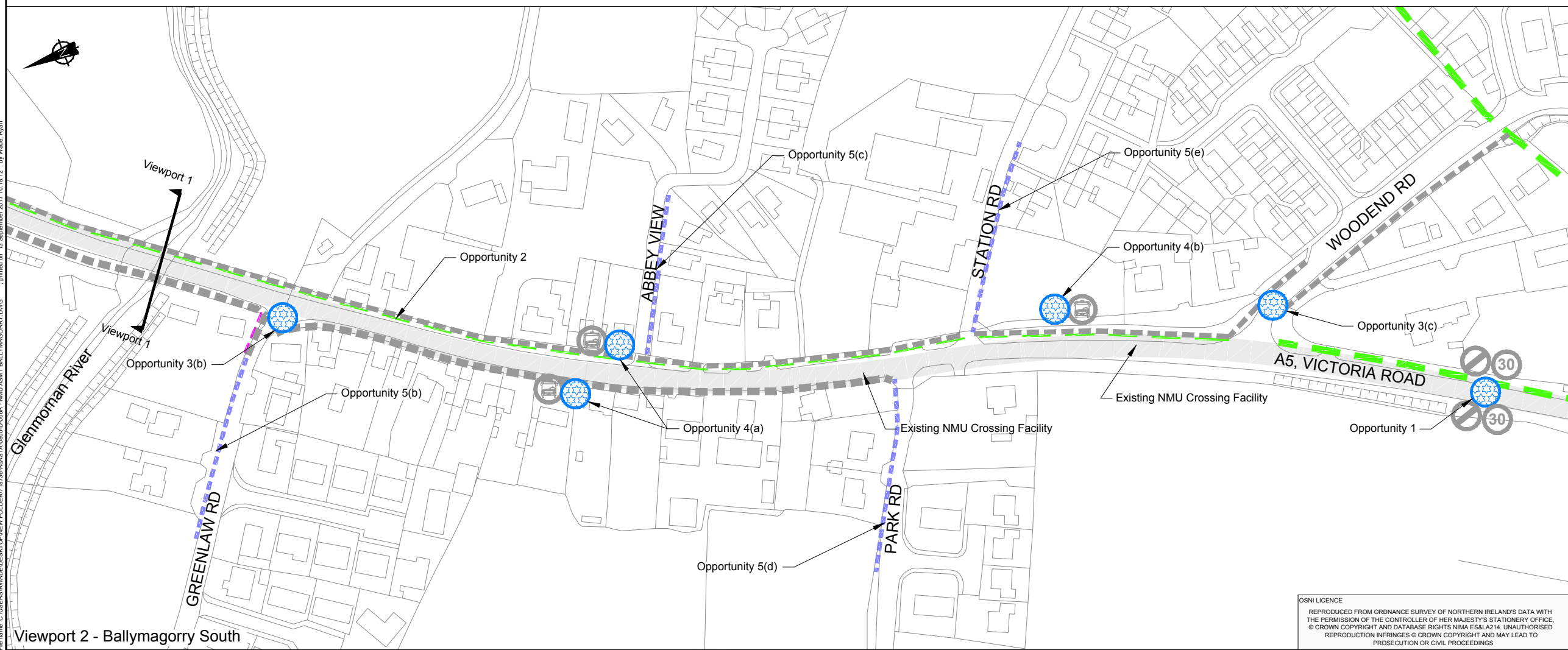
A potential 2m wide NMU facility could be introduced along Station Road. Land acquisition would be required to provide this.

Park and Ride Facilities

During the course of this assessment Translink has been consulted and have not identified Ballymagorry as a potential location for a Park and Ride Facility.



Viewport 1 - Ballymagorry North



Viewport 2 - Ballymagorry South

Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM09 - A5 - Active & Sustainable Transport Assessment: Ballymagorry" which will explain each identified opportunity.

Key

Existing Asset Features

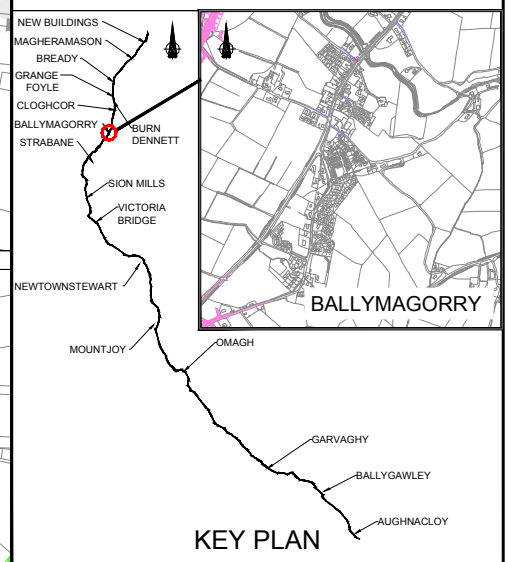
- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme



KEY PLAN

DRAWING STATUS: FOR ISSUE

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STEP/PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy

TITLE: ASSESSMENTS OF URBAN AREAS
 LOCATION: BALLYMAGORRY, CO. TYRONE
 SHEET 1 OF 1

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: OFK	DATE: 14/08/2017
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DM10 – A5 Active & Sustainable Transport Assessment: Strabane

Description

The identification and assessment of potential active & sustainable transport opportunities along the A5 in Strabane, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 – New Buildings to Aughnacloy - Terms of Reference*” document (WSP ref. 1058654/RP/001).

This memo should be read in conjunction with Drawing: 718736-A5ASTA-0800-D-007 to 012 ‘Assessments of Urban Areas, Location: Strabane, Co. Tyrone.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic.

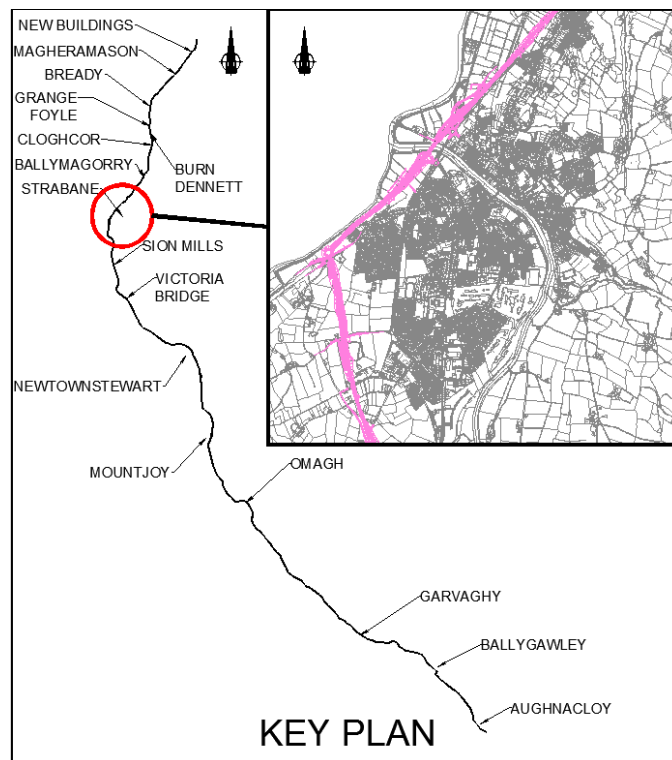


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 through Strabane are of a relatively good standard i.e. an existing footway is present on both sides of the existing A5 through Strabane.

Existing Cycling Facilities / NI Greenway Networks (See Drawing D-009, D-010 and D-011)

Sustrans have identified a “Traffic Free Route not on the National Cycle Network” along the Great Northern Link between the Bradley Way Roundabout and the Melmount Road Roundabout. This facility consists of a shared NMU facility adjacent to the southbound side of the carriageway.

An assessment of these facilities has been carried out as part of this technical note. For the most part the existing facilities appear to be sufficient.

Existing Signal Controlled pedestrian crossing facility (See Drawing D-009)

There is an existing signal controlled pedestrian crossing facility on the northbound approach to the Lifford Road / Railway Street Roundabout. Drop kerbs and tactile paving have already been installed. Crossing distance is approximately 10m. This existing pedestrian crossing links an NMU-only route linking the A38 Lifford Road to Main Street.



Photograph 1: Existing Signal Controlled Pedestrian Crossing (See Drawing D-009)

Existing Footway – A5, Bradley Way (See Drawing D-009)

The existing NMU facilities provided on both sides of the A5 appears to be sufficient between the A5 Barnhill Rd / Branch Rd / A38 Lifford Rd Roundabout and the Bradley Way Roundabout appear to be sufficient.



Photograph 2: A5 Great Northern Link exit off the Bradley Way Roundabout (See Drawing D-009)

Existing Pedestrian Crossing Facilities – Bradley Way Roundabout (See Drawing D-009)

The existing uncontrolled pedestrian crossing facilities provided on each arm of the Bradley Way Roundabout appear to be sufficient.



Photograph 3: A5 Northbound exit off the Bradley Way Roundabout (See Drawing D-009)

Existing Shared NMU Facilities – A5, Great Northern Link (Bradley Way to Urney Rd.) (See Drawing D-009 and D-010)

The existing shared used facilities provided on both sides of the A5 Great Northern Link appear to be sufficient.



Photograph 4: A5 Great Northern Link exit off the Bradley Way Roundabout (See Drawing D-009)



Photograph 5: Northbound on the A5 Great Northern Link (See Drawing D-009)



Photograph 6: Northbound on the A5 Great Northern Link at the junction with Urney Road (See Drawing D-009 and D-010)

Existing Shared NMU Facilities – A5, Great Northern Link (includes Castlegrange Pk., Strahans Rd., Orchard Rd. and Orchard Pk. exits) (See Drawing D-009, D-010 and D-011)

The existing shared NMU facilities provided on both sides of the A5 Great Northern Link appear to be sufficient.



Photograph 7: A5 Great Northern Link south of the junction with Urney Road (See Drawing D-009)



Photograph 8: A5 Great Northern Link south of entrance to Castlegrange Park (See Drawing D-010)



Photograph 9: A5 Great Northern Link south of Strahan Road exit (See Drawing D-011)



Photograph 10: A5 Great Northern Link south of Orchard Park and Orchard Road exits (See Drawing D-011)

Existing uncontrolled pedestrian crossings – Melmount Road Roundabout (See Drawing D-011)

The existing uncontrolled pedestrian crossing around the Melmount Road Roundabout appear to be sufficient.



Photograph 11: A5 Great Northern Link approach to the Melmount Road Roundabout (See Drawing D-011)

Existing NMU Facilities – A5, Melmount Rd. south of the Strabane Business Pk. entrance (See Drawing D-012)

The existing NMU facilities provided on both sides of the A5 Melmount Road appear to be sufficient.



Photograph 12: A5 Melmount Road south of the Strabane Business Park (See Drawing D-012)



Photograph 13: Southbound footway terminus prior to town gateway to the south and Strabane Business Park to the north (See Drawing D-012)

Tactile paving has been provided at the terminus of the southbound footway. There does not appear to be any NMU attractors on this side of the carriageway between the entrance to the Strabane Business Park and the next village of Sion Mills. The existing footway appears to be >2m wide so while it is not of a sufficient width for a standard shared NMU facility, it is likely that pedestrian usage would be quite low. At the terminus of the footway, signage and road markings instructing cyclist to stop and join the carriageway could be provided.

The existing footway on the northbound side of the carriageway continues for circa. 1km and links Strabane to the next urban centre along the A5, the town of Sion Mills.

Existing Parking Facilities

The existing A5 could be described as an internal ring road around Strabane town centre. For the most part, developments off the A5 have been planned residential / commercial development with their own offline parking facilities.

Existing NMU access to Three Rivers Project (See Drawing D-009)

A 2m - 3.5m wide shared use NMU facility exists on both side of the carriageway from the A5 Barnhill Rd. / Branch Rd. / A38 Lifford Rd roundabout to the border at Lifford.

There are 2no. existing uncontrolled pedestrian crossings (with refuge islands).

Drop kerbs and tactile paving have already been installed at points crossing the A38 Lifford Road and at accesses to commercial properties.

Appropriate shared use signage and footway markings have been incorporated along the length of the existing shared use NMU facility.

It should be noted that the A5WTC includes plans to increase the diameter of the A5 Barnhill Rd. / Branch Rd. / A38 Lifford Rd. roundabout resulting in increased sizes of splitter islands on each arm of the roundabout. Should this new roundabout be constructed it may negate any requirement to improve these crossing points.

Existing NMU Crossing Facilities

There are a number of controlled NMU crossing facilities located in Strabane. Their locations are as follows:



Photograph 14: Existing Pedestrian Refuge Island: A5 Barnhill Road at Roundabout Junction, Lifford Road and Railway St. (See Drawing D-008)



Photograph 15: Existing Pedestrian Refuge Island: Bradley Way to roundabout junction with A5 Great Northern Link (See D-009)

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Strabane on the existing A5, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5, potential Greenway facilities that could be introduced, potential improvements to existing bus stop facilities and consideration of the Northern Ireland Road Safety Strategy to 2020.

Proposals include for the provision of a 3m wide shared use NMU facility located along the southbound carriageway between Woodend Road and Derry Road (via the greenway proposal), and adjacent to the northbound carriageway between Derry Road and Lifford Road. Facilities from Lifford Road to Bradley Way Roundabout are deemed suitable. An existing Sustrans traffic free route has also been identified from Bradley Way roundabout to Melmount Road. Proposals have also been included to create a 3m wide shared use NMU facility from Melmount Road Roundabout to Strabane Business Park by widening existing southbound footway and carriageway in order to facilitate.

Opportunity 1 (See Drawing D-007 and D-012): Town Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements; the two images below illustrate the existing gateway features.



Photograph 16: Northbound entrance to Strabane (See Drawing D-007)



Photograph 17: Southbound entrance to Strabane (See Drawing D-012)

Opportunity 2 (See Drawing D-007 to D-011): Shared Use Facilities & Greenways

Opportunity 2(a)

A section of the A5, Victoria Road has been identified in the Northern Ireland Greenway Network Plan for upgrading to accommodate cyclists. The facility would run adjacent to the southbound side of the carriageway of the existing A5 between the junctions with Woodend Road and the Derry Road (circa. 600m).

The additional width required for the new facility could be achieved using one of two approaches. Where possible the existing footway could be extended into the grass verge at the back of the footway.



Photograph 18: Southbound on the A5 Victoria Road north of the entrance to Glenside (See Drawing D-007)

Where there is a boundary wall to the back of footway or insufficient verge width to achieve the required width, the southbound lane could be realigned towards the centre of the existing carriageway. This could be achieved by removing the central hatch, replacing it with a standard centre line and extending the existing footway into the carriageway.



Photograph 19: Southbound on the A5 Victoria Road north of Derry Road (See Drawing D-007)

Where the front of the existing footway is extended into the carriageway, the existing edge of carriageway drainage would need to be redesigned and replaced. An example of this would be between Tulacorr Drive and Derry Road (circa. 320m).

Opportunity 2(b)

A footway has been provide adjacent to the northbound carriageway of the A5, Barnhill Road (between the Sustrans route and the potential Northern Ireland Greenway Network Plan). This section is approximately 1.5km in length and would require either new or improved controlled crossings at junctions with Derry Road, Park Road and the A5 Barnhill Rd / Branch Rd / A38 Lifford Rd Roundabout.

A grassed separation strip exists between the footway and edge of carriageway. LT 1/12 - Shared Use Routes for Pedestrians and Cyclists states that the preferred width for a separation strip is 1.5m but an acceptable minimum of 0.5m is sufficient where the posted speed limit is less than or equal to 40mph.

The posted speed limit for this section of the A5, Barnhill Road is 50mph, with the exception of an approximate. 270m section before the junction with Derry Road where the posted speed limit is 30mph. With the exception of this 30mph section, the preferred 1.5m separation width could be provided.

Along this 30mph section existing street furniture (street lighting columns, road signs) restricts widening of the footway towards the carriageway. To provide the acceptable minimum width for a shared NMU facility the back of footway could be extended away from the carriageway. To achieve a smooth and consistent finish, the existing footway surfacing would need to be replaced (circa. 1.5m x 270m).

There is a drop in level at the back of the existing footway within this 30mph section, so alterations to the existing earthworks (stabilise/extend) and PED would be required. Additional land may be required to achieve this, depending on the solution. (circa. 2m width x 270m length).

The mature hedge/vegetation at the back of the footway would need to be cut back/down to facilitate the works, and certain sections may need to be replaced. A close boarded timber fence (circa. 250m) may be required at the back of the new facility to prevent vegetation encroaching across the path and reduce the costs associate with maintaining the effective width of the facility. The fence would also offer some protection to cyclists from the level drop at the back of the footway.

It is assumed that the existing street lighting would sufficiently illuminate the new facility. However, should the pre-works assessment find that new street lighting columns would be required the above proposal may be altered to reduce the land take required by relocating lighting column to the back of the new facility and reducing the segregation strip to the minimum 0.5m required in LT 1/12.

For the 50mph section, the existing footway could for the most part be widened online to achieve the acceptable minimum width for a shared NMU facility (3m) (circa. 800m). It should be noted that there is a significant level difference between the existing footway and the adjacent grass verge along the 50mph section. The footway appears to be 1m

above the verge, consequently, widening the existing footway towards the carriageway would likely require small earthworks slopes to tie-in to the existing verge.

On the southbound approach to the A5 Barnhill Rd / Branch Rd / A38 Lifford Rd. Roundabout the posted speed limit reduces to 40mph. The existing footway could potentially be widened towards the carriageway. However, it should be noted that the verge within this 40mph section is narrower than within the 50mph section and coupled with the presence of lighting columns in the verge, this may therefore prohibit widening towards the carriageway. Dense vegetation has been planted at the back of the footway to screen the A5 Barnhill Road from the adjacent Branch Road. To provide an acceptable shared NMU facility along this section (circa. 70m), the mature vegetation and trees would need to be partially removed to allow for the footway to be extended away from the A5 carriageway. To ensure adequate screening is maintained between the A5 Barnhill Road and Branch Road, an anti-dazzle fence could be erected along this section and, space permitting, new trees and vegetation could be planted.

If the existing timber post and rail fence is deemed to present a hazard to cyclists travelling on the new facility, it would be taken away as part of the works and relocated at a suitable setback from both the edge of carriageway and the front edge of the shared NMU facility.

Opportunity 2(c)

The shared NMU facility adjacent to the southbound carriageway of the A5 Great Northern Link terminates at Melmound Road Roundabout. The shared NMU facility adjacent to the northbound carriageway ends at the entrance to the Brookvale housing development.



Photograph 20: A5 Melmound Road south of the entrance to the Strabane Business Park (See Drawing D-011)

A shared NMU facility could be provided on either side of the carriageway by removing the central hatch and right turn lanes between the Melmound Road Roundabout and the entrance to the Strabane Business Park (circa. 200m).

The footway adjacent to the southbound carriageway appears to be wider than the footway on the opposite side of the carriageway and would not require street lighting columns to be relocated. For these reasons it is assumed that providing a shared NMU facility on the southbound side of the carriageway would be more cost effective than a similar facility on the opposite side of the carriageway.

To provide a suitable shared NMU facility the existing southbound footway would need to be extended into the existing carriageway by circa. 1m over the section's length. To link this new facility to the existing shared NMU facility at the entrance to Brookvale, the existing pedestrian refuge island would need to be either widened to provide sufficient width for a cyclist to wait on the island whilst crossing the carriageway or removed as the extension of the footway would reduce the effective crossing width.

Opportunity 2(d)

There is an existing footway currently adjacent to the northbound A5 carriageway between Strabane Business Park and Sion Mill

There is also an existing footway adjacent to the southbound carriageway immediately after Strabane Business Park, for a length of approximately 165m. This terminates at an uncontrolled pedestrian crossing.

The assessment of both the northbound and southbound carriageway concluded with the recommendation that the proposed 3m wide shared use NMU facility to be constructed adjacent to the southbound carriageway.

There are numerous attractors on the northbound side of carriageway with fewer constraints on the southbound side, this was considered the better option. This route would also tie in with the proposals identified for Sion Mills along the southbound carriageway.

Opportunity 3 (See Drawings D-007 to D-012): Pedestrian Crossing Facilities

Opportunity 3(a)

Uncontrolled crossings have been provided on the Branch Road and A38 Lifford Road arms of the roundabout. The splitter/refuge island on the A38 Lifford Road arm of the roundabout would need to be replaced with an island of sufficient width for a cyclist to wait on the island without their bike protruding onto the carriageway.

Opportunity 3(b)

The uncontrolled pedestrian crossings, north and south of the Derry Road exit of the A5 could be replaced with controlled pedestrian crossings (e.g. Zebra Crossings). A controlled system could be used by NMUs travelling to and from the A5 Derry Road / A5 Barnhill Road to the various NMU attractors on Derry Road. Attractors on Derry Road include schools (Strabane High School, Strabane Controlled Primary School), Derry City and Strabane District Council Offices and the North West Regional College.

A controlled facility would also allow for cyclists travelling on the proposed Greenway adjacent to the southbound carriageway to cross the carriageway and continue their journey on the proposed facility adjacent to the northbound carriageway of the A5, Barnhill Road.

Opportunity 3(c)

A controlled pedestrian crossing (Puffin or Zebra) could be considered to reduce speeds outside the school and cater for school children crossing the road to and from the new bus stop. This opportunity is to be provided in line with Opportunity 7(b).

Opportunity 4 (See Drawing D-007): Footway Improvements

Opportunity 4(a)

The footway adjacent to the northbound carriageway, prior to the entrance to Tulacorr Drive, is poorly defined and provides limited protection to pedestrians. A new kerb line with 100mm upstand, with transition kerbs for dropped kerb accesses and a new offline bus stop and shelter should be provided to improve the edge of carriageway definition, provide improved bus facilities in the area and discourage road users from parking in such a way as to force NMUs onto the carriageway. It is likely that the existing drainage system along this length of carriageway would also have to be redesigned and replaced (circa. 140m).



Photograph 21: A5 Victoria Road south of Tulacorr Drive exit (See Drawing D-007)

Opportunity 4(b)

Provide tactile paving at the dropped kerb crossing points across the entrance to the Strabane Business Park.



Photograph 22: Entrance to Strabane Business Park (See Drawing D-011 and 012)

Opportunity 5 (See Drawing D-007): Northern Ireland Road Safety Strategy to 2020

The Northern Ireland Road Safety Strategy to 2020 identifies a number of measures that could be implemented to make work safer, one of which likely “...considering the applicability of urban speed reduction initiatives and assessing the potential for wider introduction of 20mph limits in residential areas and other urban areas where there is a significant presence of vulnerable road users.”

The current posted speed limit along Derry Road is 30mph.

Due to the proliferation of NMU attractors on Derry Road, as outlined in Opportunity 6, it may be appropriate to implement a 20mph speed limit on Derry Road.

Both exits off the A5, Derry Road could be revised to include gateways to inform road users that they are entering an area where there is a significant presence of vulnerable road users.

Image on following page:



Photograph 23: A5 Derry Road north junction with Derry Road (See Drawing D-007)

Opportunity 6: Park and Ride Facilities

During the course of this assessment Translink have been consulted and have advised the following:

Strabane Bus Station... *“Presently there are 42 spaces at the bus depot. Translink do not own or control any of the adjoining lands, so expansion of the existing P & R facility would have to be by a ‘piggy-back’ lease arrangement using the neighbouring car parks for Lidl, KFC, etc. which is not that desirable. I also understand the un-developed lands behind the bus garage have planning permission granted for housing which in addition to the problems of operating a split site probably makes it prohibitively expensive to acquire. Given the fact that the existing site is close to capacity and access is likely to be compromised by the proposed slip road layout at junctions 5 & 6 (of the proposed A5WTC) on either side of the Mourne River, a better option might well be for a replacement facility...”*

Translink have indicated that in Strabane it would be preferable to provide a *“...replacement of existing P&R facility at bus station, desirable relocation would be between Junctions 5 and 6 of the proposed A5WTC.”*

Translink have indicated that they will carry out their own assessment to identify desirable locations for a new park and ride facility in Strabane.

Opportunity 7 (See Drawings D-007 to D-011): Bus Stop Facilities

Opportunity 7(a)

There does not appear to be an opportunity to develop an offline bus stop / shelter at the location of the existing stop.



Photograph 24: A5 Victoria Road south of Tulacorr Drive (See Drawing D-007)

There is sufficient room for a standard offline bus stop, with 3m wide shared NMU facility running behind it, to be provided in the grassed area at the back of the southbound footway between the Woodend Road junction with the A5 and the entrance to the Glenside residential development approximately 140m north of its current location.



Photograph 25: A5 Victoria Road immediately north of the junction with Woodend Road (See Drawing D-007)

It is likely that there are a number of different utilities in the existing footway which would have to be diverted as part of any works and that the existing trees and landscaping

would have to be protected. The shared NMU facility would tie-into the facility described in Opportunity 2(a) and would pass between any new shelter and the existing private boundary / hedgerow.

Opportunity 7(b)

There is an bus stop arrangement in operation for the Strabane High School / Strabane Controlled Primary School. An offline area has been provided prior to the entrance to the school on the southbound side of the carriageway.

This arrangement could be formalised to create an offline bus stop and shelter. This would involve resurfacing works (to create a red bus bay) and road markings. The shelter would be set back from the back of the existing footway into the grass bank. To do this a small retaining wall may be required (circa. 8m (L) x 1.5m (H)).



Photograph 26: Potential bus stop location outside Strabane Academy / Strabane High School / Controlled Primary School (Southbound) (See Drawing D-007)

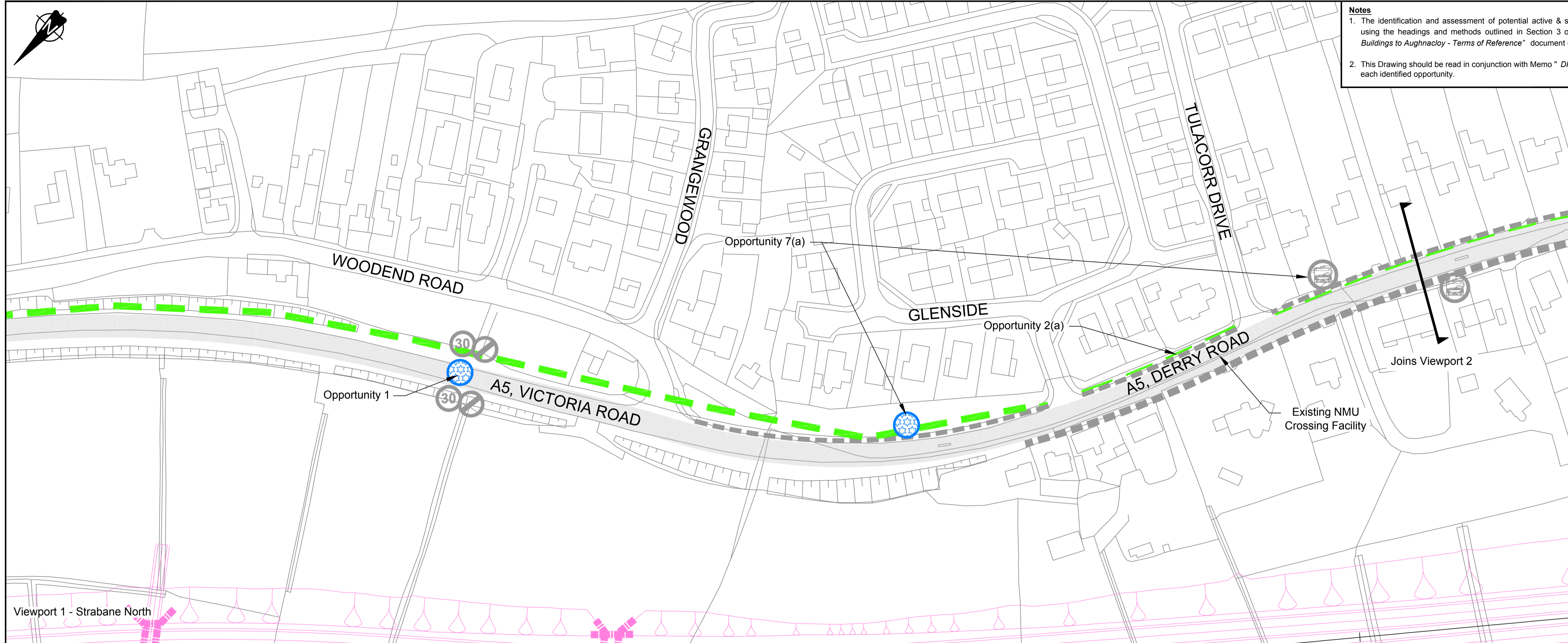
An offline bus stop could be provided on the opposite side of the carriageway to the entrance to the school. By removing the central hatch, realigning and defining the edge of the footway with 100mm upstand kerbs (and easy access kerbing where required), resurfacing (red) and marking an offline bay a formal bus stop could be provided. A bus shelter could be erected in the grass verge south of the advanced direction sign. Care should be taken ensure that existing private access are not obscured by the bus shelter / a stationary bus and that 25mm upstand with transitional kerbs are provided at each access.

Opportunity 7(c)

There does not appear to be a more suitable bus stop location in the vicinity of the existing stop. An offline bus stop could not be provided along this section of carriageway without impacting on a number of private gardens. This approach would likely be quite costly and difficult to justify without establishing a clear need for such a facility.



Photograph 27: A5 Melmount Road north of Strabane Business Park (See Drawing D-011)



Notes

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2. This Drawing should be read in conjunction with Memo "DM09 - A5 - Active & Sustainable Transport Assessment: Strabane" which will explain each identified opportunity.

Key

Existing Asset Features

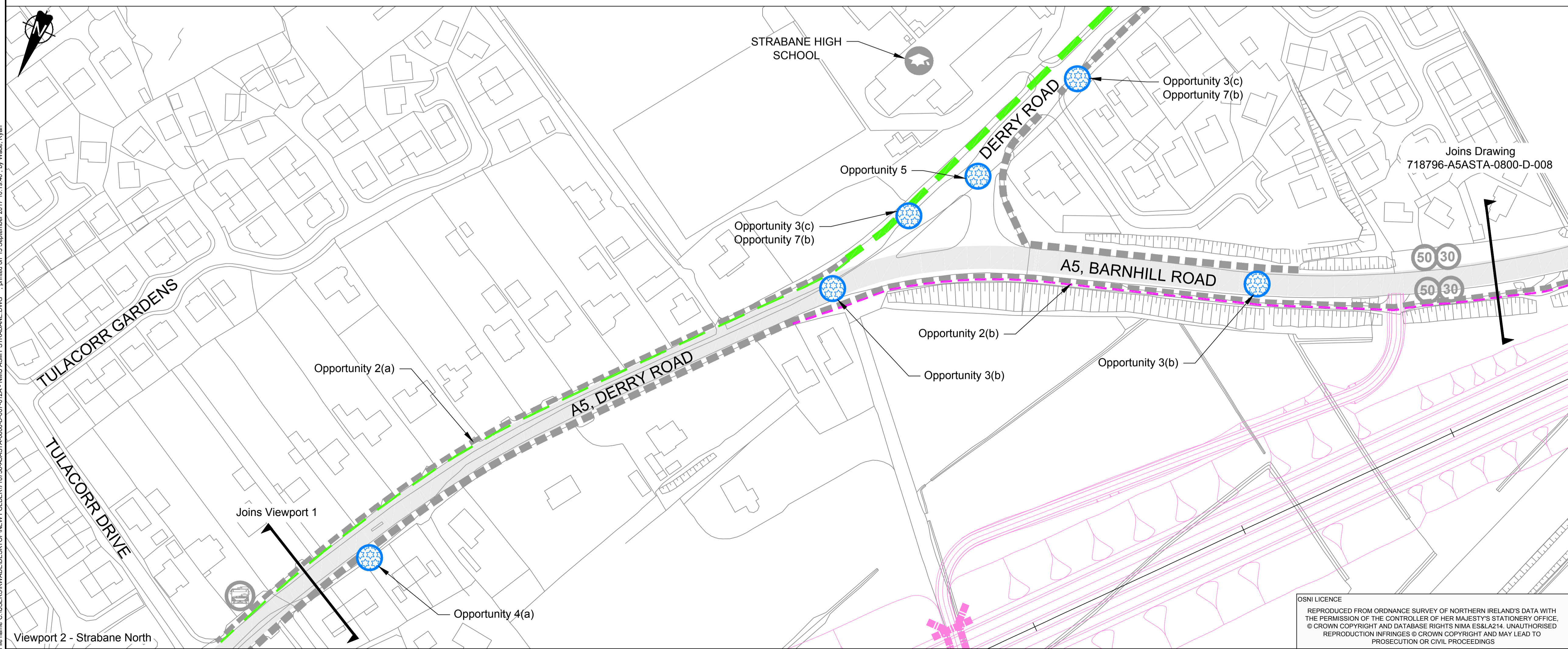
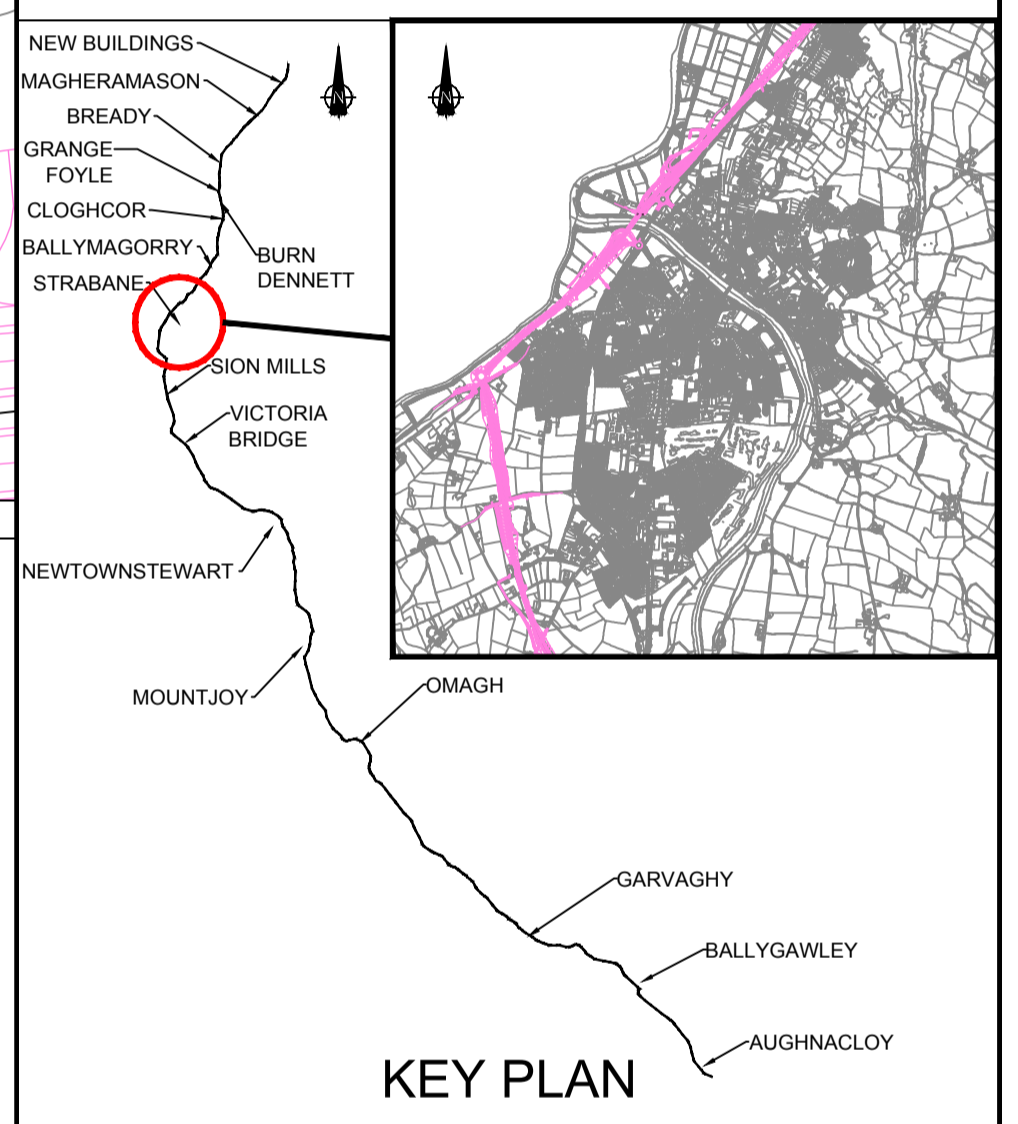
- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme



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SITE/PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy

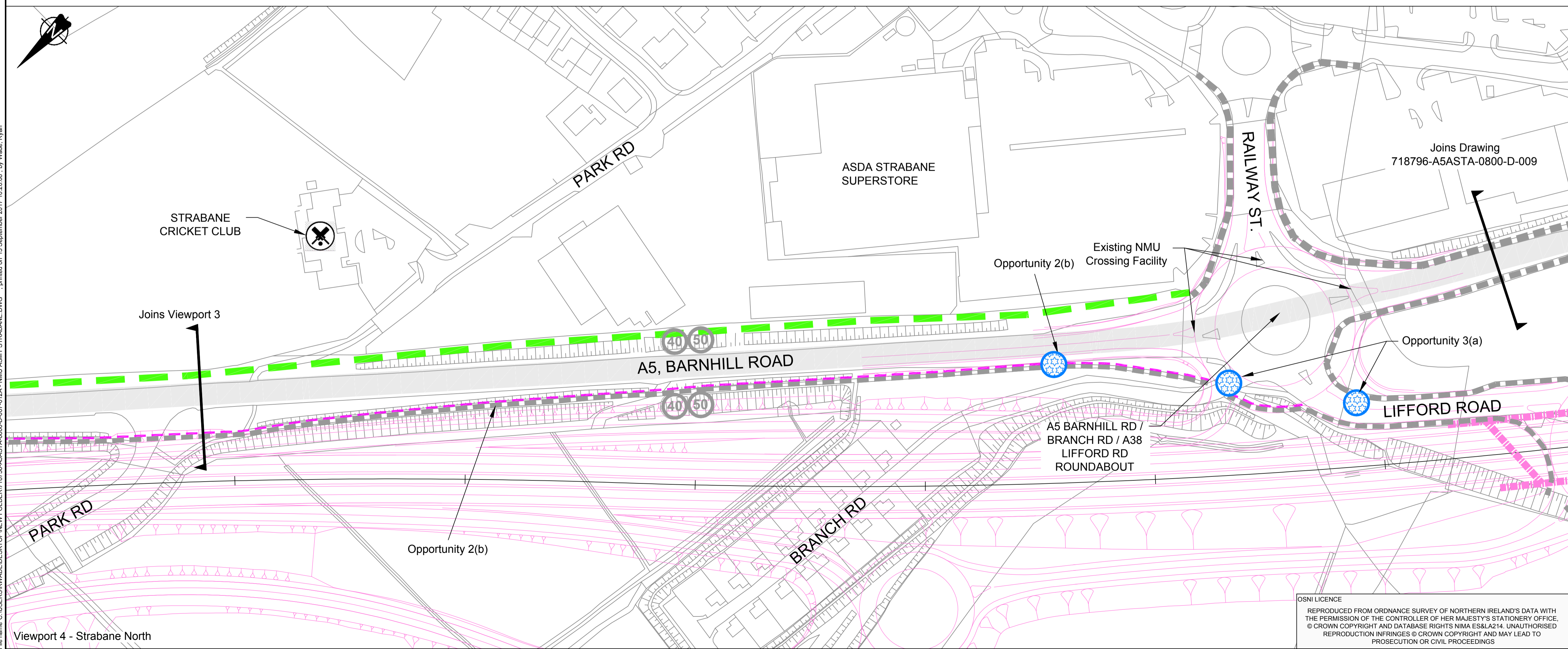
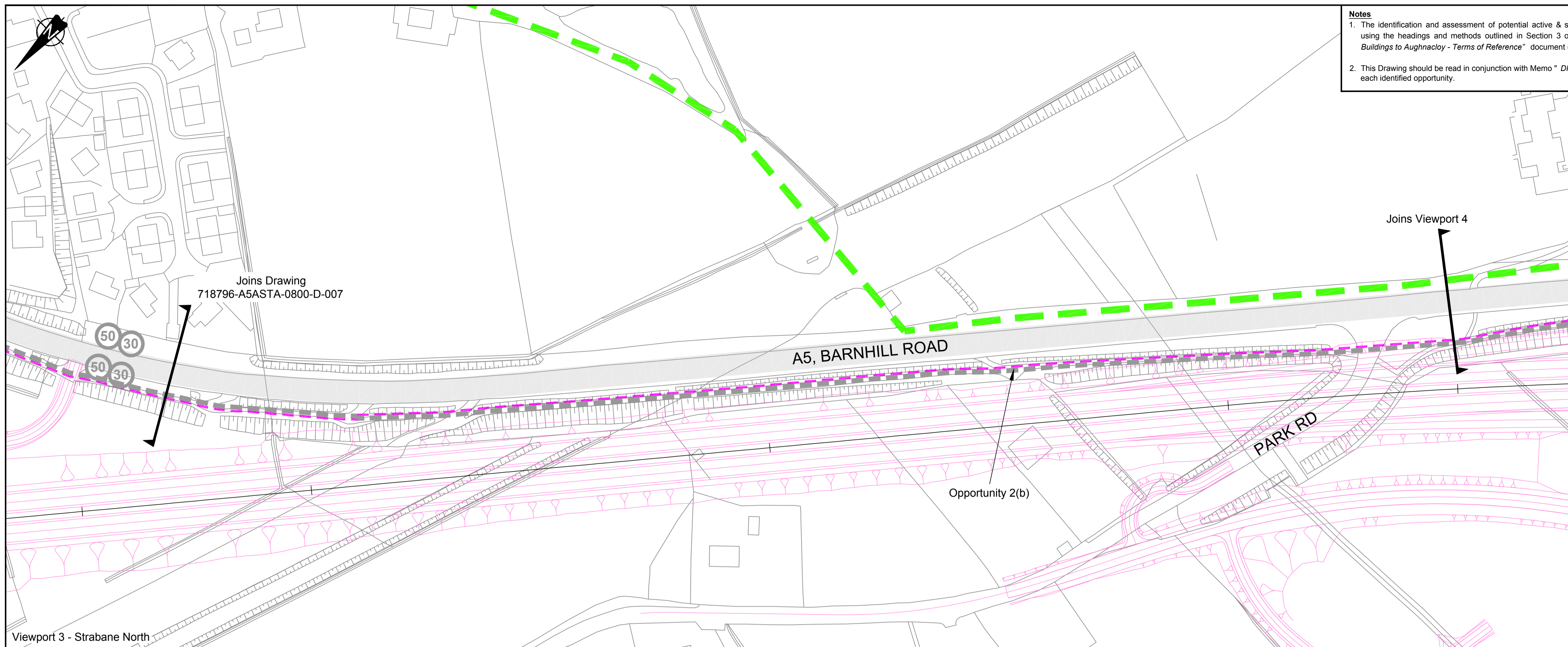
TITLE: ASSESSMENTS OF URBAN AREAS LOCATION: STRABANE, CO. TYRONE SHEET 1 OF 6

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DRAWING NO: 718736-A5ASTA-0800-D-007	REV: A	

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 2. This Drawing should be read in conjunction with Memo "DM10 - A5 - Active & Sustainable Transport Assessment: Strabane" which will explain each identified opportunity.

Key

Existing Asset Features

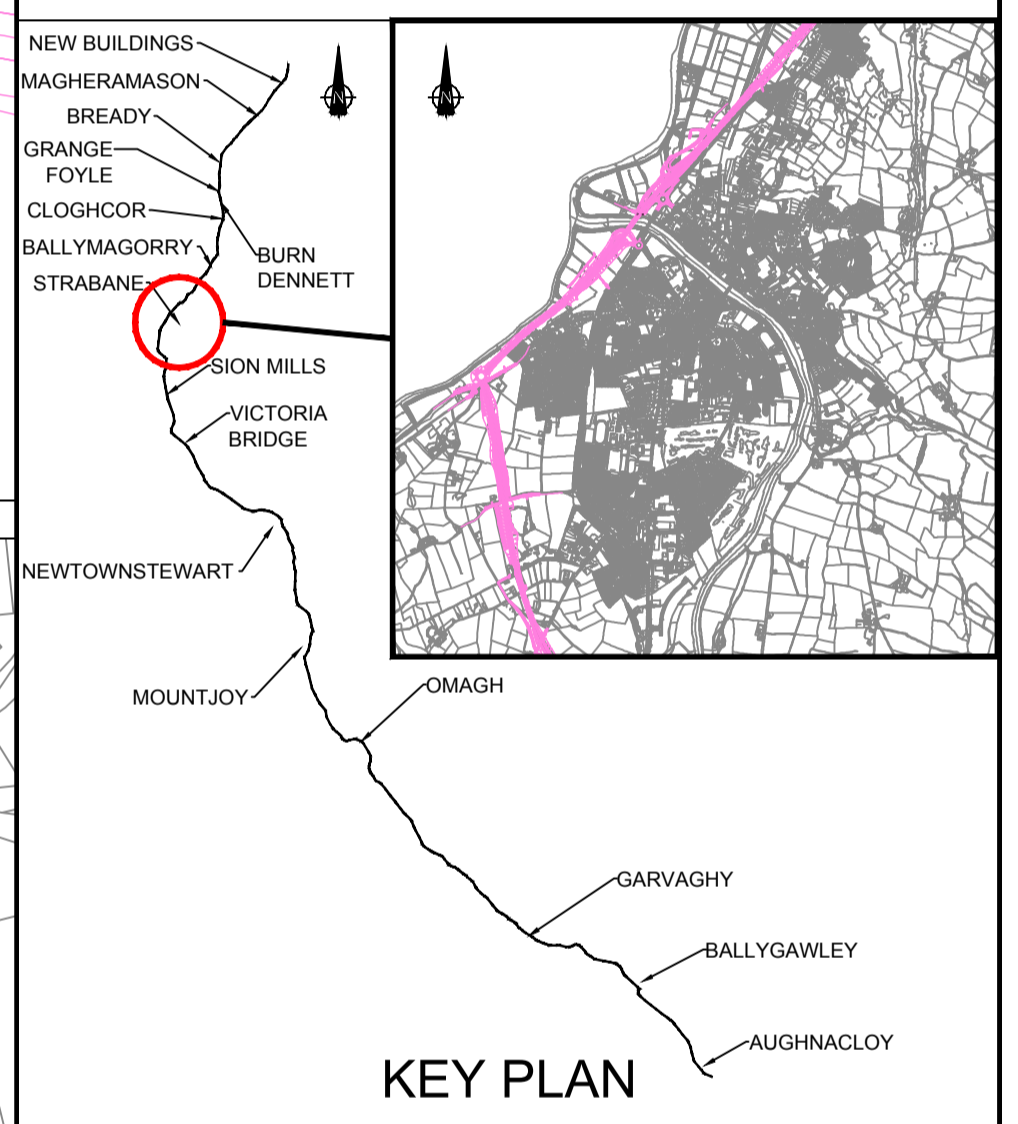
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- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme



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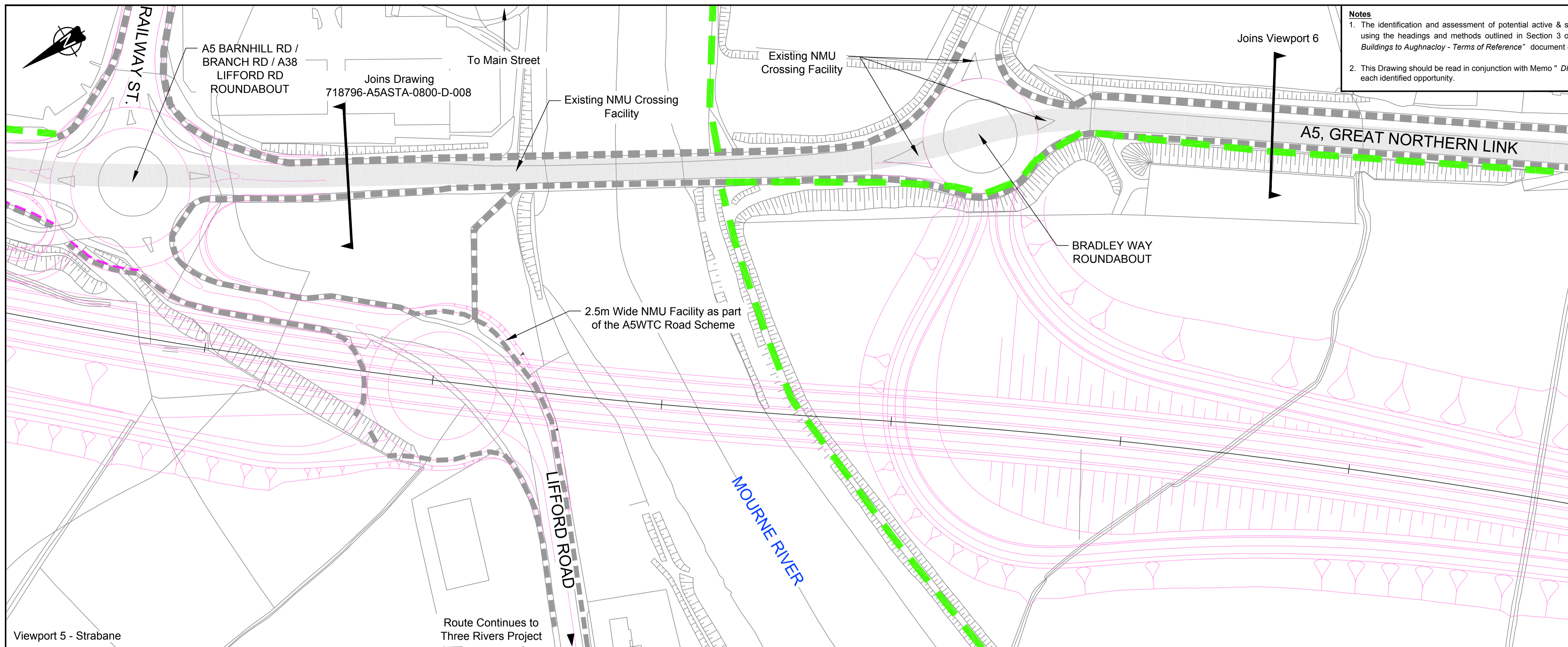
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TITLE: ASSESSMENTS OF URBAN AREAS LOCATION: STRABANE, CO. TYRONE SHEET 2 OF 6

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Key

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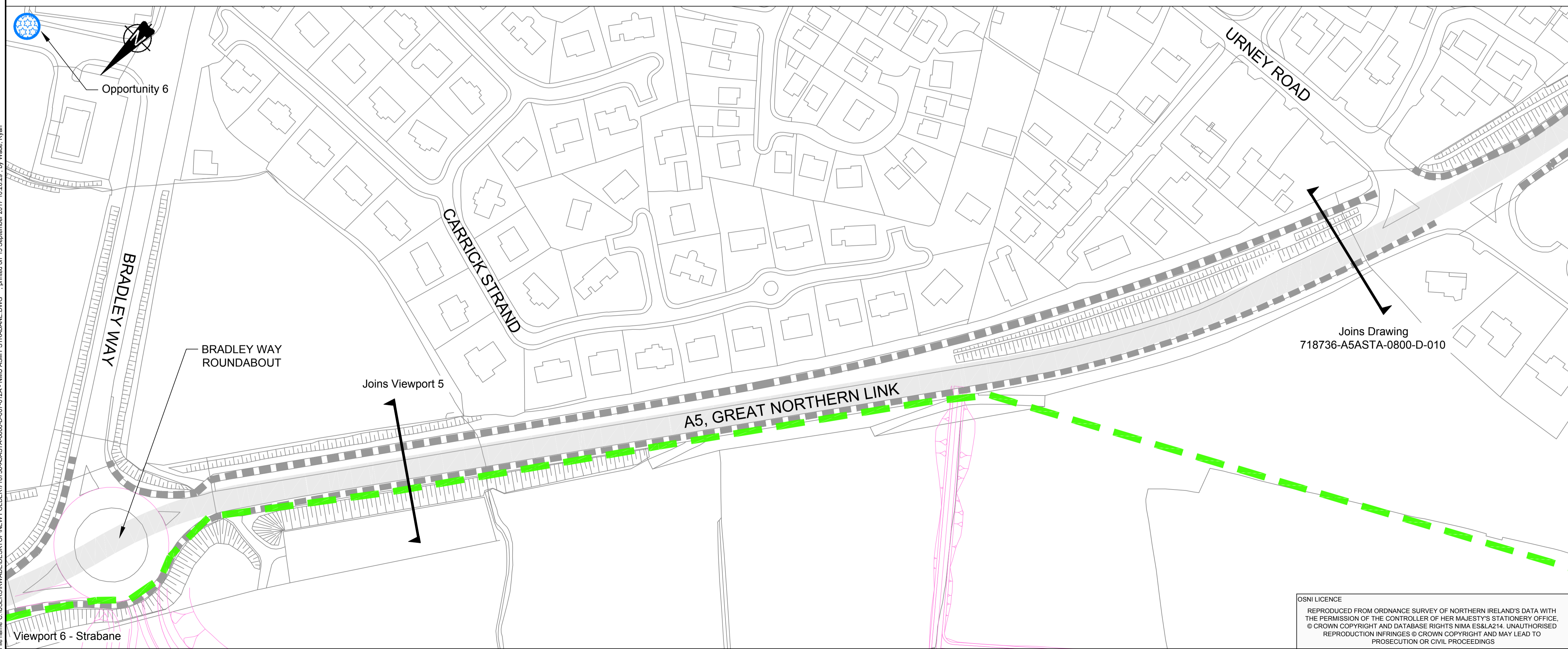
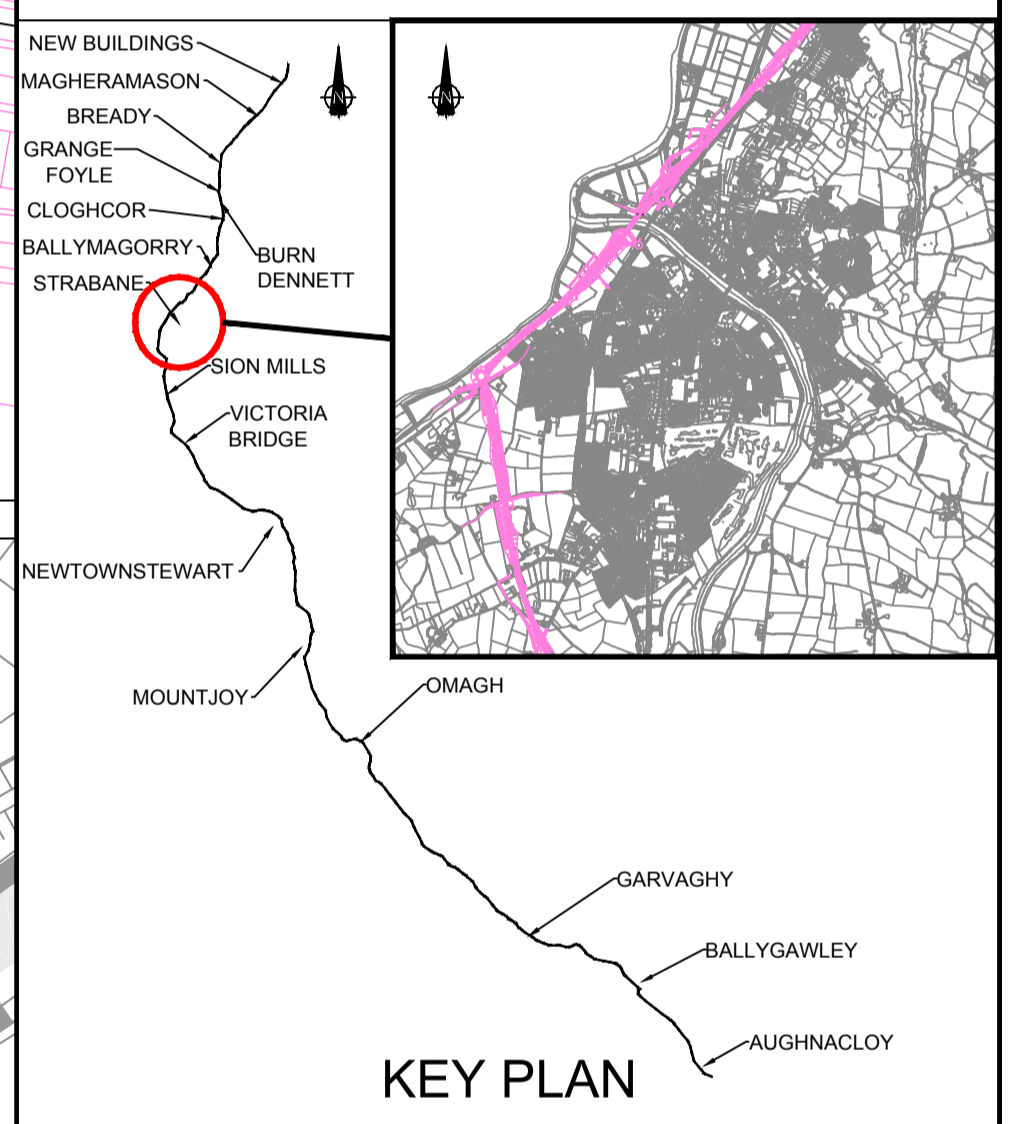
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Proposed Improvements

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- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

Proposed A5WTC Scheme



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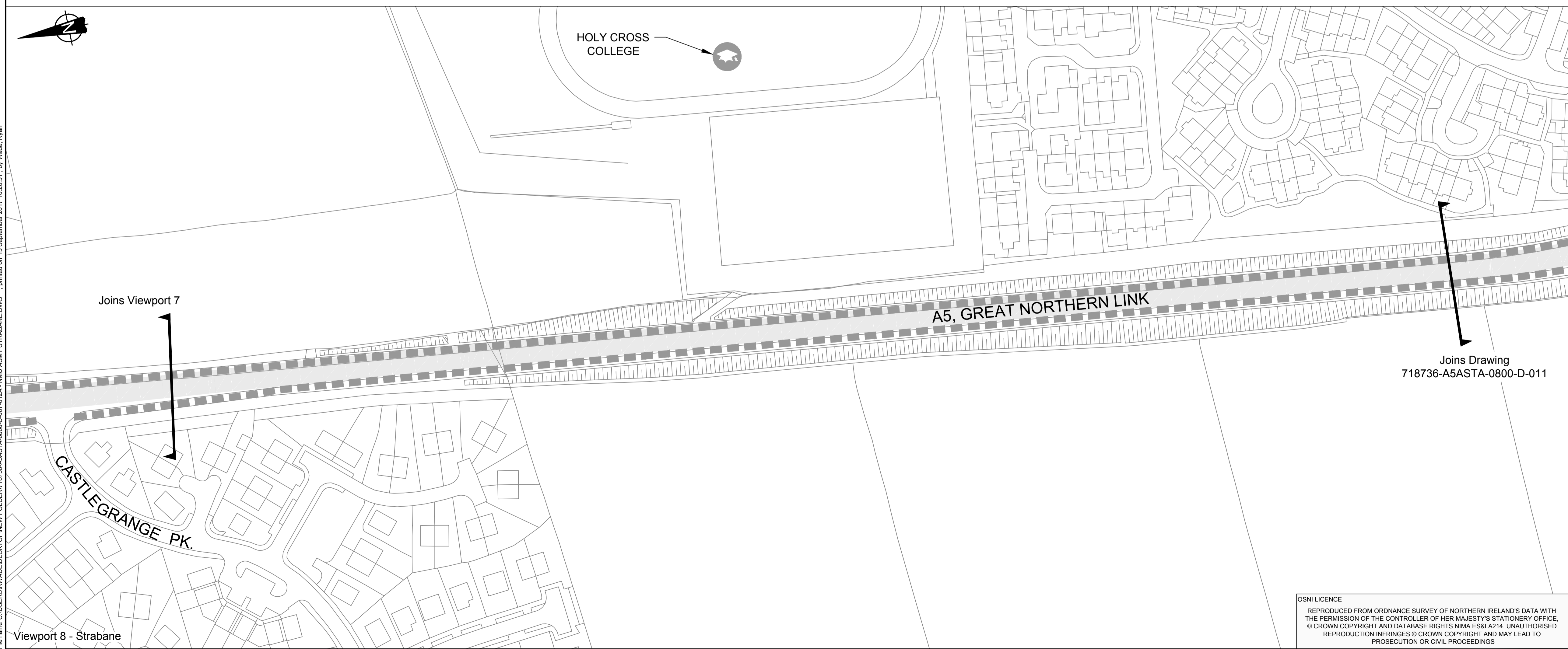
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Key

Existing Asset Features

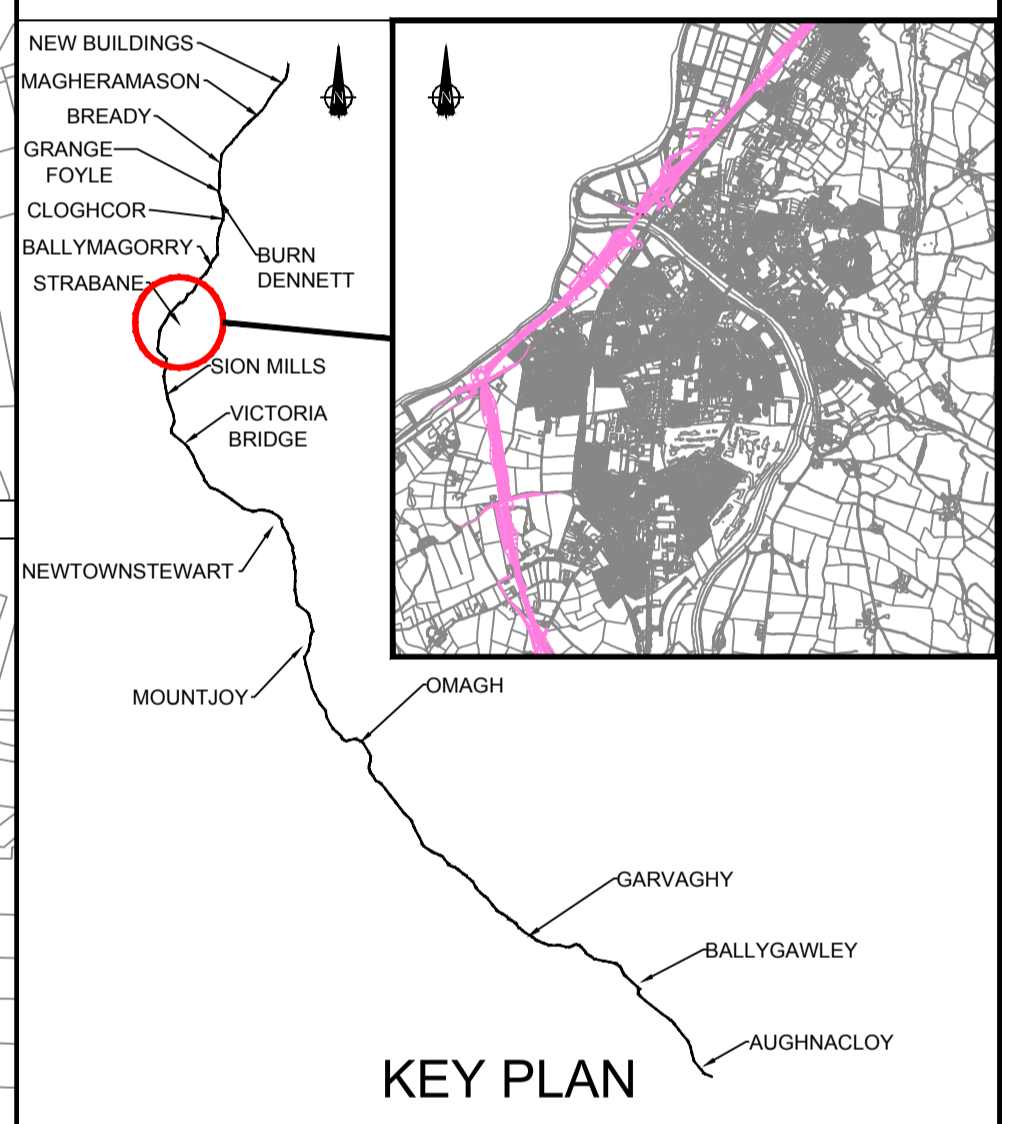
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- Signage: [Symbol] 50 [Symbol] 40 [Symbol] 30
- School: [Symbol]
- Residential Care Homes: [Symbol] RH
- Bus Stop: [Symbol]
- Sports / Cricket Club: [Symbol]
- Church: [Symbol]
- Health Care: [Symbol] HC

Proposed Improvements

- 2m Wide NMU Facility: [Symbol]
- 3m Wide (Shared Use) NMU Facility: [Symbol]
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- Existing NMU Facility to be Widened (via Potential Greenway): [Symbol]
- Potential Greenway Route: [Symbol]
- Highway Realignment with new Shared Use NMU Facility: [Symbol]
- Opportunity Described in Memo: [Symbol]

Other Features

- Proposed A5WTC Scheme: [Symbol]



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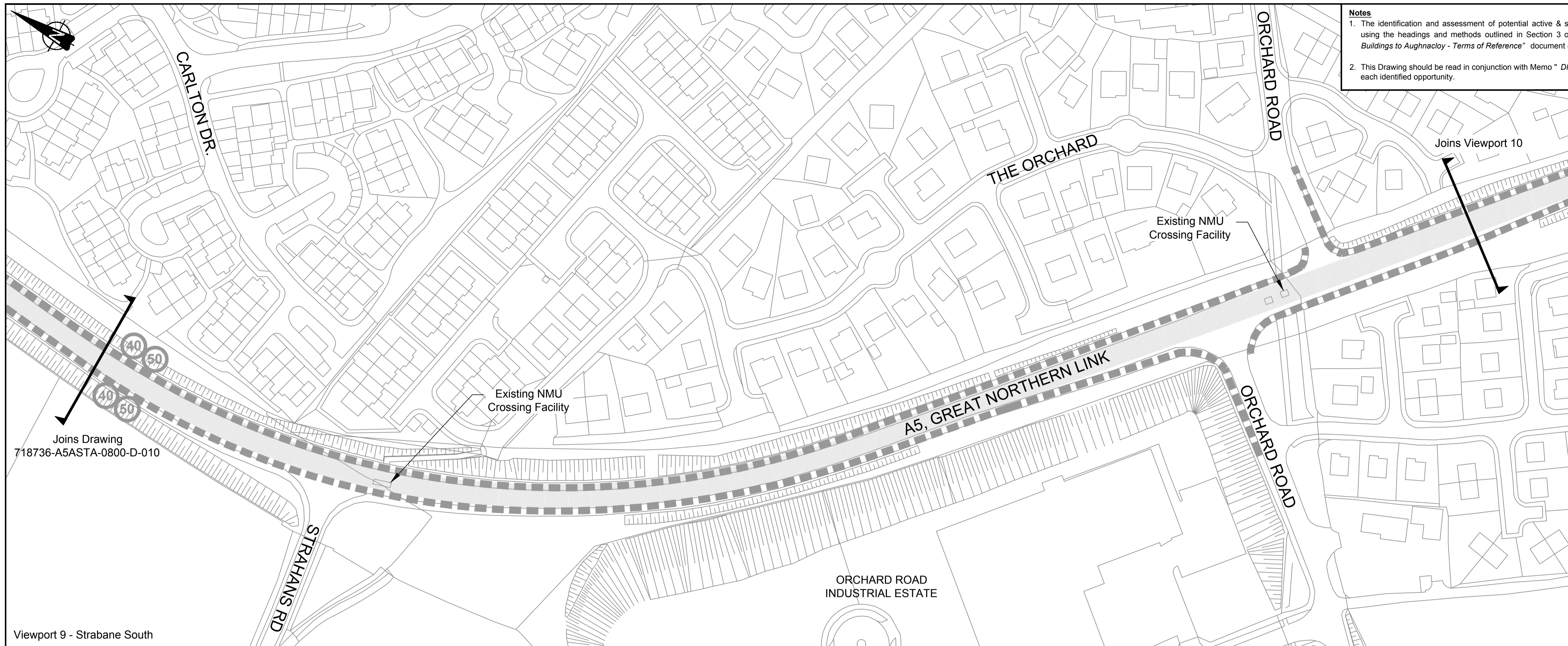
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PROJECT NO: 718736	DESIGNED: DM	DATE: 14/08/2017

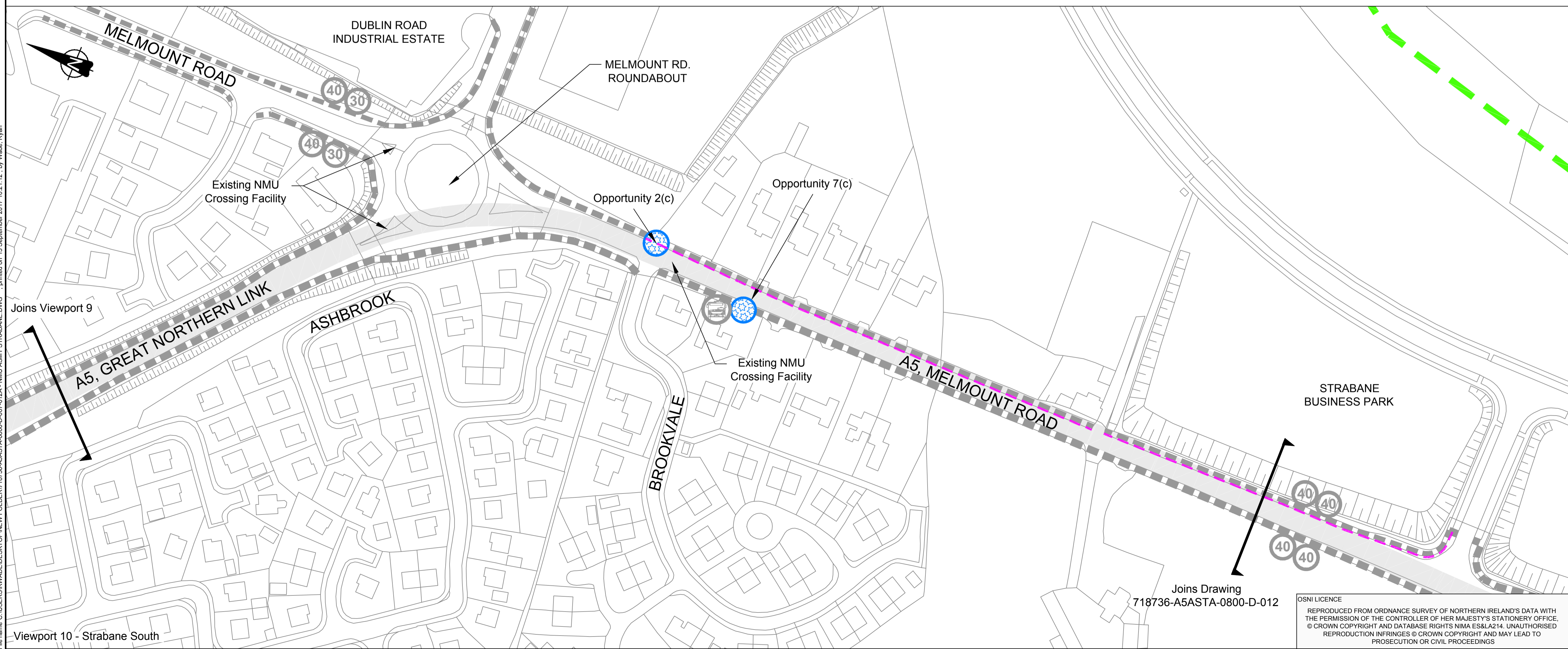
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Viewport 9 - Strabane South



Viewport 10 - Strabane South

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Key

Existing Asset Features

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- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

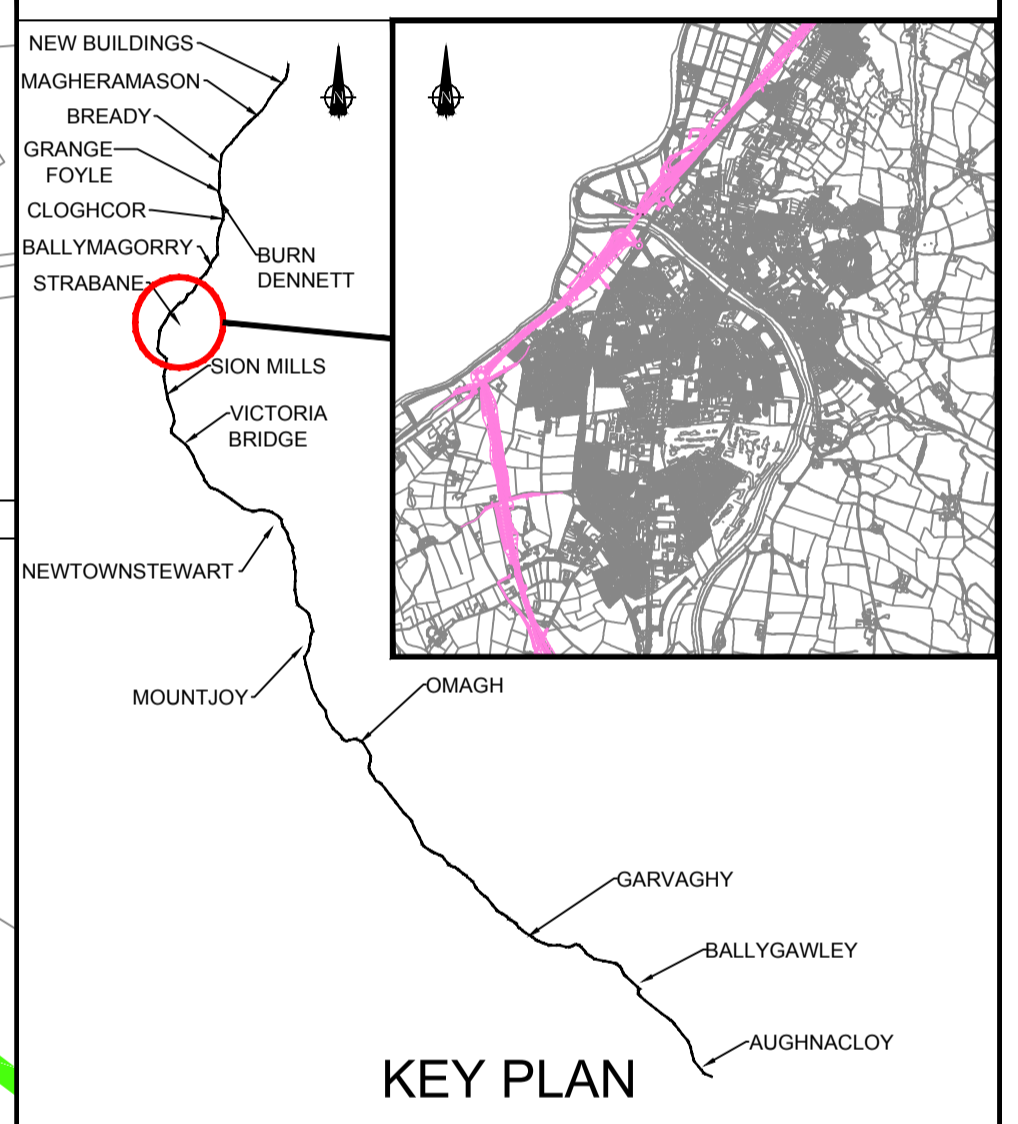
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- Existing NMU Facility to be Widened
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- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility

Opportunity Described in Memo

Other Features

Proposed A5WTC Scheme



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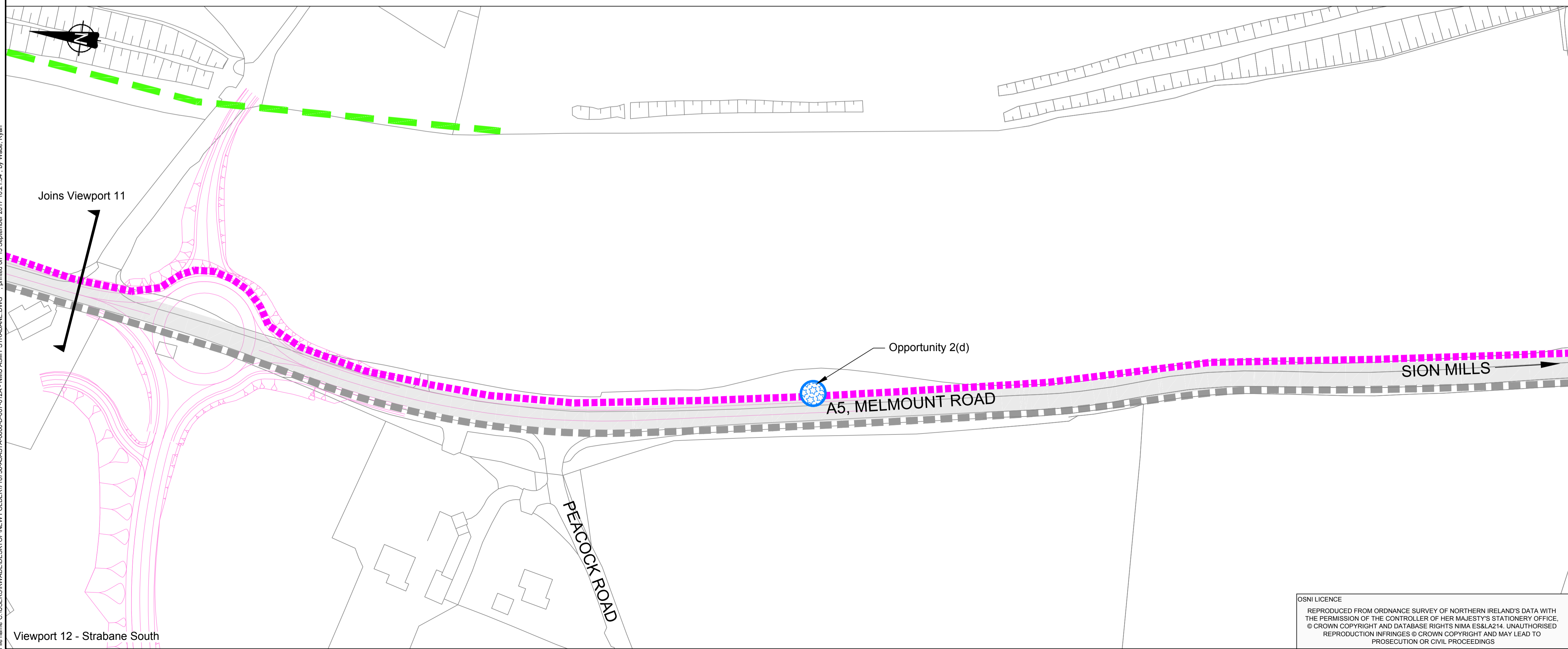
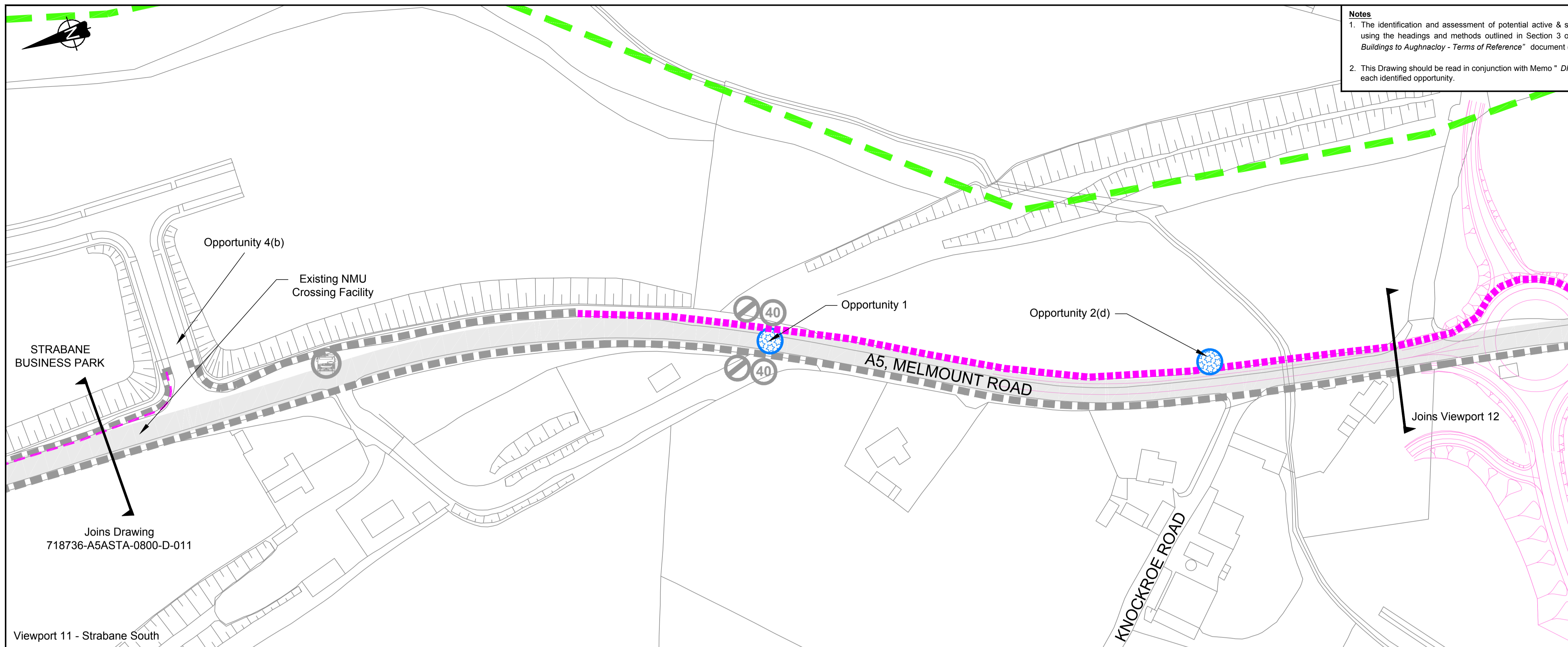
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TITLE: **ASSESSMENTS OF URBAN AREAS LOCATION: STRABANE, CO. TYRONE SHEET 5 OF 6**

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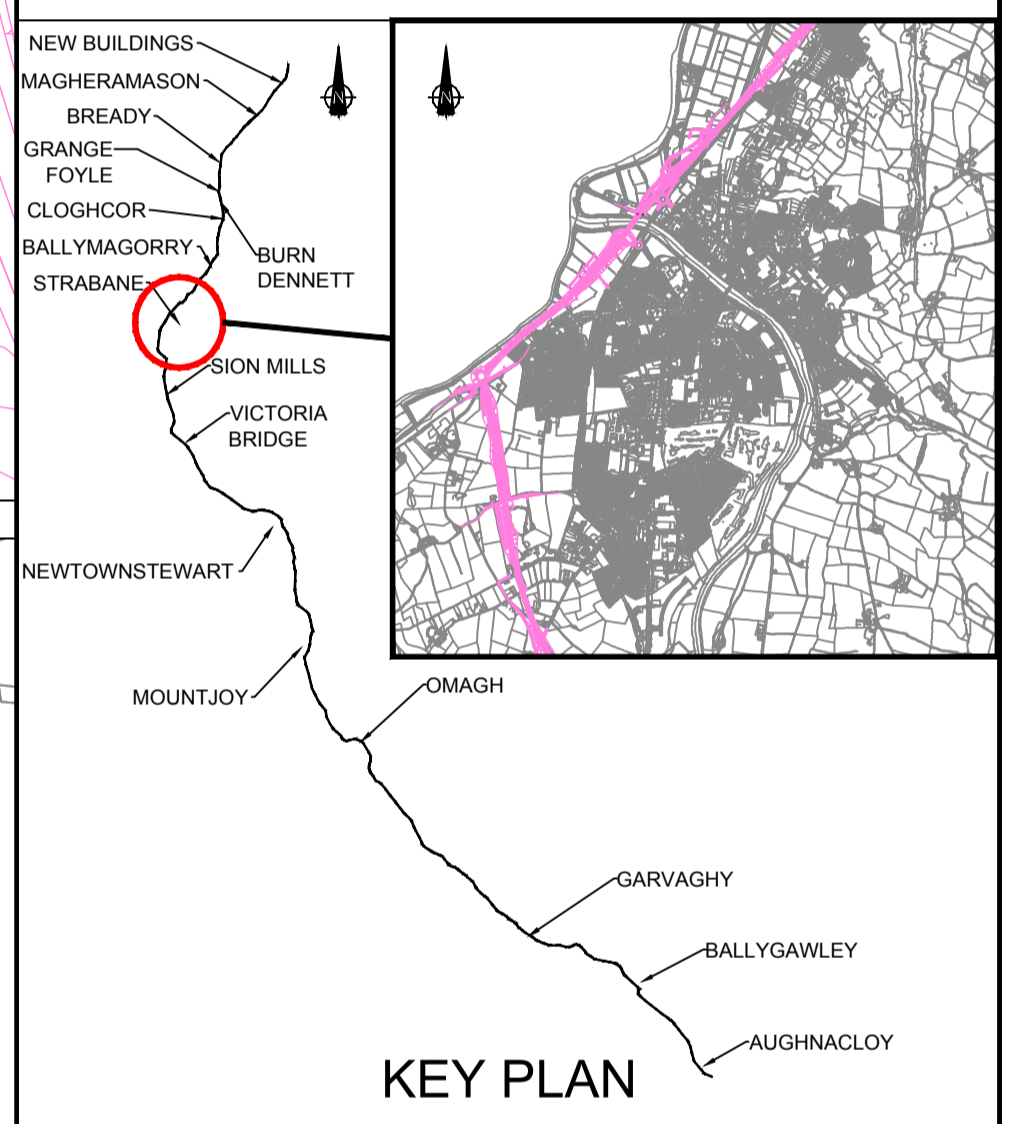
NMU Facility	■ ■ ■ ■ ■ ■ ■ ■	Bus Stop	
Signage		Sports / Cricket Club	
School		Church	
Residential Care Homes		Health Care	

Proposed Improvements

2m Wide NMU Facility	
3m Wide (Shared Use) NMU Facility	
Existing NMU Facility to be Widened	
Existing NMU Facility to be Widened (via Potential Greenway)	
Potential Greenway Route	
Highway Realignment with new Shared Use NMU Facility	
Opportunity Described in Memo	

Other Features

Proposed A5WTC Scheme



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TITLE: **ASSESSMENTS OF URBAN AREAS**
 LOCATION: STRABANE, CO. TYRONE
 SHEET 6 OF 6

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: DM	DATE: 14/08/2017

DRAWING NO: **718736-A5ASTA-0800-D-012** REV: **A**

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DM11 – A5 Active & Sustainable Transport Assessment: Sion Mills

Description

The identification and assessment of potential active & sustainable transport opportunities in Sion Mills, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 – New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo should be read in conjunction with Drawings: 718736-A5ASTA-0800-D-013 to 014 ‘Assessment of Urban Areas, Location: Sion Mills, Co. Tyrone’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic on the A5.

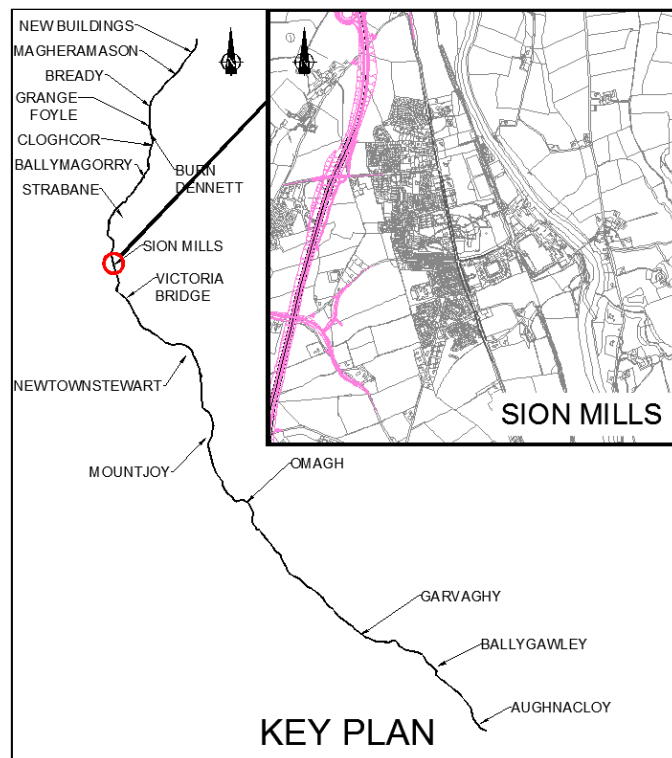


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 through Sion Mills are of a relatively good standard i.e. an existing footway is present on both sides of the existing A5 through Sion Mills for the majority of its length.

Existing NMU Crossing Facilities

There are three existing pedestrian crossings in Sion Mills, one to the south of Millhaven, at the northern end of the town. The second is to the south of the Linen Green, at the southern end of the town. The third crossing is adjacent to Dairy Park in the approximate centre of the town.

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Sion Mills on the existing A5, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5 and potential improvements to existing bus stop facilities on the existing A5.

Proposals include the provision of a new 2m wide NMU facility adjacent to the southbound carriageway over a length of circa 300m, it is likely that in order to provide this facility land acquisition may be required.

Opportunity 1 (See Drawings D-013 and D-014): Village Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds. This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flower beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements.

Opportunity 2 (See Drawing D-013): Pedestrian Crossing Facilities
Opportunity 2(a)

Provide an uncontrolled pedestrian crossing across the junction of Millhaven and the A5, Victoria Road. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 8.0m.



Photograph 1: Uncontrolled Pedestrian Crossing; Millhaven junction with existing A5 (See Drawing D-013)

Opportunity 2(b)

Provide an uncontrolled pedestrian crossing across the junction of The Hawthorn and the A5, Victoria Road. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 5.0m.



Photograph 2: Uncontrolled Pedestrian Crossing; The Hawthorn junction with existing A5 (See Drawing D-013)

Opportunity 2(c)

Provide an uncontrolled pedestrian crossing across the junction of Daisy Park and the A5, Victoria Road. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 8.5m. Two churches are in close proximity to this junction, with footways provided in both the northbound and southbound directions on the approach to each.



*Photograph 3: Uncontrolled Pedestrian Crossing; Daisy Park junction with existing A5
(See Drawing D-013)*

Opportunity 2(d)

As the closest crossing point to these 2no. churches and NMU-only link to the greenway is approximately 270m south, a new controlled crossing could be installed at a location between the churches. The crossing should be located close to Daisy Park junction, as there are NMU-attractors along this street, including a cemetery and sports grounds.

Opportunity 2(e)

Provide an uncontrolled pedestrian crossing across the junction of Church Square and the A5, Victoria Road. Drop kerbs and tactile paving could be installed along the footway's desire line. The crossing distance is approximately 9.3m. Shops, churches and other NMU attractors are in close proximity to this junction.



Photograph 4: Uncontrolled Pedestrian Crossing; Church Square junction with existing A5 (See Drawing D-013)

Opportunity 2(f)

A 'Local Cycle Route' has been identified in Sion Mills, intersecting the existing A5 at Main Street and Willows (vehicular access). However, no formal cycling infrastructure is provided. Upgrading the existing pedestrian crossing to a Toucan Crossing would provide a crossing point for cyclists to access proposed infrastructure.

Opportunity 2(g)

Provide an uncontrolled pedestrian crossing across the junction of Willows and the A5, Victoria Road. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 15m – so a refuge island may be required.

Opportunity 2(h)

Provide an uncontrolled pedestrian crossing across the junction of Main Street and the A5, Victoria Road. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 10m.

Opportunity 2(i)

Provide an uncontrolled pedestrian crossing across the junction of Albert Place and the A5, Victoria Road. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 8.0m. Albert Place is an access route to Sion Mills Primary School

Opportunity 2(j)

Provide an uncontrolled pedestrian crossing across the junction of New Street and the A5, Victoria Road. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 7.5m. New Street is an access route to Sion Mills Primary School.



*Photograph 5: Uncontrolled Pedestrian Crossing; New Street junction with existing A5
(See Drawing D-013)*

Opportunity 2(k)

Provide an uncontrolled pedestrian crossing across the junction of Seein Road and the A5, Victoria Road. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 7.5m. Seein Road is an access route to Sion Mills Primary School.

An additional pedestrian controlled crossing will not likely be required in the area, as the majority of dwellings are located on the 'school side' of the A5. Crossing the A5 is largely not required in this area.

Opportunity 3 (See Drawing D-013): Bus Stop Facilities

Opportunity 3(a)

The addition of a bus stop shelter (northbound and southbound) could be installed in the respective verges at the Millhaven bus stops. There already is an uncontrolled pedestrian crossing point (with drop kerbs and tactile paving) which serves these bus stops.



Photograph 6: Addition of Bus Stop shelters (Northbound and Southbound) (See Drawing D-013)

Opportunity 3(b)

The addition of a bus stop shelter (southbound) could be installed in the verge at the bus stop south of Primrose Park.

Opportunity 4 (See Drawing D-013): Footway Improvements

As the footway adjacent the southbound carriageway terminates after the bus stop described in Opportunity 3(a), a 2.0m wide footway could be incorporated along this stretch. The footway's length would be approximately 290m. The footway could potentially be located in the A5 verge in some areas, however vesting would potentially be required – approximately areas of 30m x 2m and 120m x 2.5m. Also, 6no. street lights would potentially need relocated to allow 2m of unobstructed width.

Opportunity 5 (See Drawing D-013): NMU Routes & Greenways

Opportunity 5(a)

Beside the Baptist Church directly opposite the Daisy Park and A5 junction, there appears to be an NMU-only link to the disused railway line. As this railway line has been identified as a potential Greenway route, it is proposed to upgrade this NMU-only route by ensuring 3.0m width is achieved throughout its length. Appropriate shared-used facility signage should be installed. Length of facility is approx. 270m.

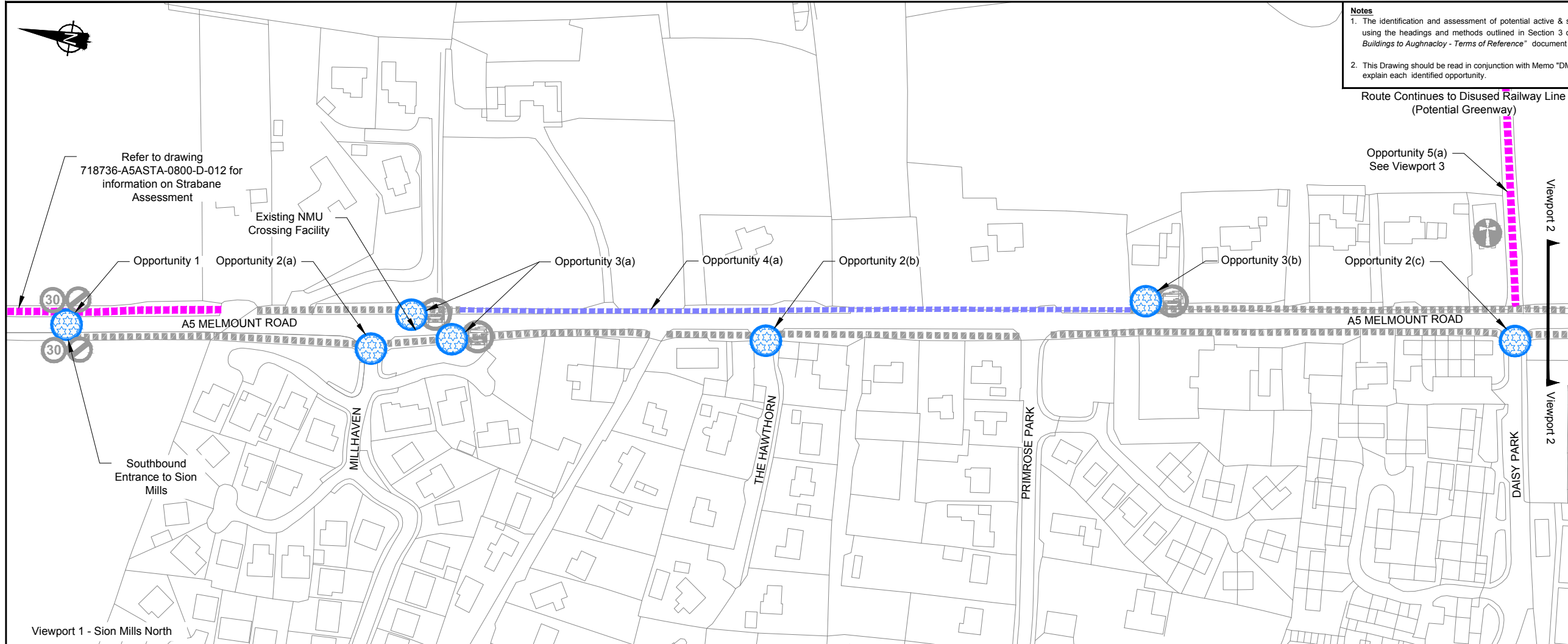


Photograph 7: Upgrade of 'Willows' NMU-only route to Greenway (See Drawing D-013)

Opportunity 5(b)

The disused railway line running parallel to the existing A5 has been highlighted as a potential Greenway location. Access to this section of the Greenway can be provided via Willows. It is proposed to improve access to this section of the line.

The controlled pedestrian crossing adjacent to Daisy Park could be upgraded for shared-use (Opportunity 2(f)) and the northern footway along Willows could be widened to incorporate shared use. Additional land (circa. 2m x 300m) and 4no. street lights will need relocated in order for shared use facility. Length of facility is approx. 300m.



Notes

- The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
- This Drawing should be read in conjunction with Memo "DM11 - A5 - Active & Sustainable Transport Assessment: Sion Mills" which will explain each identified opportunity.

Key

Existing Asset Features

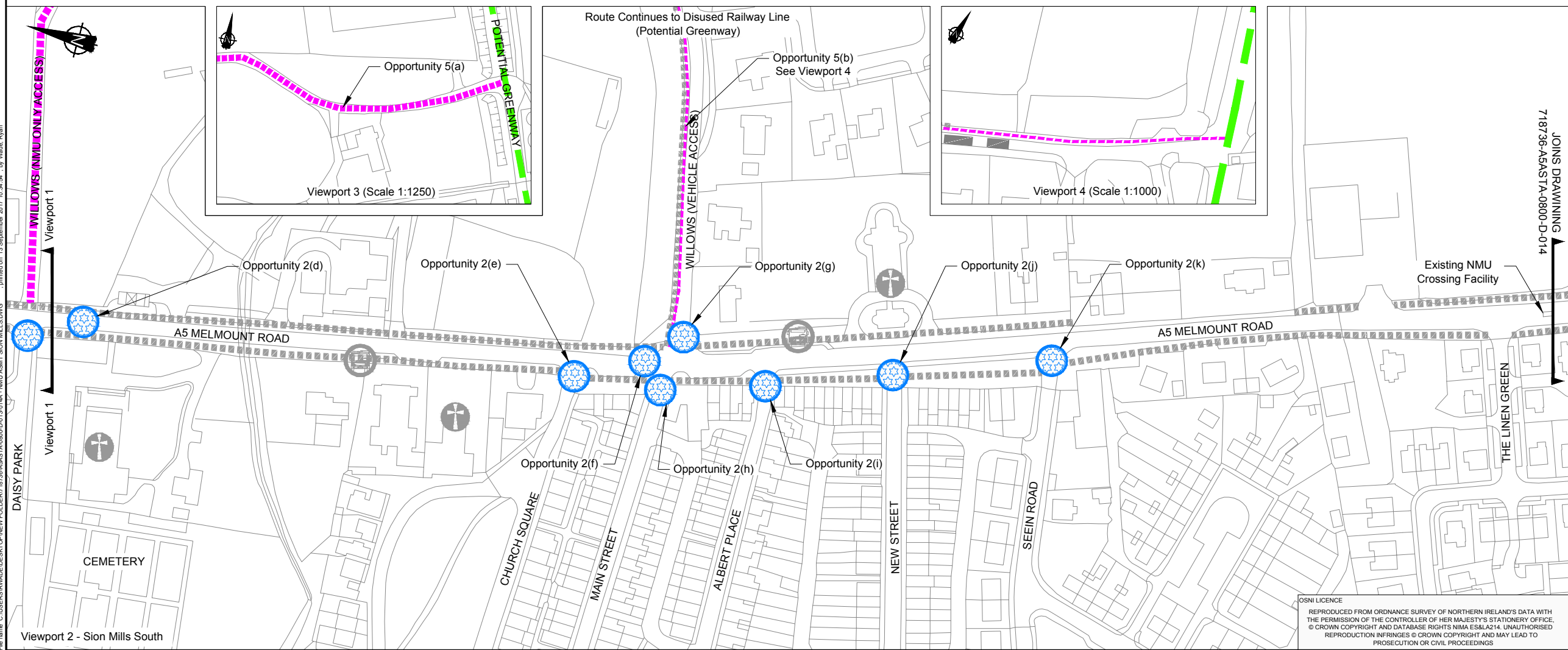
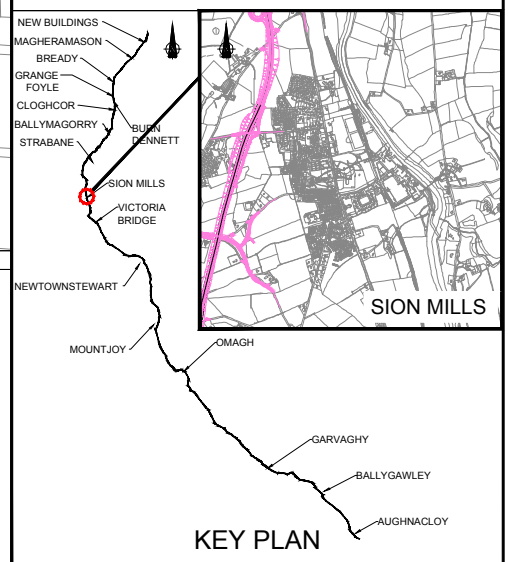
- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme



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STEP/PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy **A5WTC**

TITLE: ASSESSMENTS OF URBAN AREAS LOCATION: SION MILLS, CO. TYRONE SHEET 1 OF 2

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DATE: 14/08/2017
DRAWING NO: 718736-A5ASTA-0800-D-013	REV: A	

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Notes
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 2. This Drawing should be read in conjunction with Memo "DM11 - A5 - Active & Sustainable Transport Assessment: Sion Mills" which will explain each identified opportunity.

Key

Existing Asset Features

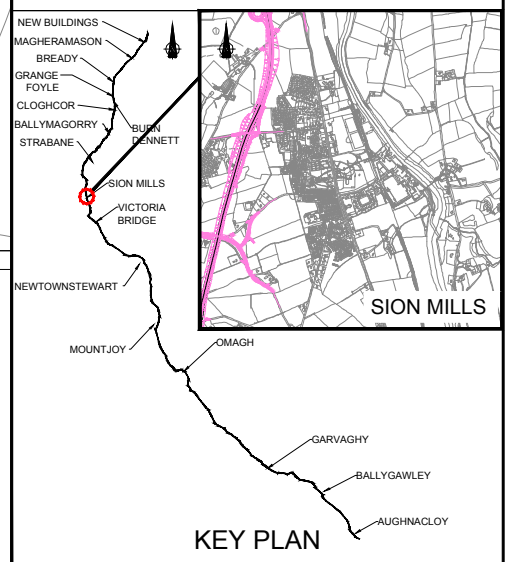
NMU Facility		Bus Stop	
Signage		Sports / Cricket Club	
School		Church	
Residential Care Homes		Health Care	

Proposed Improvements

2m Wide NMU Facility	
3m Wide (Shared Use) NMU Facility	
Existing NMU Facility to be Widened	
Existing NMU Facility to be Widened (via Potential Greenway)	
Potential Greenway Route	
Highway Realignment with new Shared Use NMU Facility	
Opportunity Described in Memo	

Other Features

Proposed A5WTC Scheme



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SITE/PROJECT: **Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy**

TITLE: **ASSESSMENTS OF URBAN AREAS
 LOCATION: SION MILLS, CO. TYRONE
 SHEET 2 OF 2**

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DATE: 14/08/2017

DRAWING NO: 718736-A5ASTA-0800-D-014	REV: A
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DM12 – A5 Active & Sustainable Transport Assessment: Victoria Bridge

Description

The identification and assessment of potential active & sustainable transport opportunities in Victoria Bridge, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 – New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo should be read in conjunction with Drawing: 718736-A5ASTA-0800-D-015 - 016 – ‘Assessments of Urban Areas, Location: Victoria Bridge, Co. Tyrone’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic.

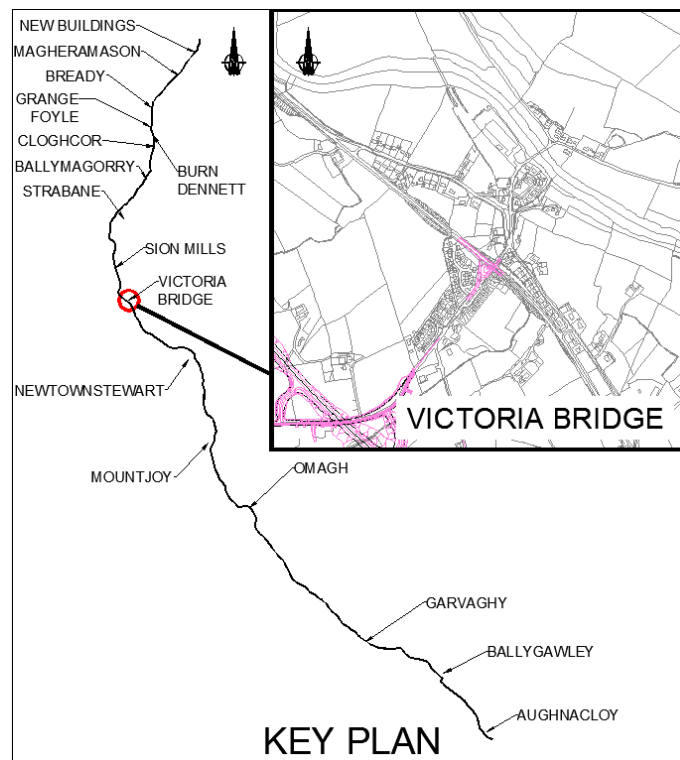


Figure 1: Indicative Location

Summary of Existing NMU Provisions

The focus of the assessment of this urban area is on the roads adjacent to the existing A5 through Victoria Bridge, with the primary focus being on facilities on Melmound Road, which runs broadly parallel to the existing A5 through Victoria Bridge. This approach has been adopted as there are minimal footways adjacent to the existing A5 Mulvin Road. Melmound Road has existing footways on both sides along its length through Victoria Bridge; therefore, it is deemed prudent to assess the existing NMU facilities on Melmound Road rather than propose new NMU facilities on A5 Mulvin Road. Moreover, there is a potential Greenway that runs broadly parallel to the existing A5 bypassing Victoria Bridge at an offset of approximately 280m which, if introduced, would negate any requirement for the introduction of segregated NMU facilities along the existing A5.

Existing Uncontrolled Pedestrian Crossing Points (See Drawing D-016)

There are a number of uncontrolled pedestrian crossing points located at the mini roundabout on Fyfin Road / Melmound Road. Drop kerbs and tactile paving have been provided.



Photograph 1: Red – Existing Uncontrolled Pedestrian Crossing. Melmound Road (Eastern Arm). Green – Existing Uncontrolled Pedestrian Crossing. Fyfin Road (Southern Arm). (See Drawing D-016)

Existing Parking Facilities

There are limited parking opportunities along Melmound Road through Victoria Bridge.

There is a church at the southern end of Melmound Road, immediately before the existing A5. This appears to be the only attractor along Melmound Road. The church has off road parking and online parking will likely not be required.

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Victoria Bridge on Melmount Road, the potential addition of pedestrian crossing facilities and potential Greenway facilities that could be introduced.

Proposals for the provision of NMU facilities in this area include the provision of a 3m wide shared use NMU facility located at the northern end of Victoria Bridge. A 3m wide shared use facility is also proposed at the southern end of Victoria Bridge. However, it should be noted that current greenway proposals might negate the requirement for these proposals.

Opportunity No.1 (See Drawings D-015 and D-016): Village Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements.

Opportunity 2 (See Drawings D-015): NMU Route

There is an existing footway provided on the northbound side of the existing A5 which then crosses over to join on to Melmount Road. This existing facility could possibly be developed into an NMU shared use facility by widening the facility by approx. 0.5m.

Opportunity 3 (See Drawings D-015 and D-016): Pedestrian Crossing Facilities

Opportunity 3(a)

The existing footway terminates to join onto the connecting side road (Melmount Road), with dropped kerbs provided. Due to the nature of the existing A5, with a national speed limit of 60mph, a signalised crossing point could be provided with tactile paving and appropriate road markings.

Opportunity 3(b)

An uncontrolled pedestrian crossing could potentially be provided along Melmount Road. As there are already speed-humps on the road, these can be altered to provide a pedestrian crossing in the form of a 'raised table'. Tactile paving and guardrailing could

be provided. Crossing distance is approximately 6.5m. This crossing point will serve domestic properties at the southern end of Victoria Bridge.

Opportunity 4 (See Drawing D-015): Greenway

A potential NI Greenway Route has been identified. It would pass to the east of Victoria Bridge travelling in a north / south direction, utilising the disused railway line. See image below. The potential greenway could be accessed via the existing footway on Fyfin Road.



Photograph 2: Potential Greenway Route through Victoria Bridge (See Drawing D-015)

Opportunity 5 (See Drawing D-016): NMU Route

The footway and kerbing terminates on the southbound side of Melmourt Road on the approach to the junction with the existing A5.

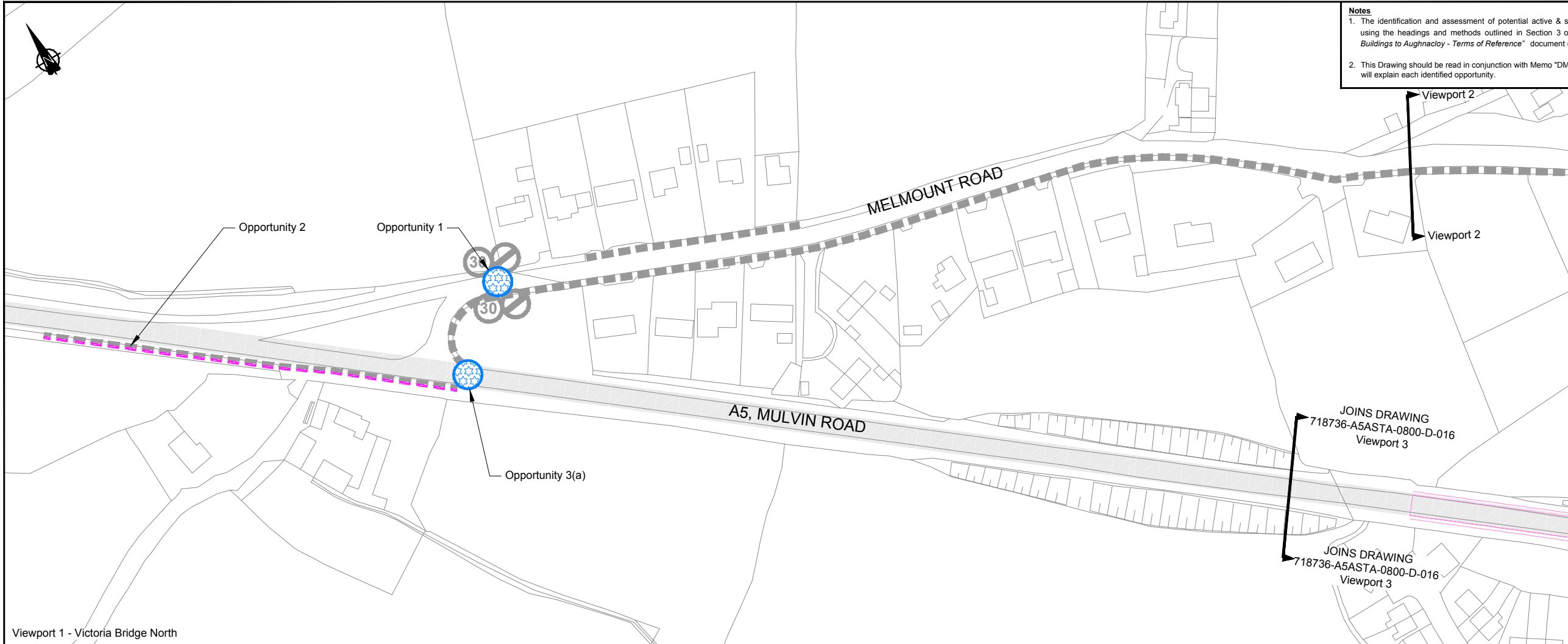
A 3m wide shared use NMU facility could be provided utilising the existing grass verge, linking the Melmourt Road footway to the existing A5. Approximately 3no. sign posts and 1no. lighting column would need to be relocated.



Photograph 3: Provision of Shared Use NMU Facility (See Drawing D-016)

Park and Ride Facilities

During the course of this assessment, Translink has been consulted and have indicated a desire to locate a new Park and Ride facility on the outskirts of Victoria Bridge located in the vicinity of Junction 9 of A5WTC.



Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aghnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM12 - A5 - Active & Sustainable Transport Assessment: Victoria Bridge" which will explain each identified opportunity.

Key

Existing Asset Features

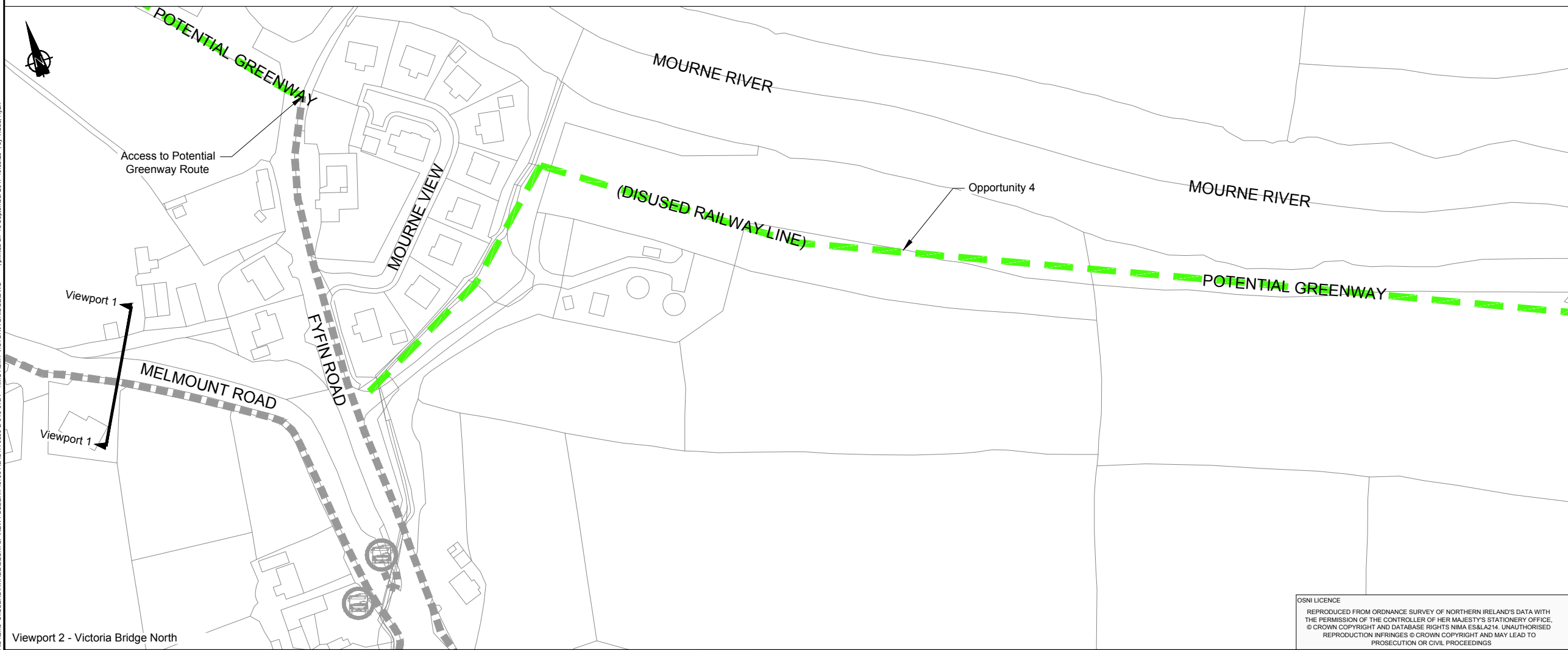
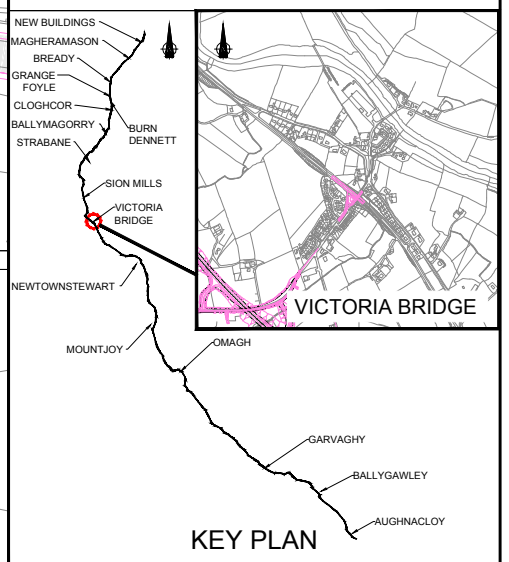
- NMU Facility
- Signage
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme



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TITLE: ASSESSMENTS OF URBAN AREAS
 LOCATION: VICTORIA BRIDGE, CO. TYRONE
 SHEET 1 OF 2

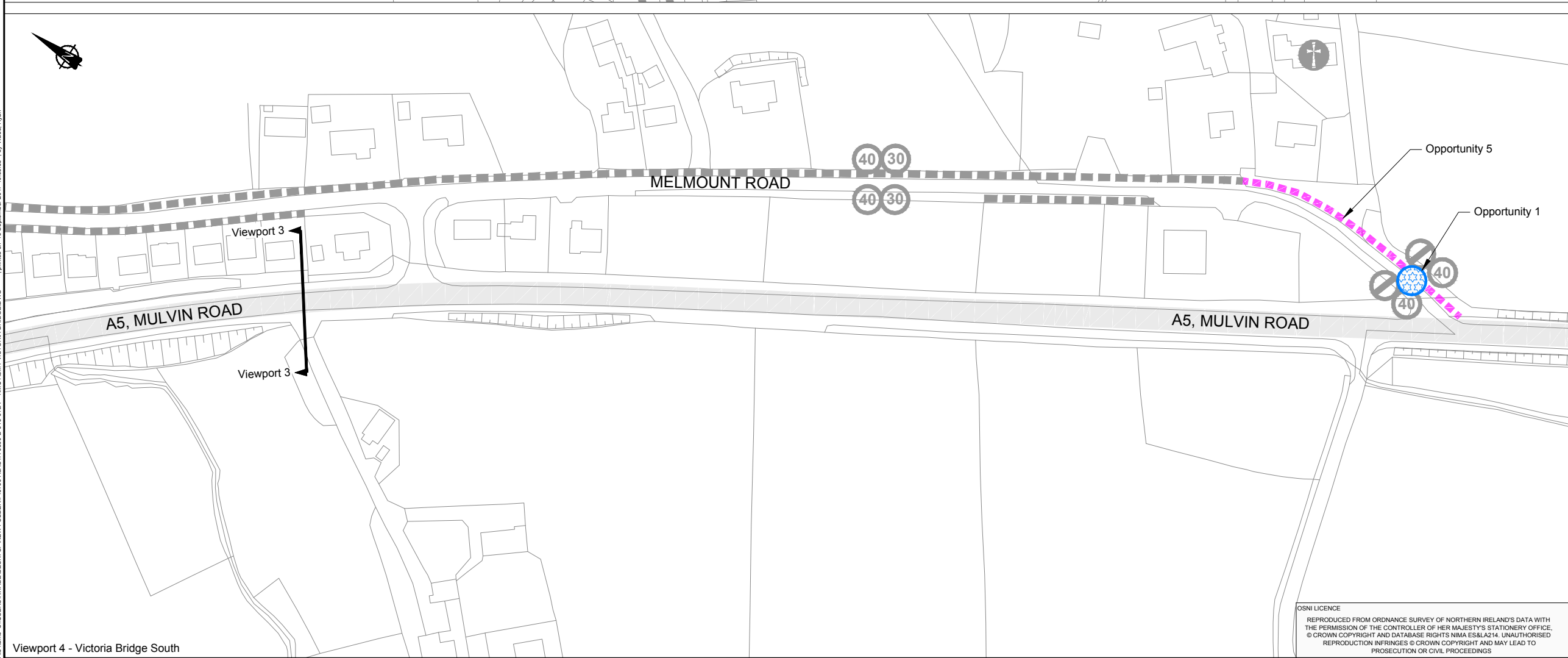
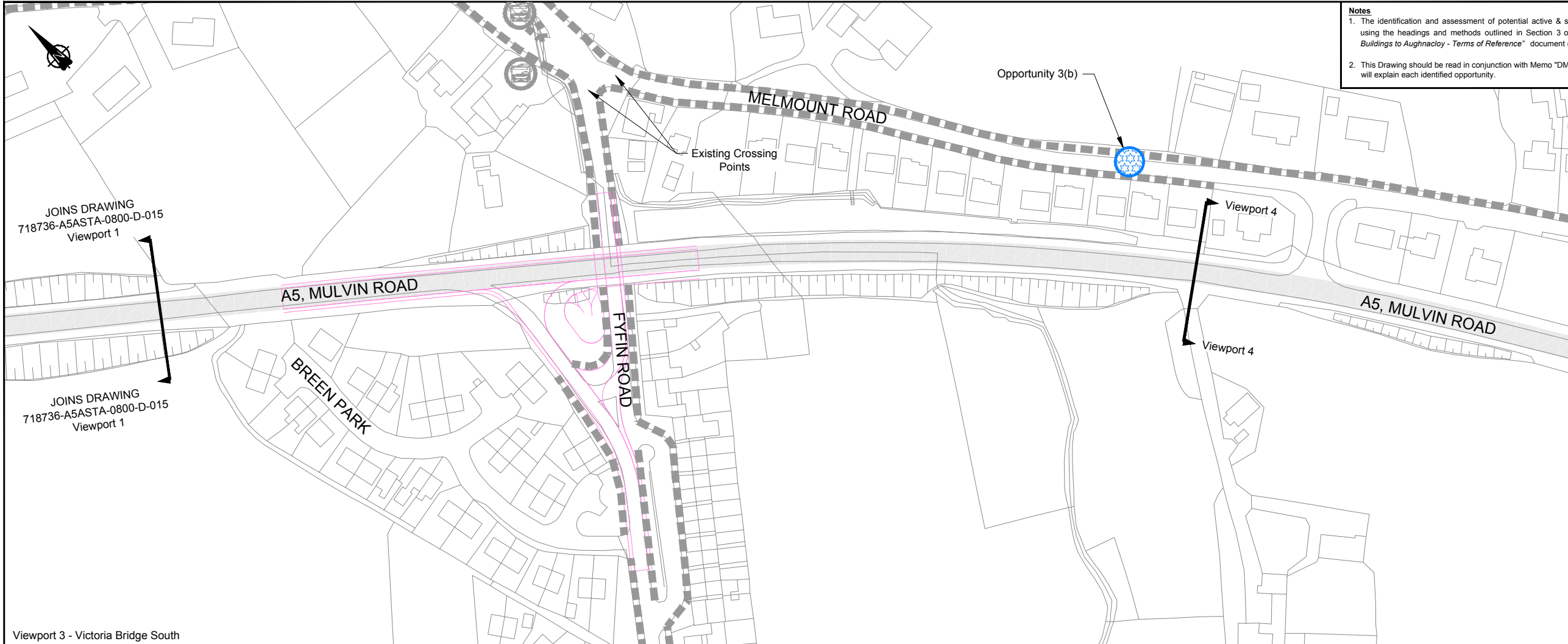
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PROJECT NO: 718736	DESIGNED: OFK	DRAWN: OFK
		DATE: 14/08/2017

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Viewpoint 2 - Victoria Bridge North



Notes

1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).

2. This Drawing should be read in conjunction with Memo "DM12 - A5 - Active & Sustainable Transport Assessment: Victoria Bridge" which will explain each identified opportunity.

Key

Existing Asset Features

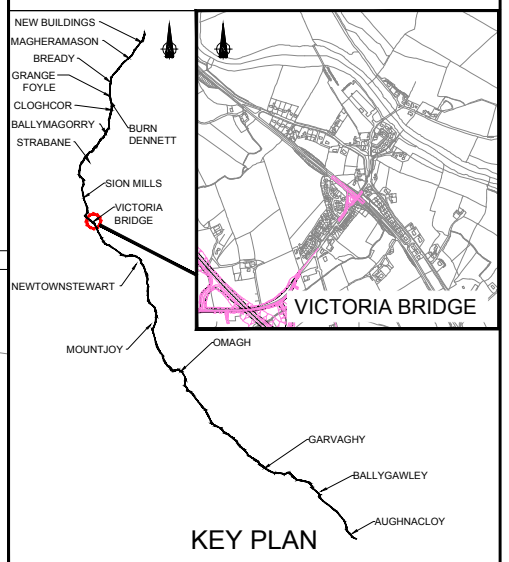
- NMU Facility
- Signage
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

Proposed A5WTC Scheme



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TITLE: ASSESSMENTS OF URBAN AREAS
LOCATION: VICTORIA BRIDGE, CO. TYRONE
SHEET 2 OF 2

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PROJECT NO: 718736	DESIGNED: OFK	DATE: 14/08/2017

DRAWING NO: 718736-A5ASTA-0800-D-016	REV: A
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DM13 – A5 Active & Sustainable Transport Assessment: Newtownstewart

Description

The identification and assessment of potential active & sustainable transport opportunities in Newtownstewart, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 – New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo could be read in conjunction with Drawings: 718736-A5ASTA-0800-D-025 to 026 ‘Assessments of Urban Areas, Location: Newtownstewart, Co. Tyrone’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic on the A5.

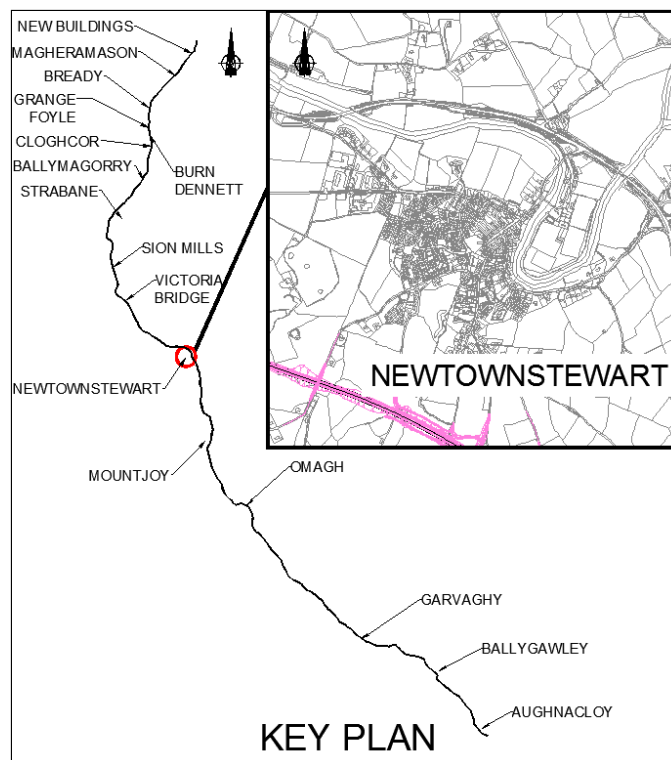


Figure 1: Indicative Location

Summary of Existing NMU Provisions

Newtownstewart the only centre of population within this study area, that is fully bypassed by the existing A5, where the existing A5 crosses to the east side of Strule River to avoid Newtownstewart. Regarding the other centres of population along the existing A5 route between Newbuildings and Aughnacloy, the A5 appears to either pass through the middle of the town, village or hamlet or pass on the perimeter of these areas. Therefore, the existing and potential NMU facilities in the centre of Newtownstewart have not been assessed; instead, any existing and potential facilities on the A5 as it passes Newtownstewart and also the NMU facilities on or adjacent to roads that provide access from the A5 to Newtownstewart (i.e. Strabane Road, Castle Brae and Douglas Road) have been assessed.

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 as it passes Newtownstewart are of a relatively good standard. Existing NMU Provisions include an existing footway adjacent to the northbound A5 carriageway, along Strabane Road as it by-passes Newtownstewart, on Castle Brae Road and Strabane Road leading into the town centre.

National Cycle Route 92 is also located along Douglas Road, as an on road facility.

Existing Underpass Serving Northbound & Southbound Bus Stops (See Drawing D-026)

There are two bus stops provided of either side of the existing A5 near the junction with Castle Brae Road. There is an underpass provided with approx. 3.6m wide pavement for NMU access to the southbound bus stop.



Photograph 1: Existing Bus Stops and NMU-only Underpass. Bus Stops highlighted in Red (See Drawing D-026)

Existing Parking Facilities

The parking facilities in Newtownstewart have not been assessed due to their remoteness to the existing A5.

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Newtownstewart, potential Greenway facilities that could be introduced, and footway improvements.

Proposals include widening the existing footway adjacent to the northbound side of the existing A5 to provide a 3m wide shared user NMU facility.

Opportunity 1 (See Drawing D-025 and D-026): Village Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements.

Opportunity 2 (See Drawing D-025 and D-026): NMU Facilities & Greenways

Opportunity 2(a)

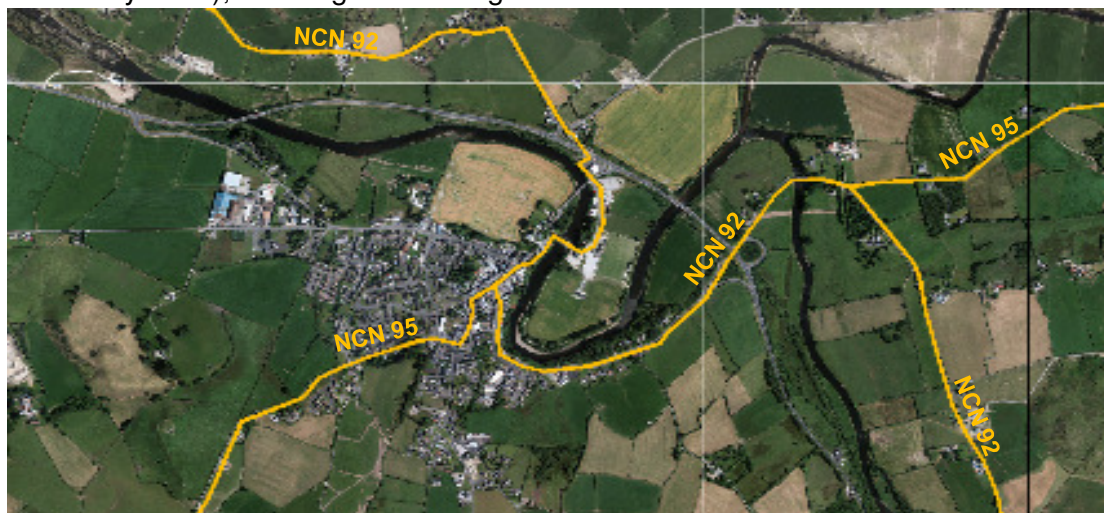
A potential greenway route has been highlighted in the area of Newtownstewart. It is proposed to run parallel to existing A5 utilising the disused railway line, and therefore bypassing the town.

In the image below, the potential greenway is shown in 'green'. The break in the green line represents a length where the disused railway line is not viable as a greenway route as it merges with the existing A5.



Photograph 2: Potential Greenway Route Highlighted in Newtownstewart (See Drawing D-025)

National Cycle Routes 92 and 95 run through Newtownstewart's town centre (shown below in 'yellow'); crossing the existing A5 at two locations.



Photograph 3: Existing Cycle Routes Highlighted in Newtownstewart (See Drawing D-026)

Opportunity 2(b)

The existing NMU facility adjacent to the northbound carriageway of the A5 Strabane Road, could be widened to afford access to the potential greenway discussed in Opportunity 2(a).

Opportunity 3 (See Drawings D-025 and D-026): NMU facilities

Opportunity 3(a)

An existing NMU facility runs parallel to the existing A5 on the northbound side. This facility could potentially be widened by approx. 1.0m over an approximate length of 660m. There appears to be few constraints to the south of the existing facility.



Photograph 4: Existing cycleway to be widened (See Drawing D-025)

Opportunity 3(b)

The proposed 3m wide shared use NMU facility identified in Opportunity 3(a) could be extended beyond the existing crossing point on the existing A5 to the junction with Strabane Road. It could continue along Strabane Road to join the existing footway near the junction with Baronscourt Road to the south.

This will create a circular walking route requested previously by a local political representatives.

Opportunity 4 (See Drawing D-026): Links to Bus Stops

Opportunity 4(a) – Southbound Bus Stop

Currently there is an approximate 2.5m wide NMU facility leading to the bus stop adjacent to the southbound carriageway. This could potentially be widened by 0.5m over a length of of approximately 103m to provide a shared use NMU facility.



Photograph 5: Widening of existing NMU access to bus stops (See Drawing D-026)

Opportunity 4(b) – Northbound Bus Stop

Currently there is an approximate 2.5m wide NMU facility leading to the bus stop adjacent to the northbound carriageway. This could potentially be widened by 0.5m over a length of approximately 75m to provide a shared use NMU facility.

Refer to the image for Opportunity 4(a).

Opportunity 5 (See Drawing D-026): Footway Improvements

Opportunity 5(a)

There is an existing 'on-road' National Cycle Route (NCN 92) that utilises Douglas Road. However, as the surface of Douglas Road does not appear to be in good condition, re-surfacing and the re-application of road markings is recommended.



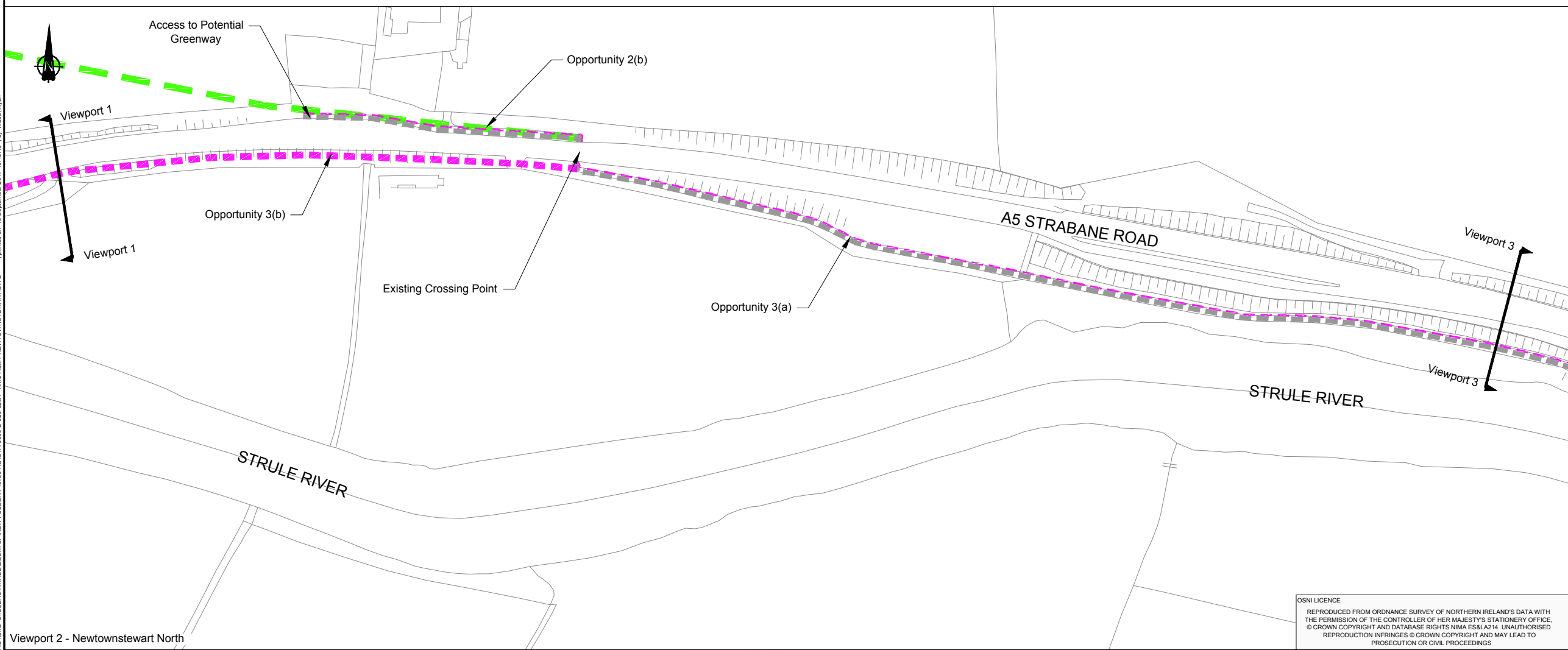
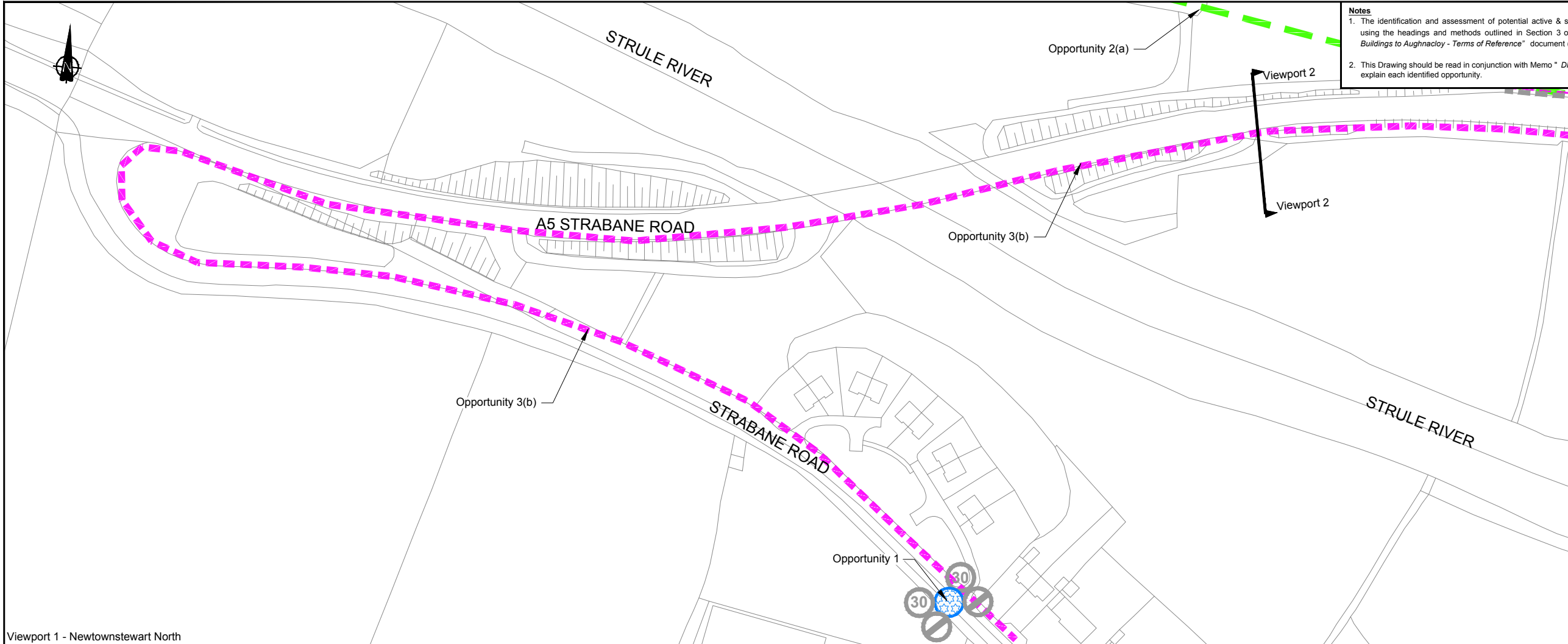
Photograph 6: Existing National Cycleway to be utilised on Douglas Rd (See Drawing D-026)

Opportunity 5(b)

The bridge on Douglas Road is approximately 3.5m wide. This bridge could potentially operate a one-way system with traffic lights to improve road safety for NMUs.

Park and Ride Facilities

During the course of this assessment, Translink has been consulted and have not identified Newtown Stewart as a desirable location for a Park and Ride facility.



Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM13 - A5 - Active & Sustainable Transport Assessment : Newtown Stewart" which will explain each identified opportunity.

Key

Existing Asset Features

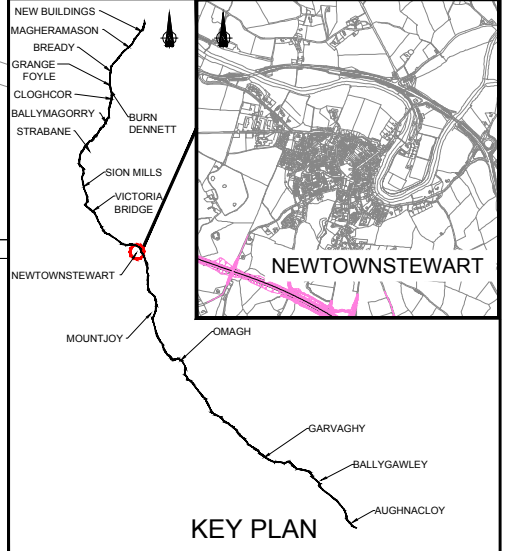
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- Signage: [Symbol 50], [Symbol 40], [Symbol 30]
- School: [Symbol]
- Residential Care Homes: [Symbol RH]
- Bus Stop: [Symbol]
- Sports / Cricket Club: [Symbol]
- Church: [Symbol]
- Health Care: [Symbol HC]

Proposed Improvements

- 2m Wide NMU Facility: [Symbol]
- 3m Wide (Shared Use) NMU Facility: [Symbol]
- Existing NMU Facility to be Widened: [Symbol]
- Existing NMU Facility to be Widened (via Potential Greenway): [Symbol]
- Potential Greenway Route: [Symbol]
- Highway Realignment with new Shared Use NMU Facility: [Symbol]
- Opportunity Described in Memo: [Symbol]

Other Features

- Proposed A5WTC Scheme: [Symbol]



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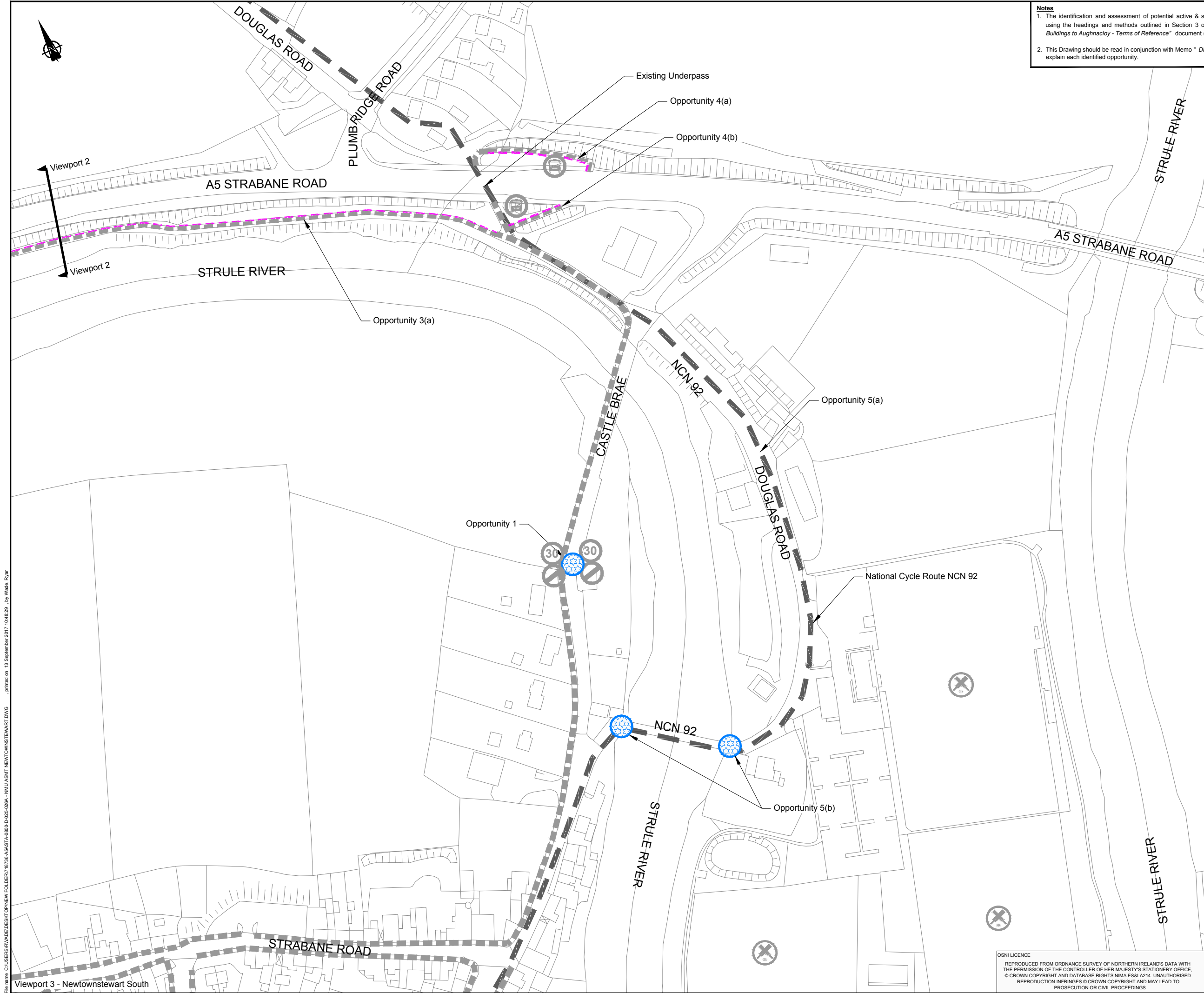
TITLE: **ASSESSMENTS OF URBAN AREAS LOCATION: NEWTOWNSTEWART, CO. TYRONE SHEET 1 OF 2**

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: OFK	DRAWN: OFK
DATE: 14/08/2017		

DRAWING NO: **718736-A5ASTA-0800-D-025** REV: **A**

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Notes

- The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aghnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
- This Drawing should be read in conjunction with Memo "DM13 - A5 - Active & Sustainable Transport Assessment : Newtownstewart" which will explain each identified opportunity.

Key

Existing Asset Features

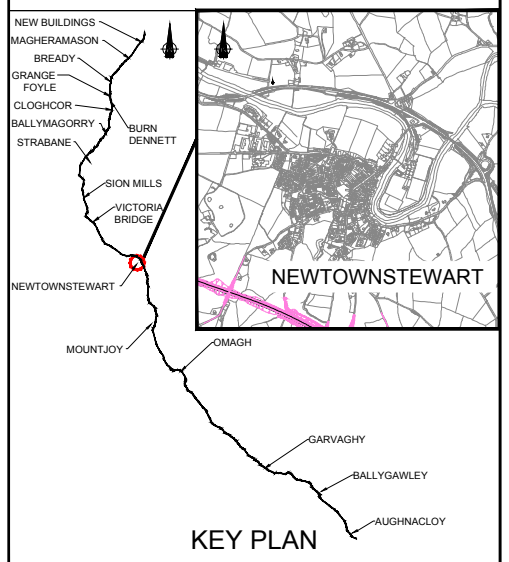
NMU Facility		Bus Stop	
Signage		Sports / Cricket Club	
School		Church	
Residential Care Homes		Health Care	

Proposed Improvements

2m Wide NMU Facility	
3m Wide (Shared Use) NMU Facility	
Existing NMU Facility to be Widened	
Existing NMU Facility to be Widened (via Potential Greenway)	
Potential Greenway Route	
Highway Realignment with new Shared Use NMU Facility	
Opportunity Described in Memo	

Other Features

Proposed A5WTC Scheme



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SITE/PROJECT: **Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aghnacloy**

TITLE: **ASSESSMENTS OF URBAN AREAS LOCATION: NEWTOWNSTEWART, CO. TYRONE SHEET 2 OF 2**

SCALE @ A1 1:1000	CHECKED: PD	APPROVED: PGE
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DRAWING NO: 718736-A5ASTA-0800-D-026	REV: A	

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Viewport 3 - Newtownstewart South

DM14 – A5 Active & Sustainable Transport Assessment: Mountjoy

Description

The identification and assessment of potential active & sustainable transport opportunities in Mountjoy, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo could be read in conjunction with Drawings: 718736-A5ASTA-0800-D-017 to 018 ‘Assessments of Urban Areas, Location: Mountjoy, Co. Tyrone’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic on the A5.

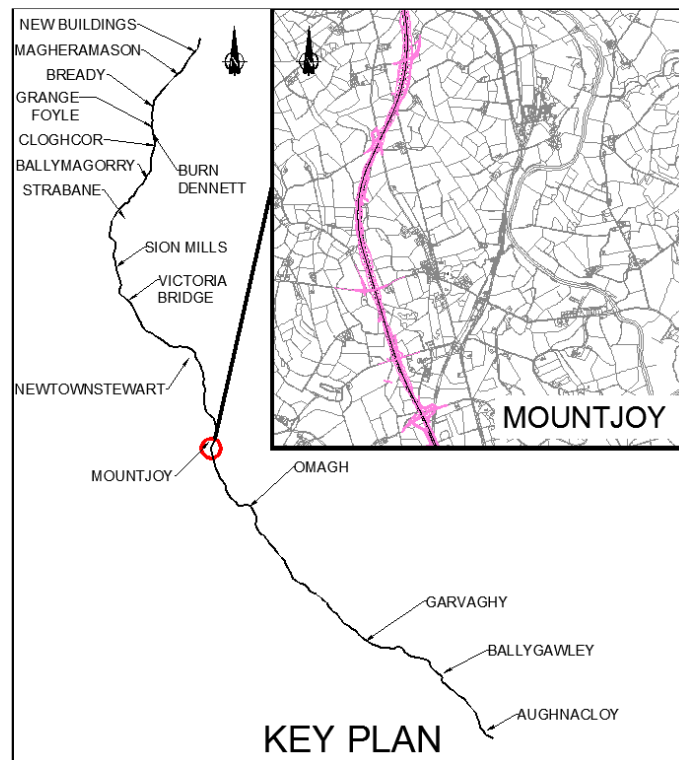


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that there are no current provisions for non-motorised users (NMUs) on and around the existing A5 through Mountjoy.

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Mountjoy, potential footway improvements, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5, potential Greenway facilities that could be introduced and potential improvements to existing bus stop facilities.

Proposals include improved footways provisions on Mountjoy Avenue leading to the existing A5, also along the existing A5 which will include provision at bus stop facilities and on the Castletown Road. It should be noted that current greenway proposals may negate the requirement for the provisions of NMU facilities through Mountjoy as NMU's will avail of this facility.

Opportunity 1 (See Drawings D-017 and D-018): Village Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements.

Opportunity No.2 (See Drawings D-017 and D-018): Footway Improvements

Opportunity 2(a) & 2(b)

The southbound bus stop on the A5 is isolated from any footways, crossing points and links to Mountjoy. A 2.0m wide footway could be constructed in the southbound verge along with a 2.0m wide footway in the northbound verge.



Photograph 1: Opportunity 2(a) Footway to Southbound Bus Stop Shelter, Opportunity 2(b) Footway to Southbound Bus Stop Shelter, Opportunity 3(a) Uncontrolled Ped. Crossing – Refuge Island (See Drawing D-017)

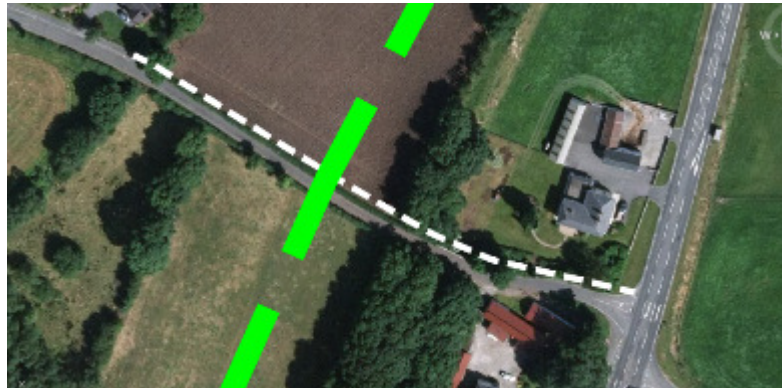
Drop kerbs and tactile paving could be constructed along the 2no. private accesses along the new footway.

Opportunity 2(c)

The northbound bus stop on the A5 is isolated from any footways, crossing points and links to Mountjoy. A 2.0m wide footway could be constructed in the existing verge

Opportunity 2(d)

Due to the limited visibility and restricted cross section (due to narrow bridge) along Mountjoy Avenue, a footpath along this stretch would not be suitable with regards to safety.



Photograph 2: Location of potential greenway along Mountjoy Avenue (See Drawing D-017)

An independent 2.0m wide, 170m long footway which crosses a small stream could be constructed linking the bus stops and greenway to Mountjoy. Land of approx. 5.0m x 170.0m would need to be acquired to provide this link. The suggested footway will tie into an existing footway – where the white dashed line ends at the top of the image. The footway was chosen on this side of the road due to a cemetery on the opposite side further towards the town. If the opposite side was selected, an additional crossing point would be required.

Opportunity No.3 (See Drawings D-017 and D-018): Pedestrian Crossing Facilities

Opportunity 3(a)

Provide an uncontrolled pedestrian crossing across the junction of Bellview and the Mountjoy Avenue. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 6.0m.

Opportunity 3(b)

Provide an uncontrolled pedestrian crossing across the junction of Mountjoy Avenue and Castletown Road. Drop kerbs and tactile paving could be installed along the footway's desire line. The crossing distance is approximately 7.0m.

Opportunity 3(c)

Provide an uncontrolled pedestrian crossing across the Castletown Road to the new footway described in Opportunity 2(e). Drop kerbs and tactile paving could be installed along the footway's desire line. The crossing distance is approximately 7.0m.

Opportunity 3(d)

Provide an uncontrolled pedestrian crossing across the junction of the Castletown Road and the cul-de-sac. Drop kerbs and tactile paving could be installed along the footway's desire line. The crossing distance is approximately 7.0m.



*Photograph 3: Uncontrolled Pedestrian Crossing - Castletown Road and Cul-de-Sac
(See Drawing D-018)*

Opportunity 3(e)

Provide an uncontrolled pedestrian crossing across the Castletown Road, approx. 35m from the crossroad. Drop kerbs and tactile paving could be installed along the footway. The crossing distance is approximately 6.5m.

Opportunity No.4 (See Drawings D-017 & D-018): Bus Stop Shelters

Opportunity 4(a)

The addition of a bus stop shelter could be installed in the verge at this location or on the new footway mentioned earlier in Opportunity 2(c).

Opportunity 4(b)

There are two bus stops within the extent of the parking facilities on Castletown Road, as identified in Opportunity 2(e). The footway in the vicinity of the bus stops could be widened to allow for a bus stop shelter to be erected. Some parking bays will have to be removed as a result.

Opportunity 4(c)

The addition of a larger bus stop shelter could be installed in the verge at this location.



Photograph 4: Larger Bus Stop shelter (northbound) outside Folk Museum (See Drawing D-017)

Opportunity 4(d)

The existing bus shelter at this location could be upgraded.

Opportunity 5 (see drawing D-017): NMU Routes & Greenways

Opportunity 5(a)

A disused railway has been identified running just south of the Ulster Folk Museum through Mountjoy which intersects Mountjoy Avenue. This has been identified as an opportunity to introduce a potential greenway route as shown in the image associated with Opportunity 2(d).

Opportunity 5(b)

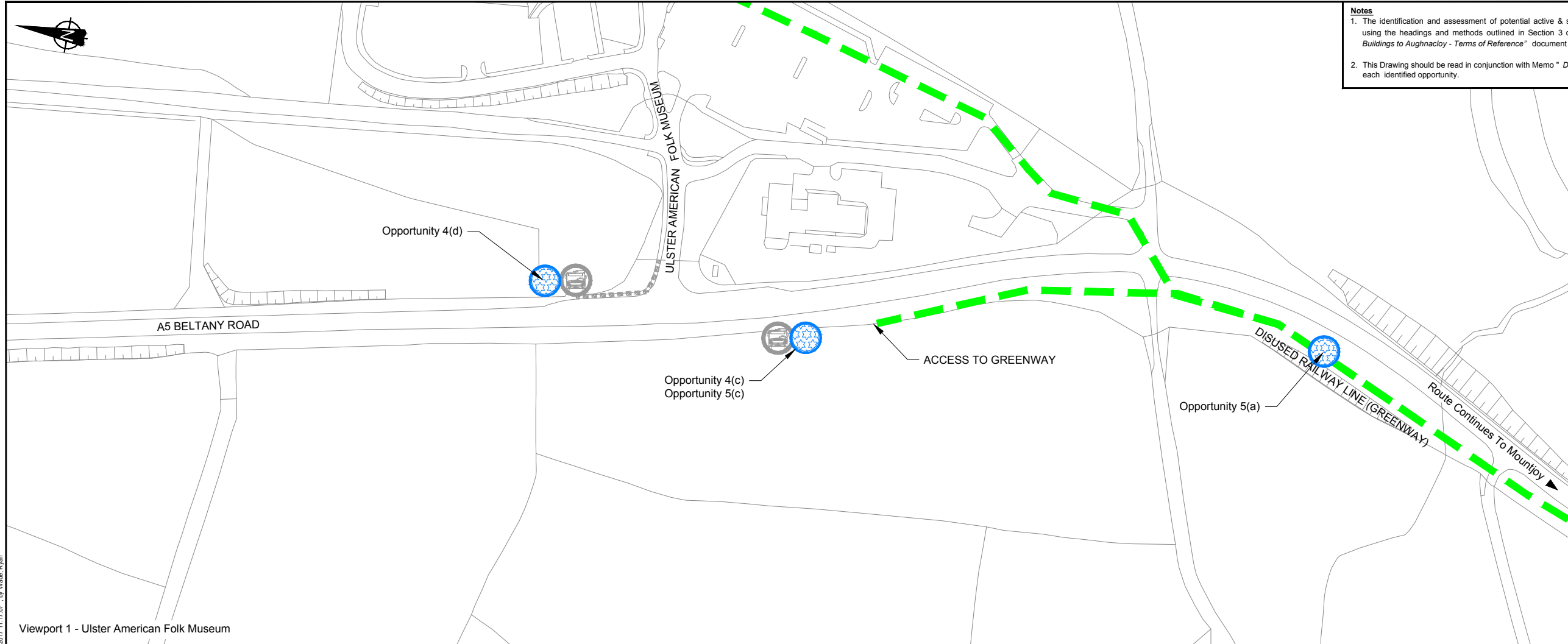
After the parallel parking bays outside the Presbyterian Church on Castletown Road, the existing footway can be widened 0.5m towards the centre of the carriageway approximately for 40.0m and converted into a shared use facility.

When the footway ends, this 3.0m wide facility can then be extended for 375m where it will meet the disused railway line (i.e. Greenway) terminating in the proximity of Mountjoy United FC pitch.

Access to pitch can be provided via greenway or new shared use NMU facility onto Lisnagirr Road.

Opportunity 5(c)

Access to the Folk Museum can be provided by the greenway that can be accessed from Mountjoy's footways highlighted in Opportunities 2(d) and 5(b).



Viewport 1 - Ulster American Folk Museum

Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo * DM14 - A5 - Active & Sustainable Transport Assessment: Mounjoy * which will explain each identified opportunity.

Key

Existing Asset Features

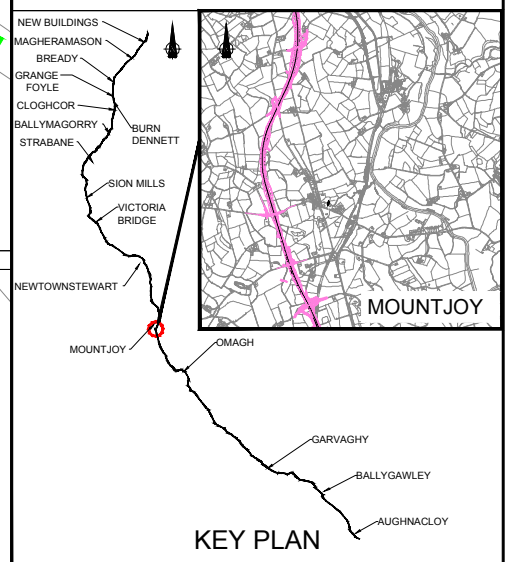
- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

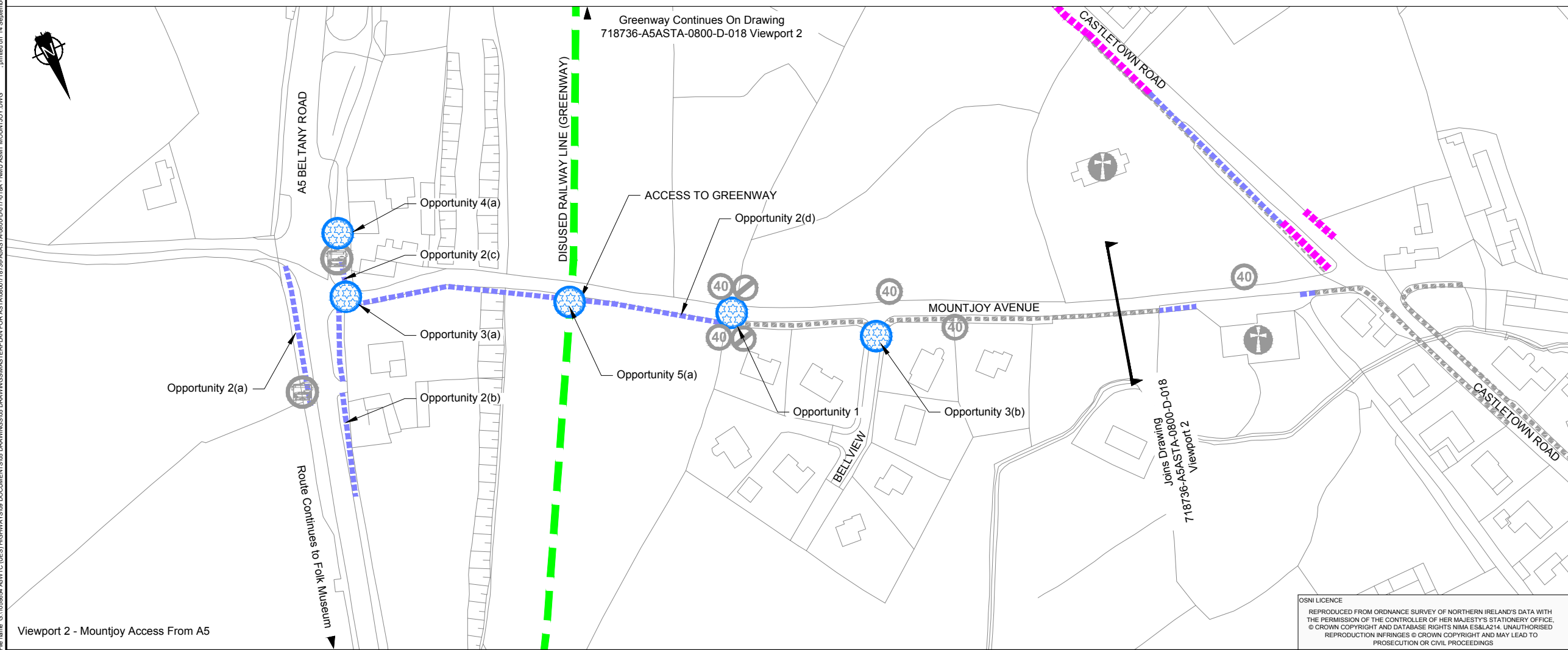
- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme



KEY PLAN



Viewport 2 - Mounjoy Access From A5

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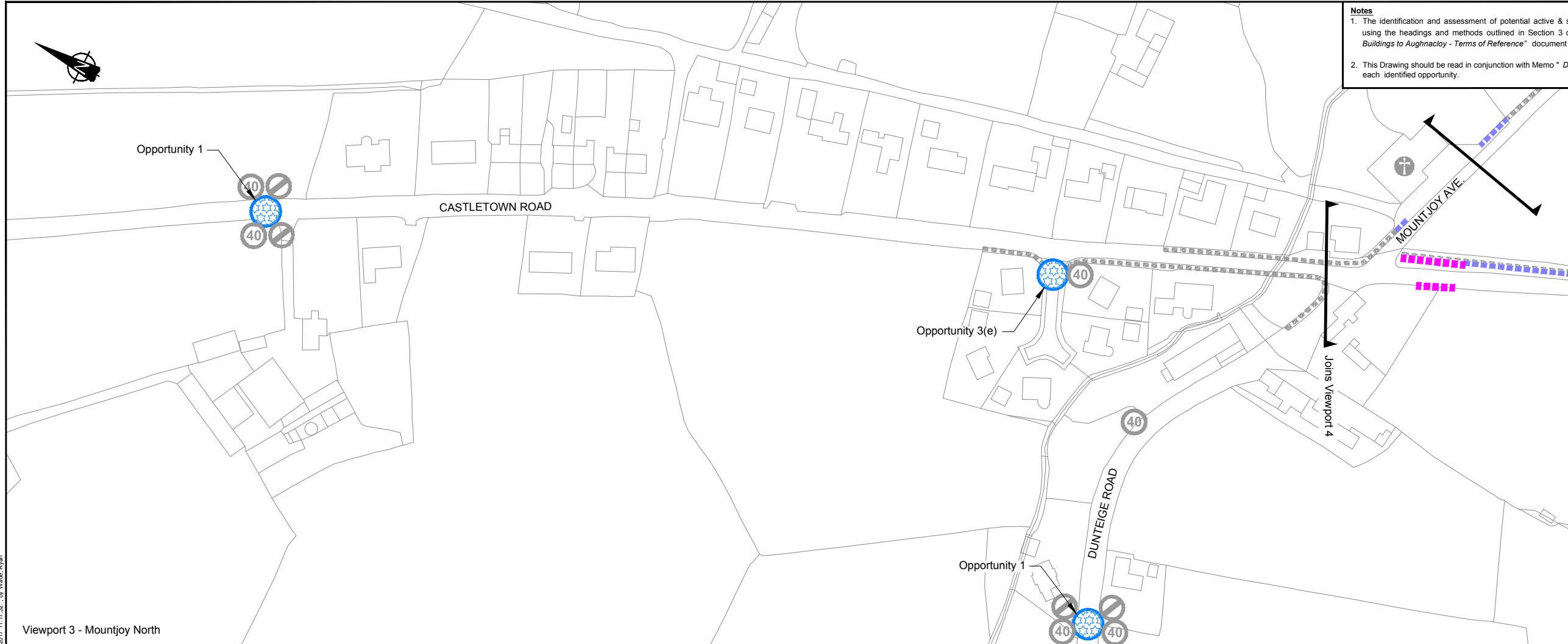
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 LOCATION: MOUNTJOY, CO. TYRONE
 SHEET 1 OF 2

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DATE: 14/08/2017

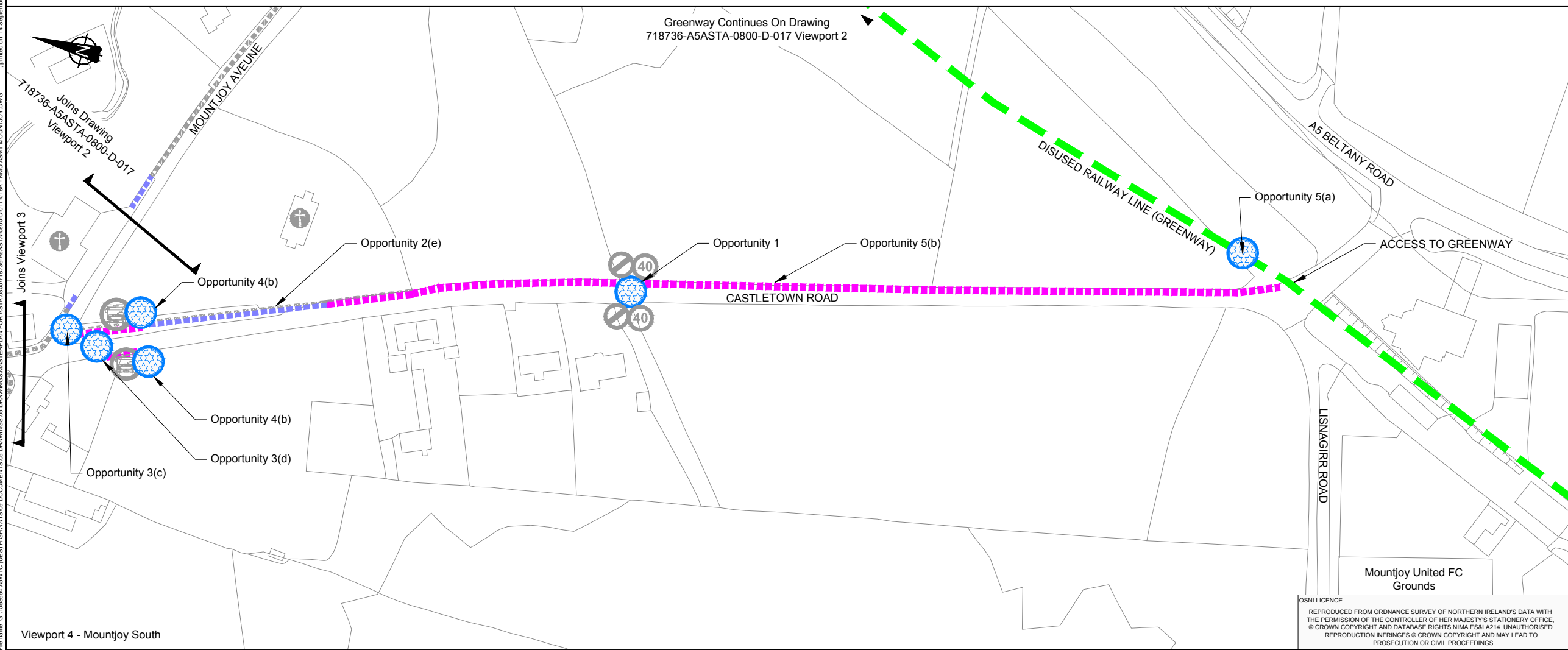
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Viewport 3 - Mountjoy North



Viewport 4 - Mountjoy South

Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM14 - A5 - Active & Sustainable Transport Assessment: Mountjoy" which will explain each identified opportunity.

Key

Existing Asset Features

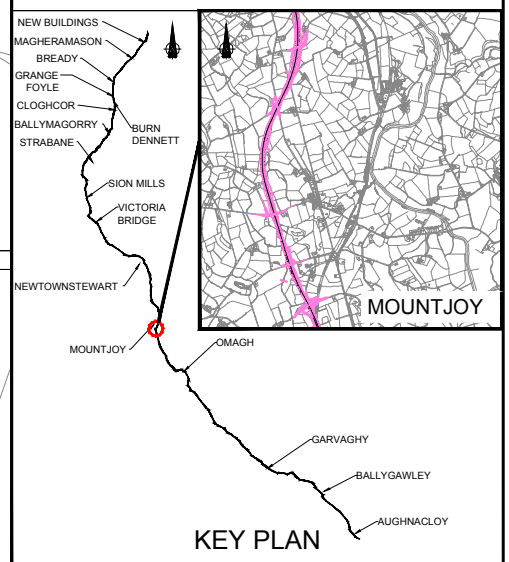
- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme



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STEP/PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy

TITLE: ASSESSMENTS OF URBAN AREAS
 LOCATION: MOUNTJOY, CO. TYRONE
 SHEET 2 OF 2

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DRAWING NO: 718736-A5ASTA-0800-D-018	REV: A
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DM15 – A5 Active & Sustainable Transport Assessment: Omagh

Description

The identification and assessment of potential active & sustainable transport opportunities along the A5 in Omagh, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo should be read in conjunction with Drawing: 718736-A5ASTA-0800-D-019 to 024 ‘Assessments of Urban Areas, Location: Omagh, Co. Tyrone’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic.

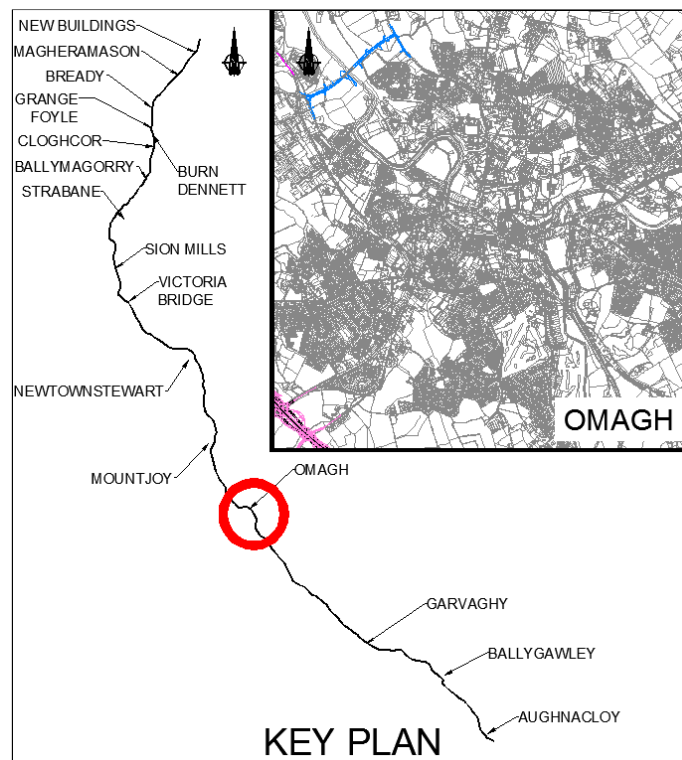


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 through Omagh are of a relatively good standard. Currently there are footway provisions at discrete locations adjacent to either the northbound and southbound carriageway along the existing A5.

Existing Footway – Beltany Grove (See Drawing D-019)

The existing footway provided outside Beltany Grove along the A5 Beltany Road on the southbound side appears to meet the Acceptable Minimum footway width of 2m quoted in Table 7.2 of TA 90/05.



Photograph 1: Southbound footway of the A5 Beltany Road outside the Beltany Grove Residential Development (See Drawing D-019)

Existing Parking Facilities

The existing A5 could be described as an internal ring round around Omagh town centre. For the most part, developments off the A5 have been planned industrial / residential / commercial development with their own offline parking facilities. Therefore, no additional on-line parking is proposed.

Existing Cycling Facilities / NI Greenway Networks

While Sustrans has identified the Gortin Road in Omagh as part of Route 92 on the National Cycle Network, the A5 is not currently identified as a route on their online map. Route 92 could be accessed by cyclists traveling on the A5 via the improvements proposed opportunities. These opportunities would connect into the facilities proposed on the Strathroy Link Road which would connect to the Gortin Road via Strathroy Road.

A potential Greenway route which utilises the disused railway line has been identified along Crevenagh Road (southern end of Omagh). NMU access to this greenway can be provided via Crevenagh Road roundabout, utilising 4(o) and 4(n)/3(j). The greenway route continues in the Dungannon direction.

Existing NMU Pedestrian Crossings (See Drawings D-019 to D-025)

There are numerous existing pedestrian refuge island crossings throughout Omagh. They are situated in the following locations:-

- Pedestrian Refuge Island crossing Existing A5 Beltany Road located to the south of entrance to Beltany Grove (See Drawing D-019)
- Pedestrian Refuge Island crossing Existing A5 Beltany Road located to the north of entrance to Watson Park (See Drawing D-019)
- Pedestrian Crossing Gillygooley Road at Derry Road Roundabout (See Drawing D-020)
- Pedestrian Refuge Island located on Existing A5 Great Northern Road at Derry Road Roundabout (See Drawing D-020)
- Pedestrian Refuge Island located on Existing A5 Great Northern Road at junction with entrance to Gortrush Industrial Estate (north entrance) (See Drawing D-021)
- Pedestrian Refuge Island located on Existing A5 Great Northern Road at junction with entrance to Gortrush Industrial Estate (south entrance) (See Drawing D-020)

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Omagh, potential footway improvements, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5 and potential Greenway facilities that could be introduced.

Proposals include for the creation of a 3m wide facility at the north end of Omagh by widening the existing footway. A 3m wide facility is proposed at the intersection with Strathroy Link Road, between Derry Road Roundabout and Brookmount Road, between Brook Street and the A5 / Railway terrace junction on the southbound carriageway. A 3m wide facility is also proposed between A5 / Railway terrace and Kelvin Avenue / Kelvin Road junction where it is proposed to reduce the 3 lanes to 2 in order to facilitate this on the northbound side. It is also proposed to create a 3m wide facility on Dublin Road by widening the existing footway as well as providing a 3m wide facility on the existing A5 between Crevenagh Road Roundabout and a new development site on the southbound carriageway by creating the new footway within the existing verge.

Opportunity 1 (See Drawings D-019 and D-024): Town Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping

features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements; the two images below illustrate the existing gateway features.



Photograph 2: Southbound entrance to Omagh (See Drawing D-019)



Photograph 3: Northbound entrance to Omagh (See Drawing D-024)

Opportunity 2 (See Drawings D-019 and D-020): Footway Improvements

Opportunity 2(a)

The end of the northbound footway outside the old Nestle factory is poorly defined. The absence of a definitive 125mm kerb upstand may lead to visually impaired pedestrians inadvertently entering the carriageway and coming into conflict with other road users. This section of footway (circa. 30m) could be upgraded to provide a 125mm upstand and the preferred 2.6m width (Table 7.2 of TA 90/05).

It is likely that the large utility chamber at the end of the footway and the two smaller chamber lids in the grass verge at the back of the footway would need to be replaced if this work was implemented.

Opportunity 2(b)

This section of footway terminates at the entrance to the AFBI Veterinary Laboratory / Free Presbyterian Church without a definitive kerb or tactile paving which may lead to a visually impaired pedestrian inadvertently entering the carriageway and coming into conflict with other road users. Opportunity 4(c) is for a potential new shared use facility, if this is introduced, the end of footway should be upgraded to provide an uncontrolled crossing with dropped kerbs and tactile paving.



Photograph 4: Southbound footway terminus at the entrance to AFBI Veterinary Laboratory / Free Presbyterian Church (See Drawing D-019)

Opportunity 2(c)

A footway has been provided adjacent to the southbound carriageway. The footway appears to be at less 2m wide (the Acceptable Minimum width quoted in Table 7.2 of TA 90/05).



Photograph 5: Left: Southbound view of A5 Great Northern Link exit from Derry Road Roundabout; Right: Southbound view of A5 Great Northern Link opposite the northern entrance to the Gortrush Industrial Estate (See Drawing D-020 for both)

In line with Opportunity 4(g), the footway could be widened to over 2m by extending the existing footway into the carriageway and removing the hard strip at the edge of the southbound lane. This approach would allow for the existing southbound lane and right turn lane into the Gortrush Industrial Estate to remain unaltered. The footway extension would entail circa. 170m x 1m of new footway and the redesign of the existing road drainage system over this length. A new combined kerb and gully approach (circa. 170m) would likely be required along this new section to ensure that gully grates are not moved into the wheel track of vehicles travelling south on the A5 Great Northern Road.

The existing pedestrian guard rail opposite the Gortrush Industrial Estate would need to be replaced (circa. 25m) in line with the new kerbline position.

2No. road lighting columns, 1No. road lighting cabinet and 3No. direction signs would also need to be relocated along this section.

The proposed footway extension would end at the point where the existing footway / shared NMU facility moves offline to run parallel to the A5 Great Northern Road.



Photograph 6: Southbound view of A5 Great Northern Link south of the northern entrance to the Gortrush Industrial Estate (See Drawing D-020)

Opportunity 3 (See Drawings D-019 to D-024): Pedestrian Crossing Facilities

Opportunity 3(a)

Tactile paving and dropped kerbs have not been provided across the entrance to disused factory. The absence of tactile paving may lead to visually impaired pedestrians crossing at an inappropriate location and potentially coming into conflict with other road users. An uncontrolled crossing could be implemented at this location.

Opportunity 3(b)

In conjunction with Opportunity 4(c), an uncontrolled pedestrian crossing could be installed across the entrance of Beltany House, with drop kerbs and tactile.

Opportunity 3(c)

The uncontrolled crossing at this location could be upgraded to a controlled crossing. NMUs travelling southbound (on the northbound side) on the new shared NMU facility will need to cross the carriageway if their destination is Omagh town centre, the proposed Strule Shared Education Campus, Watson Park or the bus stop outside Watson Park.

Providing a controlled crossing at this location would cater for these movements and reduce NMU movements on the arms of the busy Derry Road roundabout.

Opportunity 3(d)

The existing uncontrolled pedestrian crossing could be upgraded replacing the existing pedestrian island with an wider island to cater for cyclists and large group of workers which currently travel to and from the Gortrush Industrial Estate from Derry Road at peak times. For further information on upgrades in this vicinity please refer to Opportunities 2(c) and 4(g).



Photograph 7: Northbound view of A5 Great Northern Link south of the entrance to the Gortrush Industrial Estate (See Drawing D-020)

Opportunity 3(e)

The offline footway terminates at an uncontrolled crossing at Brook Street, which directs NMUs directly across Brook Street. This arrangement requires NMUs to cross three lanes of traffic on a busy road, close to a cross roads. NMUs may need to wait for long periods of time for a suitable gap in traffic to cross the carriageway and continue their journey. 2No. Toucan Crossings could be provided on Brook Street on either side of the Thornville Park junction to give added protection for NMUs crossing the carriageway. This approach would also provide an opportunity for road users turning right off Thornville Park to safely cross the westbound right turn lane of Brook Street.



Photograph 8: End of cycleway at Brook Street (See Drawing D-021 and D-022)



Photograph 9: Eastbound view of Brook St. west of the junction with Thornville Rd and the A5 Great Northern Link on-slip (See Drawing D-021 and D-022)

Opportunity 3(f)

An uncontrolled pedestrian crossing could be installed across the existing A5, Great Northern Road. Drop kerbs, tactile paving and a refuge island could be installed. This pedestrian crossing will link the footway identified in Opportunity 4(i).



Photograph 10: Southbound view of A5 Great Northern Road (See Drawing D-022)

Opportunity 3(g)

A new uncontrolled pedestrian crossing could be provided at a point approximately 100m in advance of the existing signals at the Railway Terrace / James Street junction. A large pedestrian refuge island suitable for a cyclists would be placed in the central hatch before the direct taper begins.



Photograph 11: Southbound on the A5 Great Northern Link immediately north of the Railway Terrace / Tamlaght Road Junction (See Drawing D-022)



Photograph 12: Tamlaght Rd. exit off the A5 Great Northern Link at the Railway Terrace / Tamlaght Rd. Junction (See Drawing D-022)

A new controlled pedestrian crossing(s) could potentially be incorporated into the existing signals across the Tamlaght Road junction to tie into the new facility proposed in Opportunity 4(j). This would involve adding dropped kerbs and tactile paving to revised splitter islands (3No.) and the removal of street furniture (flower pots).

Opportunity 3(h)

The shared NMU facility identified in Opportunity 4(k) would terminate at the A5 Great Northern Link / Kelvin Avenue / Kelvin Road junction. New signals could be provided at the junction to allow for NMUs to cross from the northbound side of the carriageway to a new facility on the opposite side of the carriageway.



Photograph 13: Southbound on the A5 Great Northern Link at the Kelvin Avenue junction (See Drawing D-023)

As discussed in the Opportunity 4(k), the left turn lane off the A5 would be removed to facilitate the construction of the proposed facility adjacent to the northbound carriageway. In removing the left turn lane the entire junction could potentially be realigned to a simplified T-Junction arrangement where the A5 Great Northern Link would remain as the major road.

The existing raised islands where the traffic signals currently sit would be removed and replaced with at grade toucan crossings across the A5 Great Northern Link and the Kelvin Road / Avenue exit. NMUs travelling on the northbound shared use facilities would be directed to cross over to a new facility adjacent to the southbound carriageway of the A5 Great Northern Link.

Opportunity 3(i)

A new uncontrolled pedestrian crossing, directing NMUs travelling southbound to the opposite side of Dublin Road could potentially be provided.

Opportunity 3(j)

A new uncontrolled crossing could potentially be provided at the A505 exit off the Creveneagh Road Roundabout between the roundabout and the Crevenagh Road.



*Photograph 14: Southbound on the A505 towards the Crevenagh Rd. Roundabout
(See Drawing D-024)*

To provide a crossing at a location which is close to the likely NMU desire line, a break in the existing VRS would be required. Approximately 30m of new VRS, including 3No. new terminals, would be required southbound side of the carriageway.

Opportunity 3(k)

An uncontrolled pedestrian crossing could be installed across the private field access on the Existing A5, Doogary Road. Drop kerbs, tactile paving could be installed. This pedestrian crossing will link the footway identified in Opportunity 4(o).

Opportunity 4 (See Drawings D-019 to D-024): NMU Facilities & Greenways

Opportunity 4(a)

A shared use NMU facility could potentially be provided adjacent to the northbound carriageway. To achieve the acceptable minimum width of 3m (Table 7.3 of TA 90/05) a combination of carriageway and landowner boundaries realignment would be required.

The existing footway between the northbound carriageway and the boundary of the Old Nestle factory could be extended into the grounds of the old factory (circa. 1.2m x 140m). This approach would allow for the existing right turn lanes along this section of the A5 Beltany Road to be retained. The entrance to Donnell & Ellis Heavy Haulage yard opposite the Beltany Grove residential development could be retained, approximately 60m (x 3m) of new footway along the boundary of the yard would need to be constructed. A section of the boundary fence (circa. 40m) would need to be realigned and replaced, with 6no. road lighting columns relocated to the back of the new shared used facility.

The existing fence line and hedgerow along the boundary of the Old Nestle factory (circa. 140m) will need to be removed, realigned and replaced.

South of the entrance to the Donnell & Ellis Heavy Haulage yard, a combination of extending the footway into private land and carriageway realignment would be required. To achieve the acceptable minimum width of 3m for a shared NMU facility the right turn pocket into the AFBI Veterinary Laboratory / Free Presbyterian Church and central hatch, would need to be removed to cater for the footway being extended into the carriageway.

The beginning of the extension of the footway into the A5 Beltany Road would taper out from the line of the existing pedestrian refuge island at the start of the right turn lane (RTL) for Beltany Grove. This approach would require approximately 35m x 1.2m of additional land to be taken from the Donnell & Ellis Heavy Haulage yard to create a new 3m wide shared NMU facility.

Replacement boundary fences and hedge (circa. 140m total) would need to be provided for each of the affected properties. The carriageway drainage along the realigned section of carriageway would also need to be redesigned and replaced (circa. 120m).



Photograph 15: Northbound footway adjacent to A5 Beltany Road at the entrance to the Old Nestle Factory (See Drawing D-019)



Photograph 16: Northbound footway adjacent to A5 Beltany Road outside the Donnell & Ellis Heavy Haulage yard (See Drawing D-019)



Photograph 17: Existing entrance to the Donnell & Ellis Heavy Haulage yard to be retained (See Drawing D-019)



Photograph 18: Southbound footway extension at start of RTL into Beltany grove (See Drawing D-019)



Photograph 19: Southbound footway extension to the entrance to Omagh Rugby Club (See Drawing D-019)

Opportunity 4(b)

Discrete sections of the existing footway between Omagh Rugby Club (South of Beltany Grove) to Beltany Mews already appear to be >3m wide. However, a number of street lighting columns (9No.), utility poles (4No.) and a mini pillar are currently located within the footway. A cost benefit analysis would be required to assess whether extending the footway into the properties or into the carriageway would be more cost effective.

For the purpose of this assessment it shall be assumed that the footway would be extended into the carriageway. To achieve the acceptable minimum width of 3m for a shared NMU facility the northbound lane would be realigned by removing the central hatch. The existing footway would then be extended (approximately 1m x 330m) into the carriageway avoiding the need to purchase private land and negating the need to relocate services.

The carriageway drainage along the realigned section of carriageway would also need to be assessed and replaced, where required (circa. 330m length).



Photograph 20: Southbound view of A5 Beltany Road south of the entrance to the Omagh Rugby Club (See Drawing D-019)



Photograph 21: Southbound view of A5 Beltany Road north of the entrance to Beltany Mews (See Drawing D-019)

Opportunity 4(c)

A 440m long, 3m wide Shared Use NMU facility could be constructed along the southbound side of the Existing A5 carriageway. The grass verge along this stretch could be utilised for the construction of the facility. This 3m wide footway will link two existing footways.



Photograph 22: Southbound view of A5 Beltany Road north of the entrance to Beltany Mews (See Drawing D-019)

Opportunity 4(d)

The existing footway along the northbound side of the carriageway to a point north of the entrance to the Church of Jesus Christ of Latter Day Saints (circa. 220m x 1m). The existing footway could potentially be widened away from the carriageway. The carriageway drainage along the realigned section of carriageway would also need to be redesigned and replaced (circa. 220m).



Photograph 23: Southbound view of A5 Beltany Road south of the entrance to Beltany Mews (See Drawing D-019)

Opportunity 4(e)

NMU's would be directed to cross at the A5 Beltany Road at the new controlled crossing identified in Opportunity 3(c) and continue their journey on a widened offline path on the southbound side of the carriageway between Watson Park and Derry Road. The footway on either side of Watson Park could be widened into the verge to achieved the acceptable minimum width.



Photograph 24: Northbound view of A5 Beltany Road at entrance Watson Park (See Drawing D-019)



Photograph 25: Southbound offline footway between Watson Pk. & Derry Rd. from Watson Pk. (See Drawing D-019)



Photograph 26: Northbound view of offline footway between Watson Pk. & Derry Rd. from Derry Rd. (See Drawing D-019)

This approach would link the new shared use NMU facility into the proposed shared use NMU facility of the proposed Strathroy Link Road off Derry Road and provide an uninterrupted link for cyclists travelling to the Strule Shared Education Campus. 1No. feeder pillar and 1No. sign north of the entrance to Watson Park would need to be relocated to facilitate the widening of the footway.

Opportunity 4(f)

A link between A5 Great Northern Road south of Derry Road Roundabout / Gortrush Industrial Estate and the shared NMU facility described in Opportunity 4(e) and subsequently Strathroy Link Road could be achieved by providing a 3m Shared Used Facility between Derry Road and the A5 Great Northern Road.



Photograph 27: Cul de Sac between Derry Road and A5 Great Northern Road (South of the proposed Strathroy Link Road Roundabout) (See Drawing D-020)

As part of the Strathroy Link Road a toucan crossing will be provided immediately north of this cul de sac. A NMU facility would connect non motorised road users travelling on Derry Road with the A5 Great Northern Road opposite the northern entrance of the Gortrush Industrial Estate.



Photograph 28: Gap in Hedge – Photo Taken from A5 Great Northern Road (See Drawing D-020)

The gap in the hedge at the end of the cul de sac leads to a footway adjacent to the southbound carriageway on the A5 Great Northern Road.

As this is not a formalised connection between Derry Road and A5 Great Northern Road, it is not necessary to provide a full NMU Shared use facility, a simple sign and line options would be suitable.

Opportunity 4(g)

The Acceptable Minimum width of 3m for a shared used facility (quoted in Table 7.3 of TA 90/05) would need to be provided in the section between the facility link from Derry Road and the point where the footway moves offline.

The additional width required could be achieved by removing the mature hedgerow at the back of the footway and replacing it with a suitable fence. Additional land may be required to achieve the 3m width. For the purpose of any future costing it shall be assumed that 25m x 1m of private land would also be required.

Opportunity 4(h)

The offline footway / shared use facility which runs parallel to the A5 Great Northern Road for approximately 1.2km does not appear to be greater or equal to the Acceptable Minimum width of 3m for a Shared Use NUM facility. The section between the northern entrance to the Gortrush Industrial Estate and immediately north of the Brookmount Road junction with the A5 Great Northern Road runs through green spaces.

This opportunity highlights a potential 3.0m shared use NMU facility, over a length of 1140m. To achieve the unobstructed 3.0m width, various suggestions along the 1140m length are described below.

The existing shared use NMU facility does not appear to achieve the Acceptable Minimum width of 3m along its entire length. Discrete sections could be widened to achieve this standard. The following photos have been taken in the southbound direction starting opposite the northern entrance to the Gortrush Industrial Estate.



Photograph 29: Offline Footway adjacent to the A5 Great Northern Link (See Drawing D-020 and D-021)

It appears that the acceptable minimum width could be achieved along this section, approximately 115m by widening the existing footway on either side so that it is flush with the fence on either side. The existing footway could be widened to 3m along this section. Where additional space is required the fenceline between the carriageway and the existing footway could be re-aligned (assume 115m of new fence and hedge, 2m x 115m of general site clearance and 1m x 115m of new footway).



Photograph 30: Offline Footway adjacent to the A5 Great Northern Link (See Drawing D-020 & D-021)

The next section of the facility, approximately 300m in length adjacent to the public playground / playing field, appears to provide the acceptable minimum width for a shared NMU facility.



Photograph 31: Offline Footway adjacent to the A5 Great Northern Link (See Drawing D-021)

The next 560m between the entrance to the Gortrush Industrial Estate (southern entrance) / Omagh Business Park and the Brookmount Road. junction does not appear to meet the acceptable minimum width for a shared NMU facility.



Photographs 32, 33 & 34: Offline Footway adjacent to the A5 Great Northern Link (See Drawing D-021 for all)

The acceptable minimum width could be achieved along this section by widening the existing footway on either side so a total width of 3m is achieved. Where additional space is required the fenceline between the carriageway and the existing footway could be realigned (assume 560m of new fence and hedge, 2m x 560m of general site clearance and 1m x 560m of new footway).



Photograph 35: Offline Footway adjacent to the A5 Great Northern Link (See Drawing D-021)

There is a short pinch point approximately 35m in length, in the existing footway opposite the Brookmount Road junction where the footway passes between a high boundary wall and the edge of the A5 Great Northern Road. The wall appears to be retaining higher ground and passes within 5m of the property behind it. Realigning this wall does not appear to be an option as the associated cost would be amplified as any work would likely impact upon the foundations of the property behind it.



Photograph 36: Offline Footway adjacent to the A5 Great Northern Link (See Drawing D-021)

There is limited scope for extending the footway towards the carriageway as there appears to be <1m between the existing railing and the edge of carriageway kerblineline. The presence of the railing appears to be appropriate. There are a number of necessary items of existing street furniture (directional signage, marker post, street lighting columns) in the grass verge which cannot be moved closer to the edge of carriageway. Were the footway to be extended, these items would reduce the effective width of the footway.

The final 130m to the terminus at Brook Street appears to provide the acceptable minimum width for a shared NMU facility.

Opportunity 4(i)

A 100m long, 3m wide shared use NMU facility could be constructed along the right hand grass verge along the A5 on-slip. This facility will tie into an existing footway at the Brook Steet end of the on-slip, and meet a potential crossing point identified in Opportunity 3(f). The vehicle restraint system (VRS) along this 100m stretch may require repositioning to ensure a 3m width is achieved.



Photograph 37: Southbound on the A5 on-slip from Brook St (See Drawing D-022)

After crossing the existing A5, the 3m wide shared use facility could continue along the northbound side of the A5 carriageway utilising the wide grass verge.



Photograph 38: Southbound on the Existing A5, north of the A5 on-slip from Brook St (See Drawing D-022)

This facility would measure approximately 350m in length, and could potentially link an existing NMU facility close to Strule Park and Tamlaght Road.

Opportunity 4(j)

This new facility would be sign posted on the offline facility to direct NMUs travelling south on the A5 Great Northern Link or to Tamlaght Road to cross at this point. A new shared NMU facility would then be provided in the northbound grass verge and would run south towards the Tamlaght Road exit.

This new shared NMU facility would require approximately 100m x 3m of new footway. A number of items of street furniture (5No. road lighting columns, 3No. signs, 2No. masonry utility marker posts) would require relocation/redesign to accommodate the facility. The boundary hedge and trees could be replaced with a low fence to accommodate the new facility.

Opportunity 4(k)

This opportunity highlights a potential shared use NMU facility, approximately length 635m, linking Railway Terrace and Kelvin Avenue.

There is the potential to realign Great Northern Road between Railway Terrace junction and Kelvin Avenue / Kelvin Road junction to provide a new shared NMU facility. This is anticipated to be provided in the grass verge adjacent to the northbound carriageway. This could be achieved by reducing the number of current lanes from three to two by removing the northbound right turn lane onto Railway Terrace and the southbound left turn lane onto Kelvin Avenue / Road. This will reduce the overall width of the central hatch allowing for the additional space required to facilitate a shared NMU facility in this area.



Photograph 39: Southbound on the A5 Great Northern Link immediately south of the Railway Terrace / Tamlaght Rd. Junction (See Drawing D-022)



Photograph 40: Southbound on the A5 Great Northern Link at the Kelvin Ave. junction (See Drawing D-023)

Such an approach would require the construction of a new 3m wide shared NMU facility with a 1m grass separation strip. The existing carriageway drainage system on this side of the carriageway would need to be re-designed and constructed. The cost associated with temporary works for such a task would likely be significant, as would reconfiguring the signalised junction arrangement at each end of the opportunity.

Opportunity 4(L)

A 90m long, 3m wide shared use NMU facility could be constructed linking Dromore Retail Park and the A5, Great Northern Road. This facility could be constructed through Sacred Heart College's sports pitches/facilities. Land acquisition would be required for the construction of this facility.

Opportunity 4(m)

A new 3m wide shared use NMU facility could potentially be provided on the southbound side of the existing A5, Great Northern Road between the Kelvin Avenue / Kelvin Road junction and Dublin Road exit.

This could potentially be achieved by a combination of the construction of a new shared use NMU facility in the verge on the southbound side and the reassignment of part of the existing carriageway width at other locations over this length. The existing carriageway width consists of either three lanes on the approach to the Kelvin Avenue / Kelvin Road junction or one lane in each direction with curve widening present throughout.

Reassigning part of the carriageway width to NMUs would require an assessment of whether the elimination of curve widening would be acceptable in terms of driver safety. This opportunity could potentially also require the removal of a length of the northbound right turning lane to Kelvin Avenue requiring the reconfiguration of the signalised junction arrangement.



Photograph 41: Southbound on the A5 Great Northern Link immediately south of Kelvin Ave. / Rd. Junction (See Drawing D-023)



*Photograph 42: Southbound on the A5 Great Northern Link north of Dublin Rd. Exit
(See Drawing D-023)*



Photograph 43: Southbound on the A5 Great Northern Link immediately north of Dublin Road Exit (See Drawing D-023)

To construct this 3m wide shared use NMU facility, the length of the existing right turn lane would need to be reduced in length from 100m to 50m to facilitate widening of the verge towards the carriageway. This would also facilitate the inclusion of a 1m wide grass separated strip between the shared use facility and the carriageway.

Opportunity 4(n)

The terminus of the existing shared NMU facility on the A5 Great Northern Link would be extended by circa. 45m onto the A505 exit off the Crevenagh Road Roundabout. Approximately 65m of new VRS would also be required on the northbound side of the A505.

A potential Greenway route which utilises the disused railway line has been identified along Crevenagh Road. NMU access to this greenway can be provided via Crevenagh Road Roundabout, utilising Opportunities 4(o) and 4(n)/3(j). The greenway route continues in the Dungannon direction.

Opportunity 4(o)

A new shared NMU facility could be provided in the southbound verge of the A5 Doogary Road exit off the Crevenagh Road Roundabout. The new facility could be provided as far as a point approximately 850m south of the roundabout, which has been identified for development. A ghost-island junction has been constructed to facilitate access to this area for vehicular traffic; provision of this potential NMU facility would provide NMU access.



Photographs 44, 45 & 46: Southbound on A5 Doogary Road (See Drawing D-024 for all)

This opportunity would entail the provision of at least a 3m (min.) wide shared NMU facility with a 1.5m separation strip.

The roadside topography is such that the opportunity would likely require structural earthworks as existing slopes and embankment may need to be altered.

The existing carriageway drainage along this section would need to be redesigned to cater for the additional area of hardstanding and the redefined edge of carriageway (i.e. there would now be a kerb line rather than over the edge drainage).

The new shared use NMU facility would pass behind the existing VRS between the Crevenagh Road. exit off the A505 and the A5 exit of the Crevenagh Road. Roundabout.

A potential greenway route which utilises the disused railway line has been identified along Crevenagh Road. NMU access to this greenway can be provided via Crevenagh Road Roundabout, utilising Opportunities 4(o) and 4(n)/3(j). The greenway route continues in the Dungannon direction. See drawing D-024.

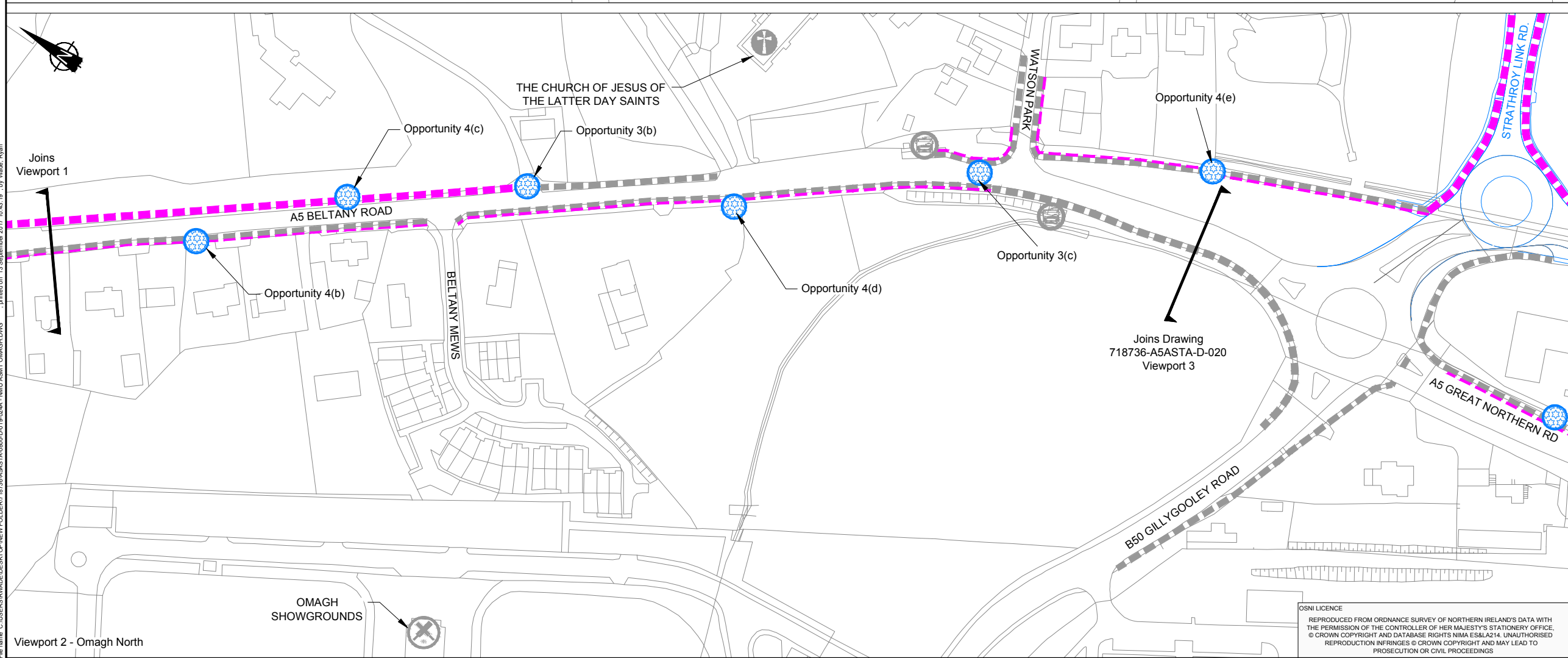
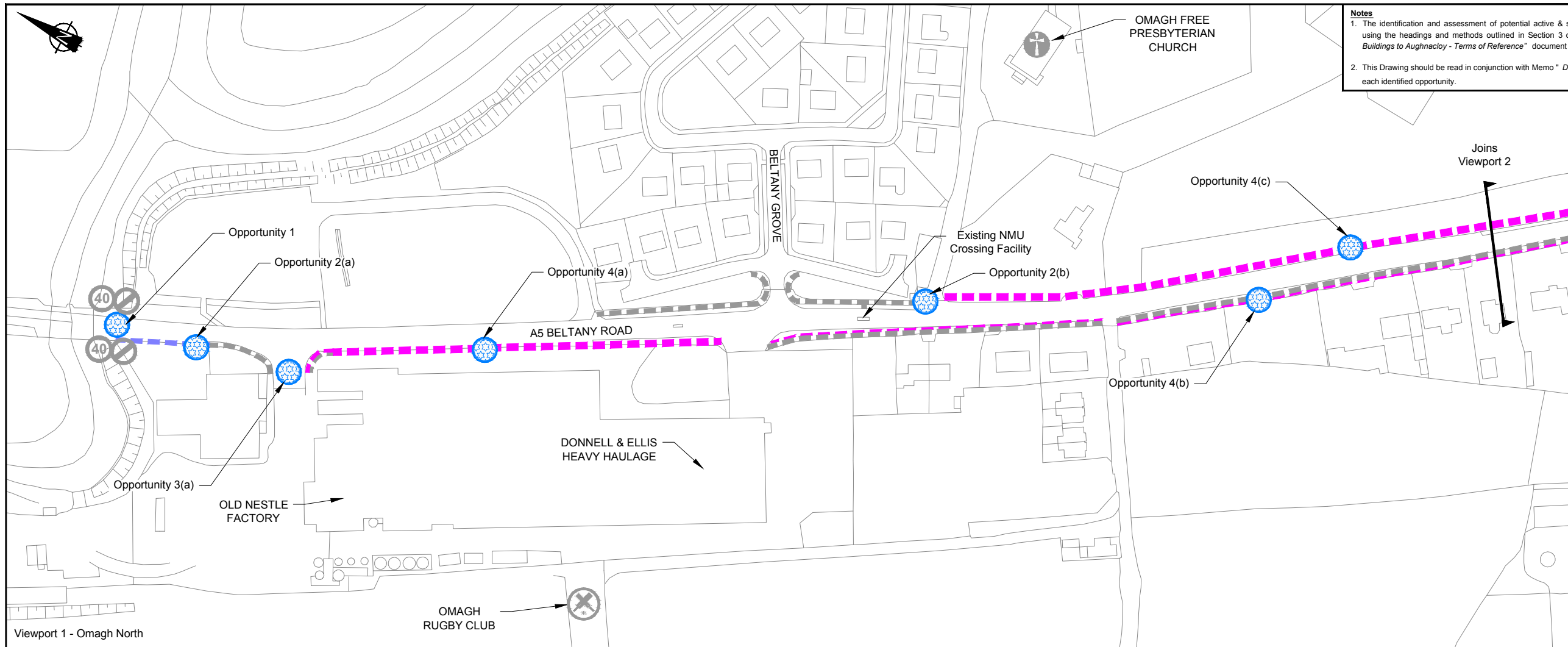
Northern Ireland Road Safety Strategy to 2020

The Northern Ireland Road Safety Strategy to 2020 identifies a number of measures that could be implemented to make work safer, one of which likely "...considering the applicability of urban speed reduction initiatives and assessing the potential for wider introduction of 20mph limits in residential areas and other urban areas where there is a significant presence of vulnerable road users."

The existing A5 could be described as an internal ring road around Omagh town centre. There are no schools / churches / etc. immediately adjacent to the A5 as it passes through Omagh, therefore it is not deemed necessary to propose the implementation of a 20mph speed limit on the A5 in Omagh or on immediately adjacent roads.

Park and Ride (P&R) Facilities

A Park and Ride facility is provided in the vicinity of Crevenagh, Omagh with capacity for approximately 250 spaces. During the course of this assessment, Translink has been consulted and have not identified further locations for a Park and Ride Facility within Omagh.



Notes

1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).

2. This Drawing should be read in conjunction with Memo "DM15 - A5 - Active & Sustainable Transport Assessment: Omagh" which will explain each identified opportunity.

Key

Existing Asset Features

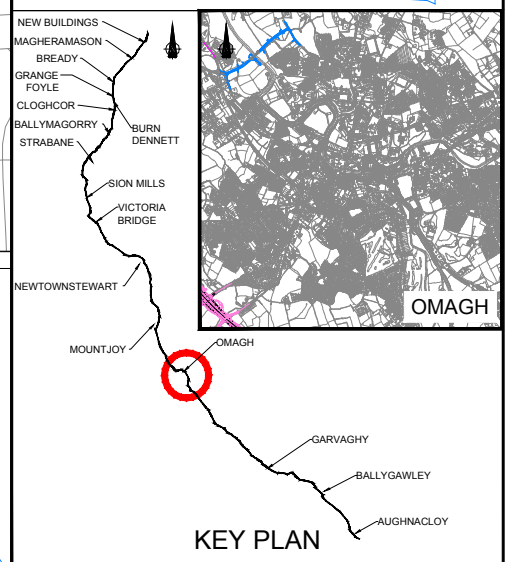
- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme
- Proposed Strathroy Link Road



DRAWING STATUS: FOR ISSUE

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SITE/PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy **A5WTC**

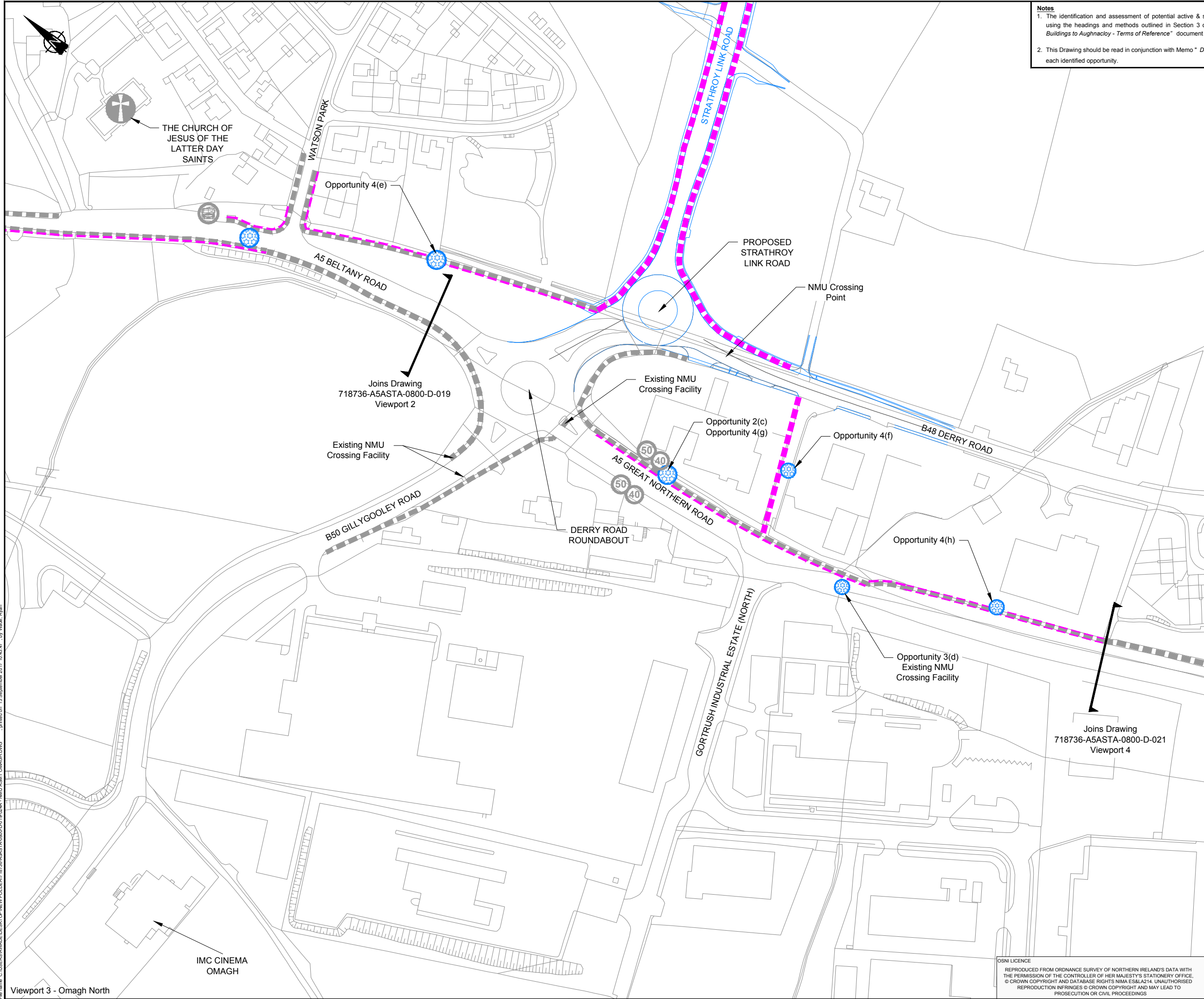
TITLE: ASSESSMENTS OF URBAN AREAS
LOCATION: OMAGH, CO. TYRONE
SHEET 1 OF 6

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DATE: 14/08/2017

DRAWING NO: 718736-A5ASTA-0800-D-019 REV: A

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Notes

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- This Drawing should be read in conjunction with Memo "DM15 - A5 - Active & Sustainable Transport Assessment: Omagh" which will explain each identified opportunity.

Key

Existing Asset Features

- NMU Facility: [Symbol]
- Signage: [Symbol] 50, [Symbol] 40, [Symbol] 30
- School: [Symbol]
- Residential Care Homes: [Symbol] (RH)
- Bus Stop: [Symbol]
- Sports / Cricket Club: [Symbol]
- Church: [Symbol]
- Health Care: [Symbol] (HC)

Proposed Improvements

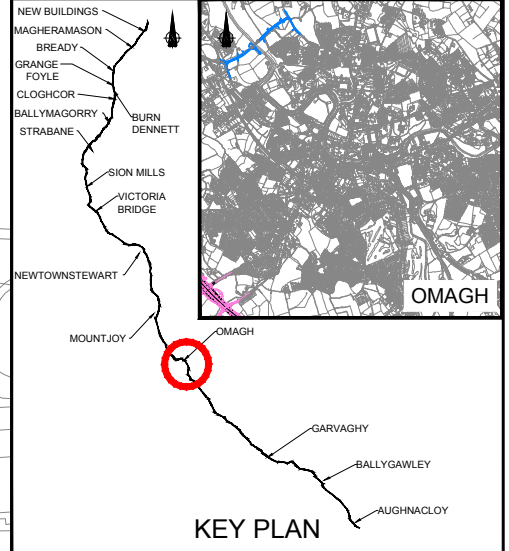
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- 3m Wide (Shared Use) NMU Facility: [Symbol]
- Existing NMU Facility to be Widened: [Symbol]
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- Potential Greenway Route: [Symbol]
- Highway Realignment with new Shared Use NMU Facility: [Symbol]

Opportunity Described in Memo

- [Symbol]

Other Features

- Proposed A5WTC Scheme: [Symbol]
- Proposed Strathroy Link Road: [Symbol]



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PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy

TITLE: ASSESSMENTS OF URBAN AREAS
LOCATION: OMAGH, CO. TYRONE
SHEET 2 OF 6

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DATE: 14/08/2017

DRAWING NO: 718736-A5ASTA-0800-D-020	REV: A
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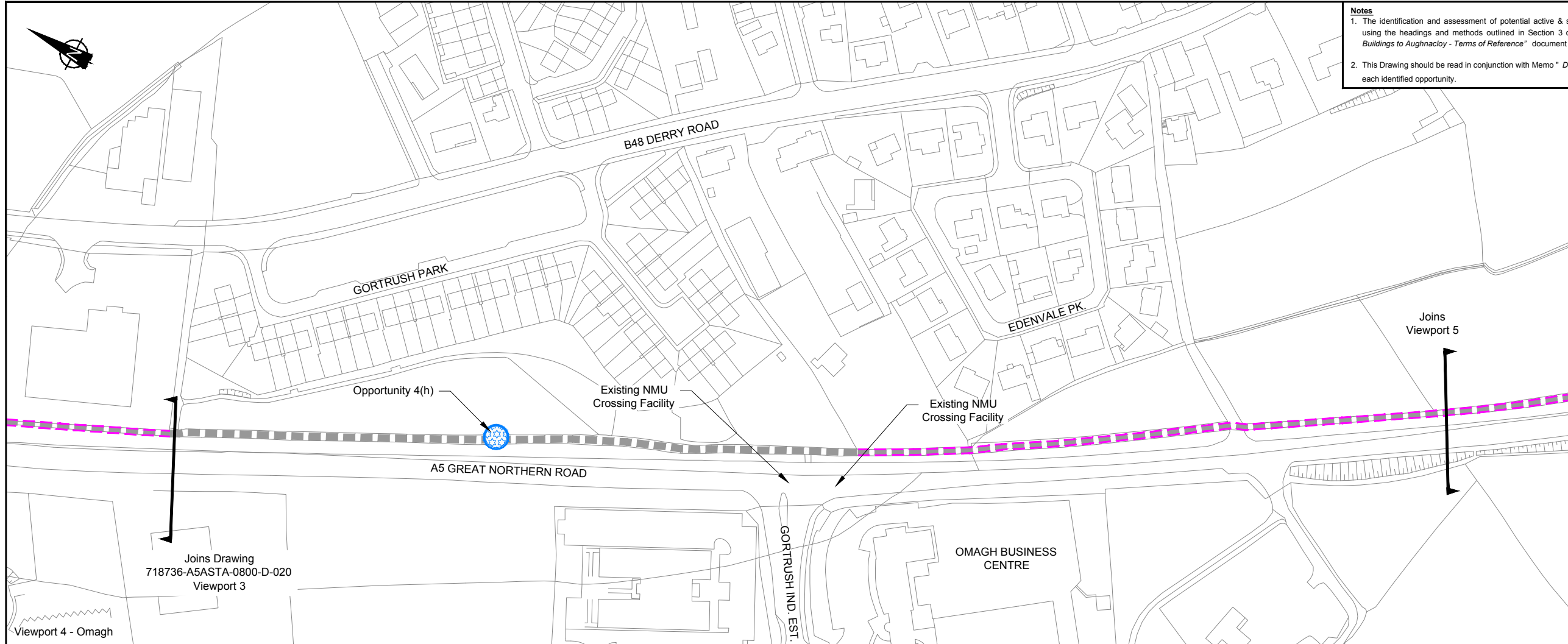
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Viewport 3 - Omagh North

IMC CINEMA OMAGH

Joins Drawing 718736-A5ASTA-0800-D-019 Viewport 2

Joins Drawing 718736-A5ASTA-0800-D-021 Viewport 4



Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
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Key

Existing Asset Features

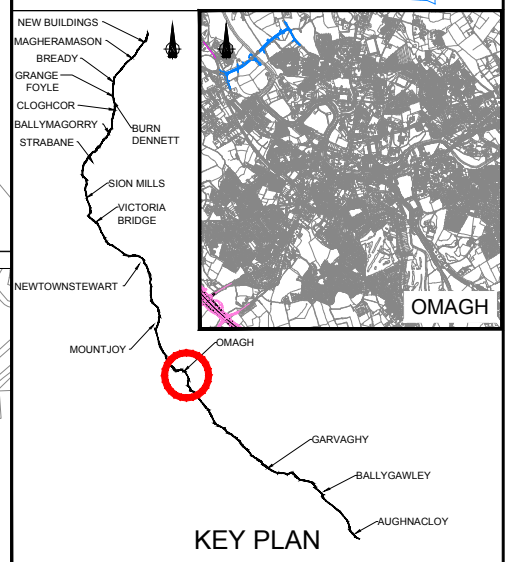
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- School: [Icon]
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- Bus Stop: [Icon]
- Sports / Cricket Club: [Icon]
- Church: [Icon]
- Health Care (HC): [Icon]

Proposed Improvements

- 2m Wide NMU Facility: [Icon]
- 3m Wide (Shared Use) NMU Facility: [Icon]
- Existing NMU Facility to be Widened: [Icon]
- Existing NMU Facility to be Widened (via Potential Greenway): [Icon]
- Potential Greenway Route: [Icon]
- Highway Realignment with new Shared Use NMU Facility: [Icon]
- Opportunity Described in Memo: [Icon]

Other Features

- Proposed A5WTC Scheme: [Icon]
- Proposed Strathroy Link Road: [Icon]



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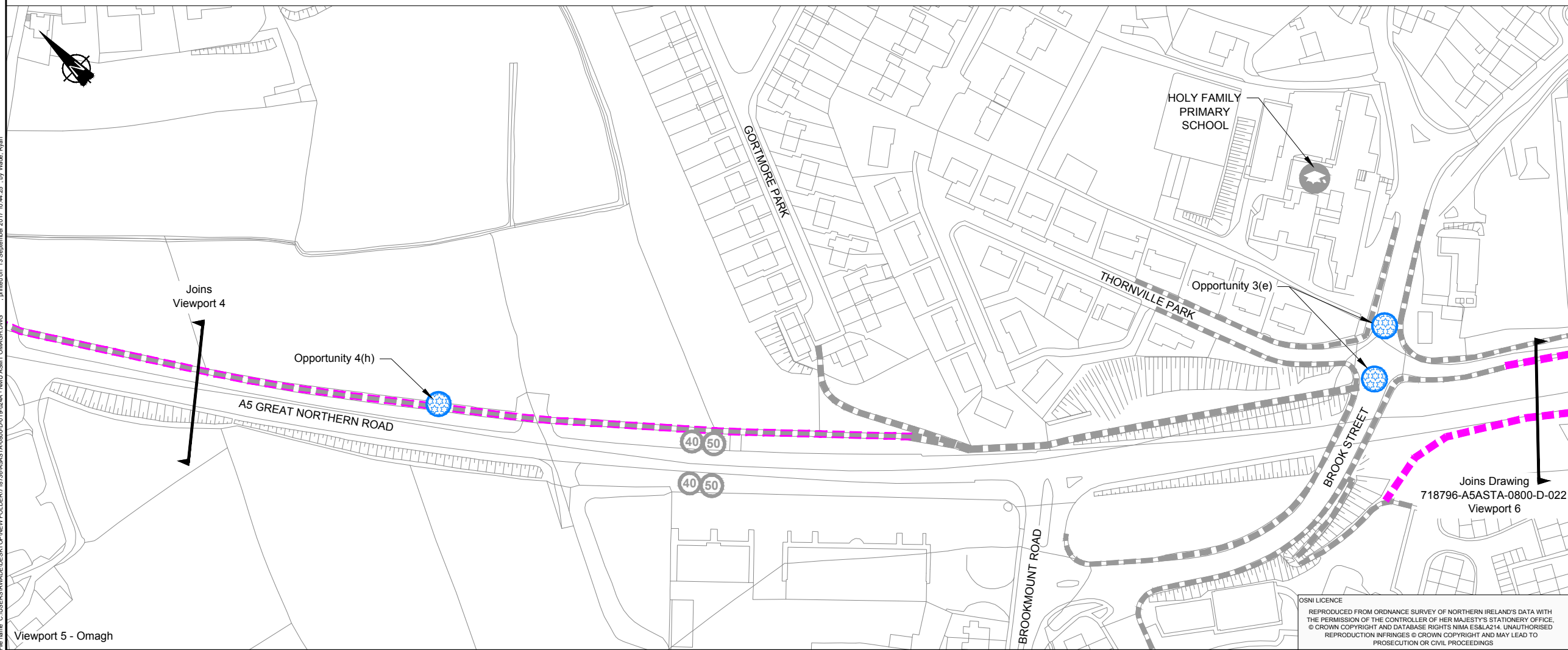
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SITE PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy **A5WTC**

TITLE: ASSESSMENTS OF URBAN AREAS LOCATION: OMAGH, CO. TYRONE SHEET 3 OF 6

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DATE: 14/08/2017
DRAWING NO: 718736-A5ASTA-0800-D-021		REV: A



Joins Drawing 718736-A5ASTA-0800-D-020 Viewport 3

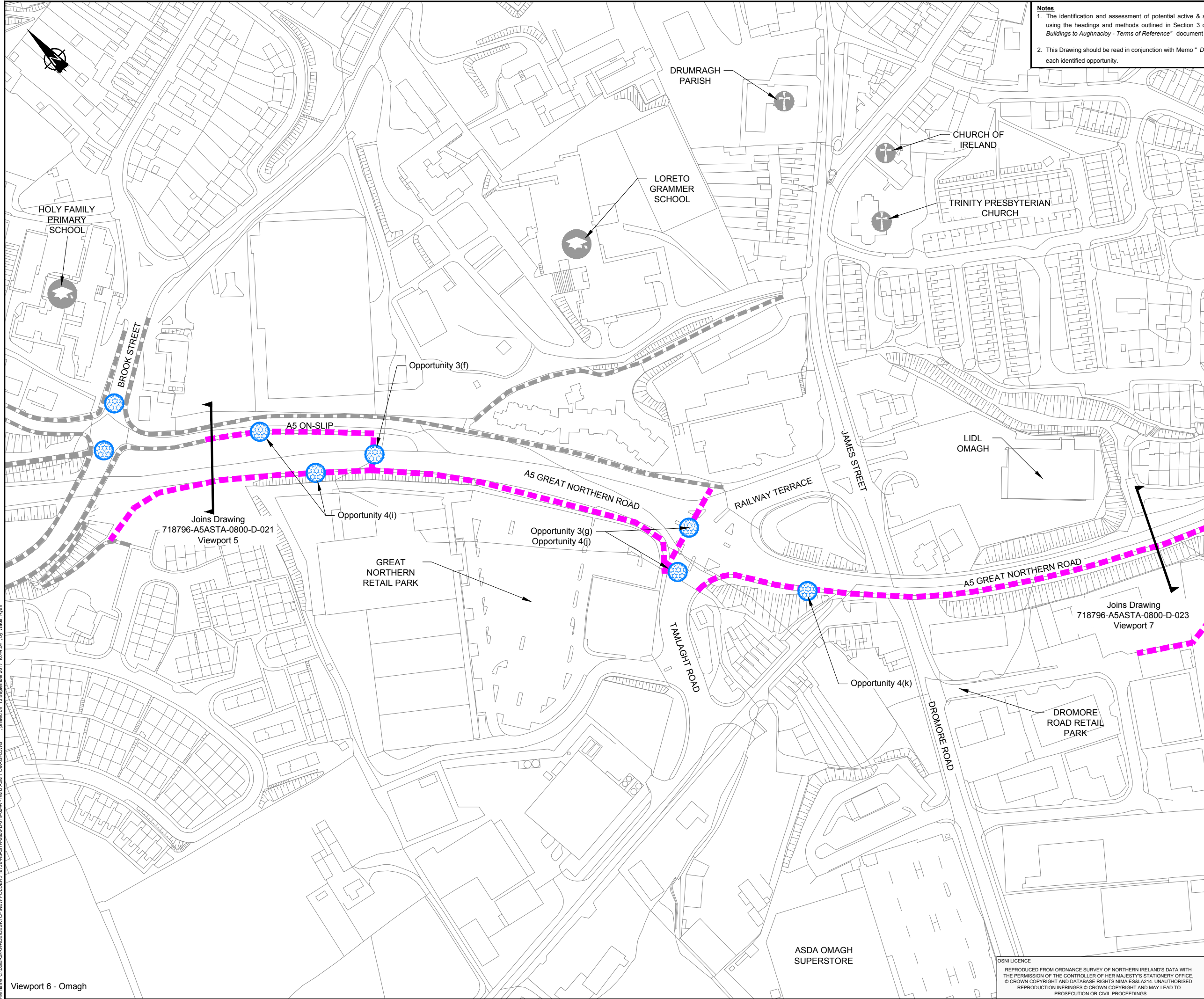
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Joins Drawing 718736-A5ASTA-0800-D-021 Viewport 4

Joins Drawing 718796-A5ASTA-0800-D-022 Viewport 6

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Key

Existing Asset Features

- NMU Facility: [Icon]
- Signage: [Icons for 50, 40, 30]
- School: [Icon]
- Residential Care Homes (RH): [Icon]
- Bus Stop: [Icon]
- Sports / Cricket Club: [Icon]
- Church: [Icon]
- Health Care (HC): [Icon]

Proposed Improvements

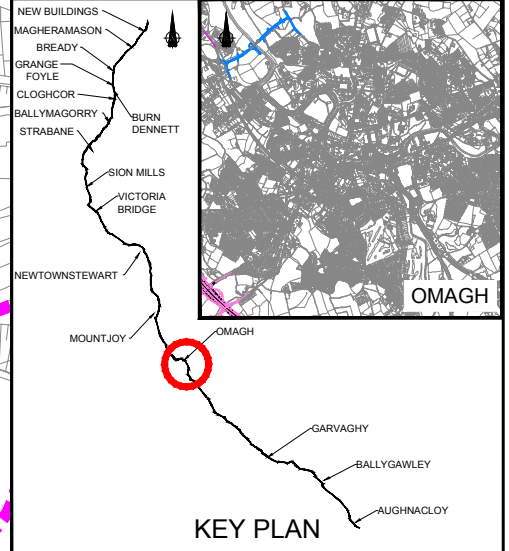
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- Existing NMU Facility to be Widened (via Potential Greenway): [Icon]
- Potential Greenway Route: [Icon]
- Highway Realignment with new Shared Use NMU Facility: [Icon]

Opportunity Described in Memo

- [Icon]

Other Features

- Proposed A5WTC Scheme: [Icon]
- Proposed Strathroy Link Road: [Icon]



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Viewport 6 - Omagh

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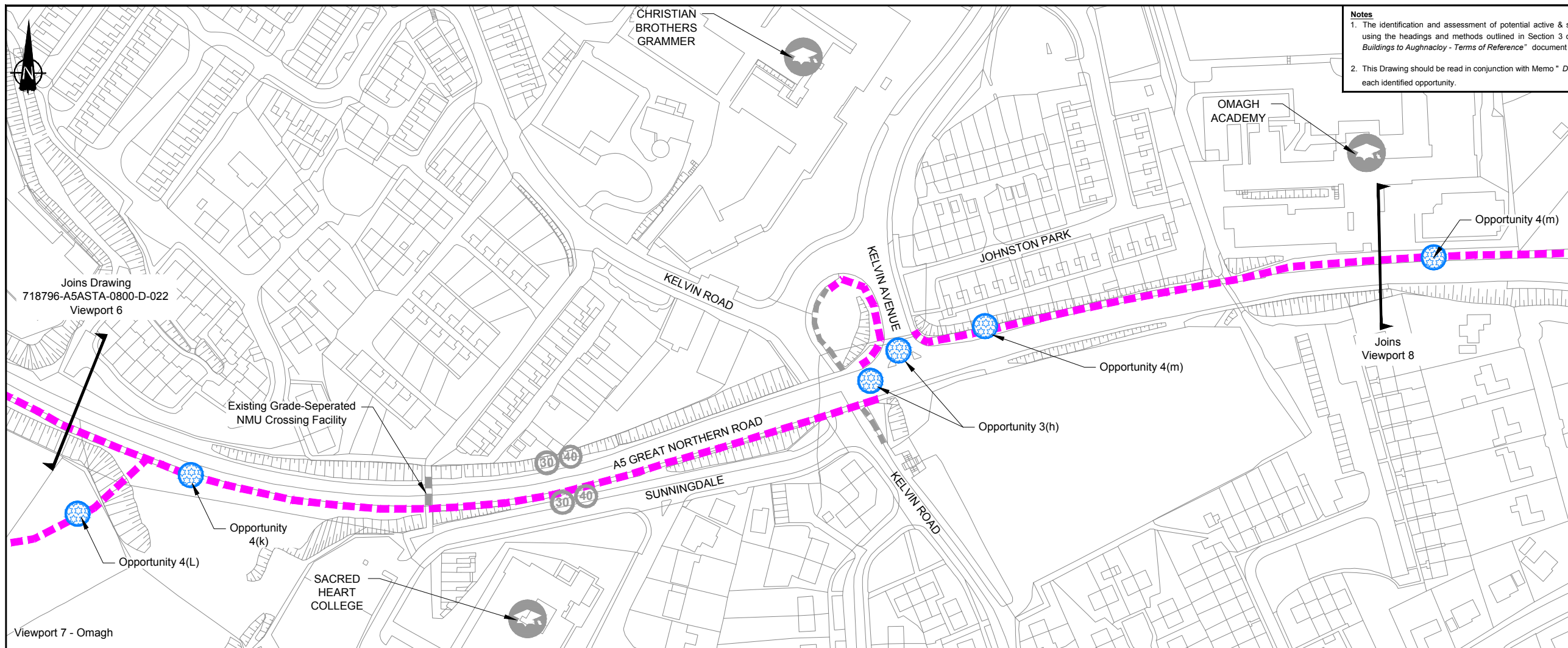
SITE/PROJECT: **Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy**

TITLE: **ASSESSMENTS OF URBAN AREAS LOCATION: OMAGH, CO. TYRONE SHEET 4 OF 6**

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DRAWING NO: **718736-A5ASTA-0800-D-022** REV: **A**

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Key

Existing Asset Features

- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

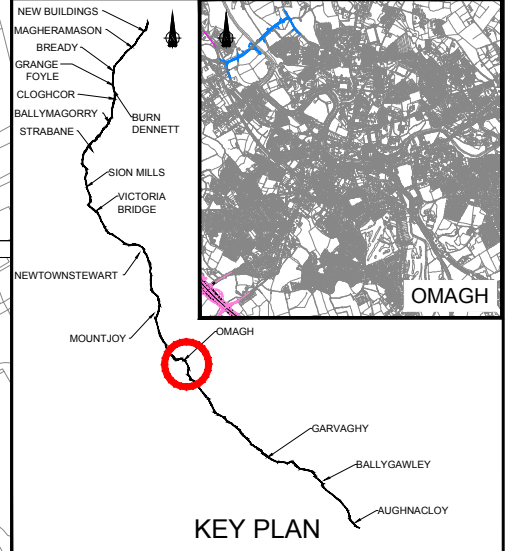
Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility

Opportunity Described in Memo

Other Features

- Proposed A5WTC Scheme
- Proposed Strathroy Link Road



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A5WTC

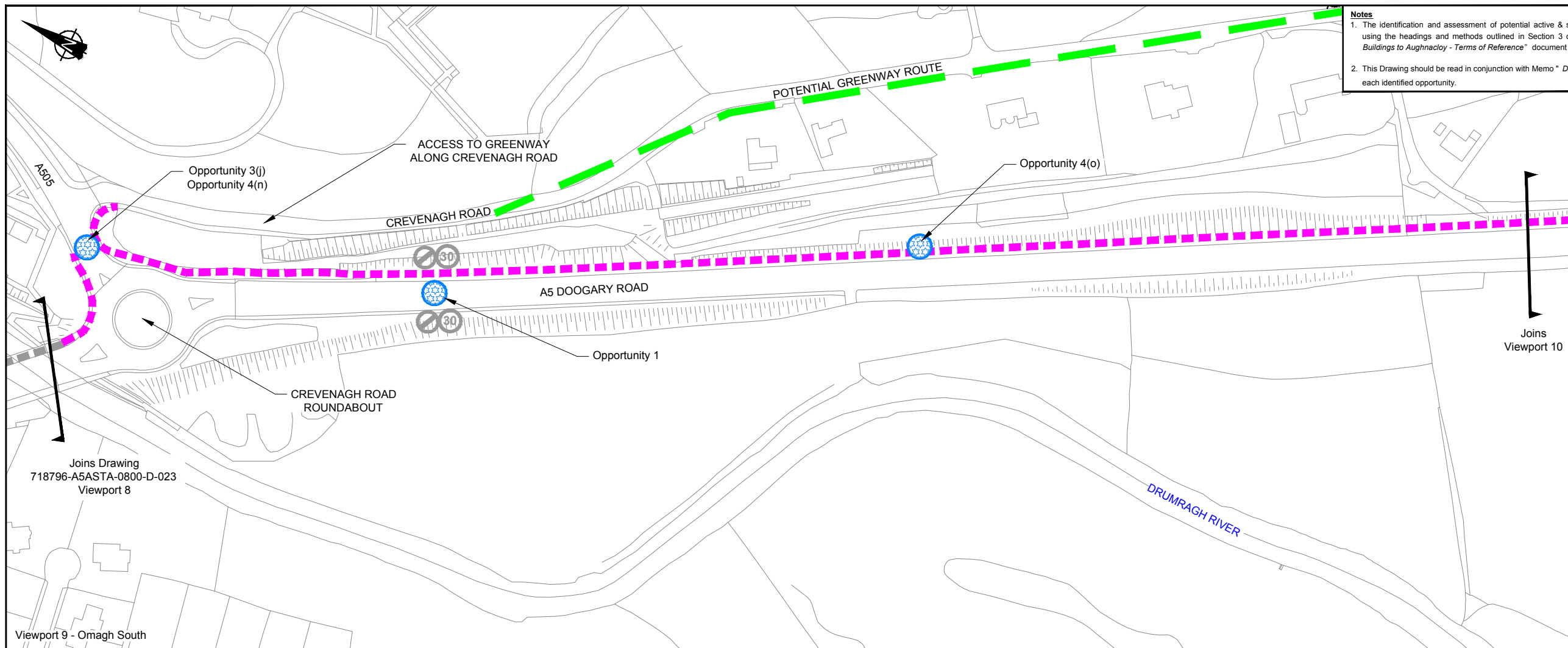
TITLE: ASSESSMENTS OF URBAN AREAS
LOCATION: OMAGH, CO. TYRONE
SHEET 5 OF 6

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
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 2. This Drawing should be read in conjunction with Memo "DM14 - A5 - Active & Sustainable Transport Assessment: Omagh" which will explain each identified opportunity.

Key

Existing Asset Features

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- Signage: [Symbol] 50 [Symbol] 40 [Symbol] 30
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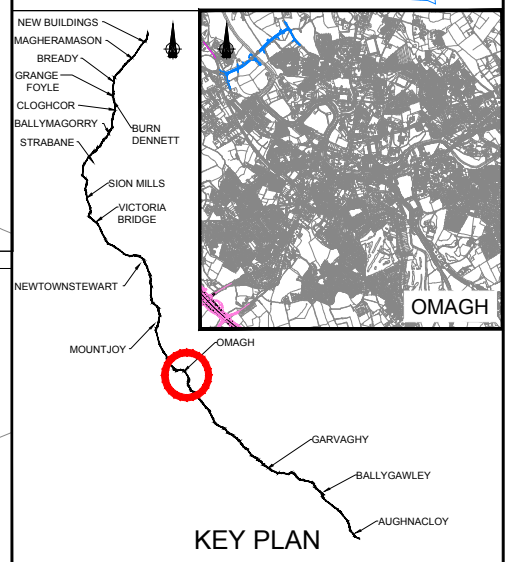
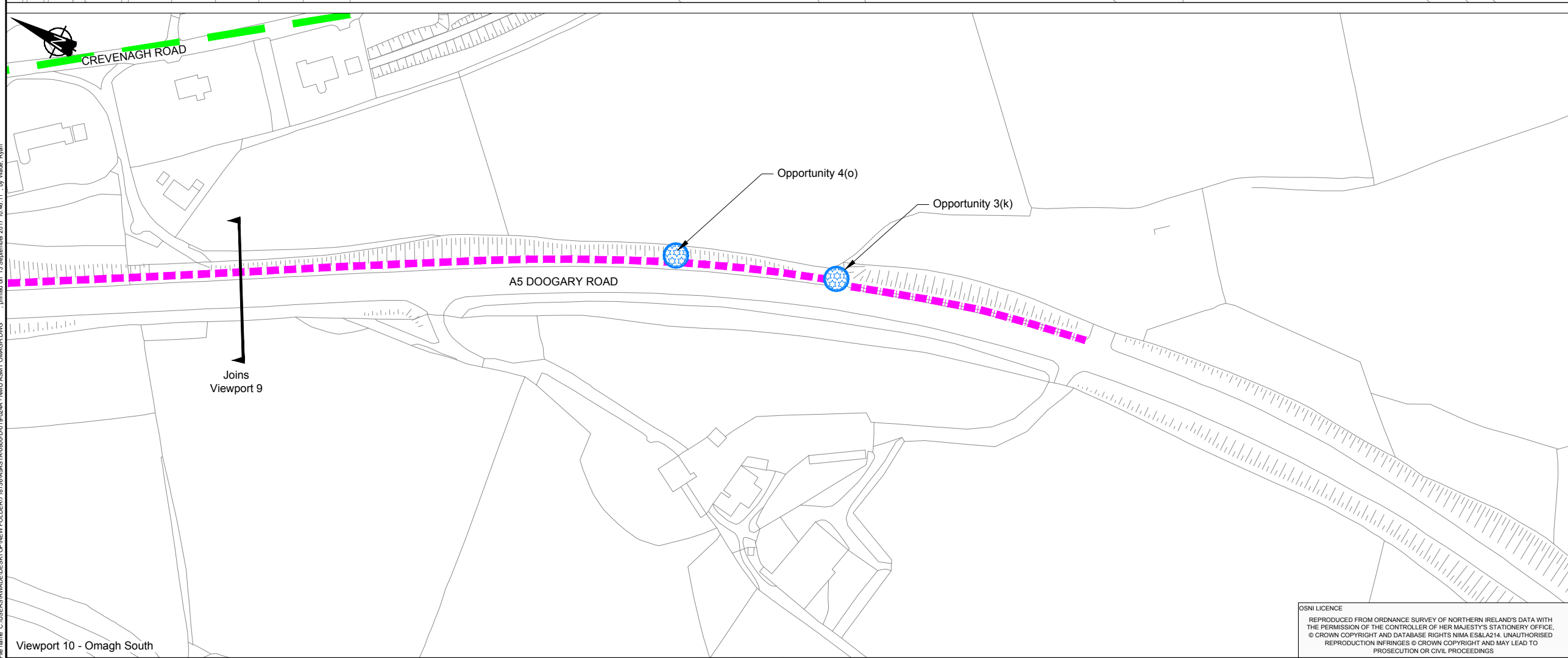
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- 2m Wide NMU Facility: [Symbol]
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- Existing NMU Facility to be Widened: [Symbol]
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- Potential Greenway Route: [Symbol]
- Highway Realignment with new Shared Use NMU Facility: [Symbol]

Opportunity Described in Memo: [Symbol]

Other Features

- Proposed A5WTC Scheme: [Symbol]
- Proposed Strathroy Link Road: [Symbol]



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STEP/PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy **A5WTC**

TITLE: ASSESSMENTS OF URBAN AREAS
 LOCATION: OMAGH, CO. TYRONE
 SHEET 6 OF 6

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DRAWN: PM
		DATE: 14/08/2017

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DM16 – A5 Active & Sustainable Transport Assessment: Garvaghy

Description

The identification and assessment of potential active & sustainable transport opportunities in Garvaghy, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo should be read in conjunction with drawings: 718736-A5ASTA-0800-D-033 ‘Assessments of Urban Areas, Location: Garvaghy, Co. Tyrone’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic.

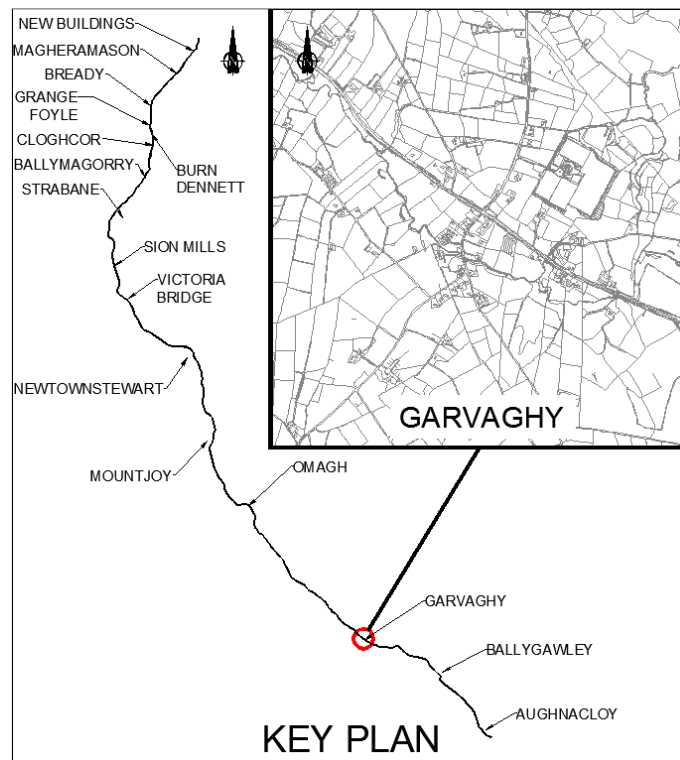


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 through Garvaghy are at a relatively good standard. There is an existing footway present on both sides of the existing A5 through Garvaghy except for a short length of approximately 150m adjacent to the southbound direction of travel opposite the church and cemetery. Moreover, considerable lengths of the footways on both sides are segregated from the existing A5 by a grass verge. There are limited NMU crossing opportunities in Garvaghy.

Existing Parking Facilities (see Drawing D-033)

There are ample parking opportunities along the existing A5 through Garvaghy. There is a car park directly opposite Kelly's Inn. With regards to attractors along the A5 Curr Road / Omagh Road, Kelly's Inn, St Matthew's Church, St Matthew's Primary School and the petrol filling station all have their own parking facilities.



Photograph 1: Existing parking facilities opposite Kelly's Inn (see drawings D-033)

Existing Speed Limit Reduction from NSL to 50mph (see Drawing D-033)

A reduced speed limit of 50mph has been introduced through Garvaghy, for a length of approximately 0.95km. Opportunity 1 identifies a potential speed limit reduction from 50mph to 40mph for a length of 0.52km.

The Design Manual for Roads and Bridges (DMRB) recommends that where appropriate, the use of 30mph limits in villages is encouraged. In such villages, traffic calming or traffic management measures are likely to be needed to help enforce the limit. Most traffic calming schemes will consist of a combination of physical and non-physical measures and as far as practicable schemes with speed reductions introduced should be self-enforcing. Such schemes would be subject to detailed assessments carried out by the Departments Traffic Management and Network Development section and the Police Service of Northern Ireland (PSNI) traffic section.

Existing NMU Crossing Facilities (see Drawings D-033)

There is one existing NMU crossing facility that traverses the existing A5 Curr Road at the petrol filling station. Drop kerbs and tactile paving appear to be present. See Opportunity 2(d) for potential improvements to this existing NMU crossing facility.



Photograph 2: Uncontrolled NMU Crossing (see drawing D-033): Petrol Filling Station, A5 Curr Road

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this design memo discuss potential improvements to the two gateway entrances to Garvaghy on the existing A5, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5, potential improvements to existing bus stop facilities on the existing A5, and the potential reduction in speed limit on the Existing A5 through Garvaghy.

Opportunity 1 (see Drawing D-033): Village Gateway signage

Gateway signage could be added to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements; the two images below illustrate the existing gateway features.

A reduction in speed limit from 50mph to 40mph through Garvaghy (for an approximate length of 0.52km) may benefit the safety of NMUs.



Photograph 3: Southbound entrance to Garvaghy (see drawing D-033)



Photograph 4: Northbound entrance to Garvaghy (see drawing D-033)

Existing speed limit signs on the Existing A5, Rosscavey Road, Rarogan Road and Radergan Road would need to be modified to display the revised speed limit of 40mph.

Opportunity 2 (see Drawing D-033): Pedestrian Crossing facilities

Opportunity 2(a)

Provide an uncontrolled NMU crossing across the Existing A5 approximately 110m north of Radergan Road's junction with the Existing A5 (with drop kerbs, tactile paving and potentially a pedestrian refuge island located in the central reserve hatching). This crossing facility will cater the existing northbound and southbound bus stops.



Photograph 5: Southbound on A5 Curr Road, North of Radergan Road (see drawing D-033)

Opportunity 2(b)

Provide an uncontrolled NMU crossing across the junction where Rarogan Road meets the Existing A5.

Opportunity 2(c)

Provide an uncontrolled NMU crossing across the junction where Radergan Road meets the Existing A5. This NMU crossing will form part of the potential footway network identified in Opportunities 4(a) and 4(d).

Opportunity 2(d)

Upgrade the existing NMU crossing across at the petrol filling station. Drop kerbs and tactile paving are currently present, however a pedestrian refuge island located in the central reserve hatching could potentially be added. This crossing will cater the existing southbound bus stop and car park users. This crossing with Central Island would be provided in combination with the reduction of speed limit outlined in Opportunity 1.

Opportunity 3 (see Drawing D-033) Bus Stop Facilities

Bus stops have been provided in three locations along the Existing A5 (two catering for the southbound direction and one for northbound).

The southbound bus stops do not have shelters, however the northbound bus stop does.

A shelter could potentially be installed at both southbound bus stops.

Opportunity 3(a)

A shelter could potentially be installed at the southbound bus stop. The southbound bus stop is also on-line; to alert drivers' attention to the presence of the on-line bus stop red bus bay road markings could be provided.

Opportunity 3(b)

The northbound off-line bus stop shelter is positioned south of the actual bus stop. The shelter could potentially be relocated approximately 50m north from its current location. So it is in the middle of the existing bus stop layby.

Opportunity 3(c)

A shelter could potentially be installed at the southbound bus stop.

Opportunity 4 (see Drawing D-033): Footway Improvements

Opportunity 4(a)

A 175m long, 2.0m wide NMU facility could potentially be provided linking Rosscavey Road crossing point identified in Opportunity 2(c) to the southbound bus stop. This NMU footway facility will utilise the existing wide grass verge adjacent the southbound carriageway.



Photograph 6: Southbound on A5 Curr Road north of Radergan Road junction (see drawing D-033)

Opportunity 4(b)

A 65m long, 2.0m wide NMU facility could potentially be provided linking the existing footway outside St. Matthew's Church to the repositioned northbound bus stop Opportunity 3(b) and the potential NMU crossing point identified Opportunity 2(a). This NMU footway facility will utilise the existing grass verge adjacent to the northbound carriageway; the layby could potentially be utilised for footway construction.

Opportunity 4(c)

A 65m long, 2.0m wide NMU facility could potentially be provided linking the existing footway outside St. Matthew's Primary School to the existing footway located outside St Matthew's Church (on Rarogan Road). This footway will cater for NMUs travelling to and

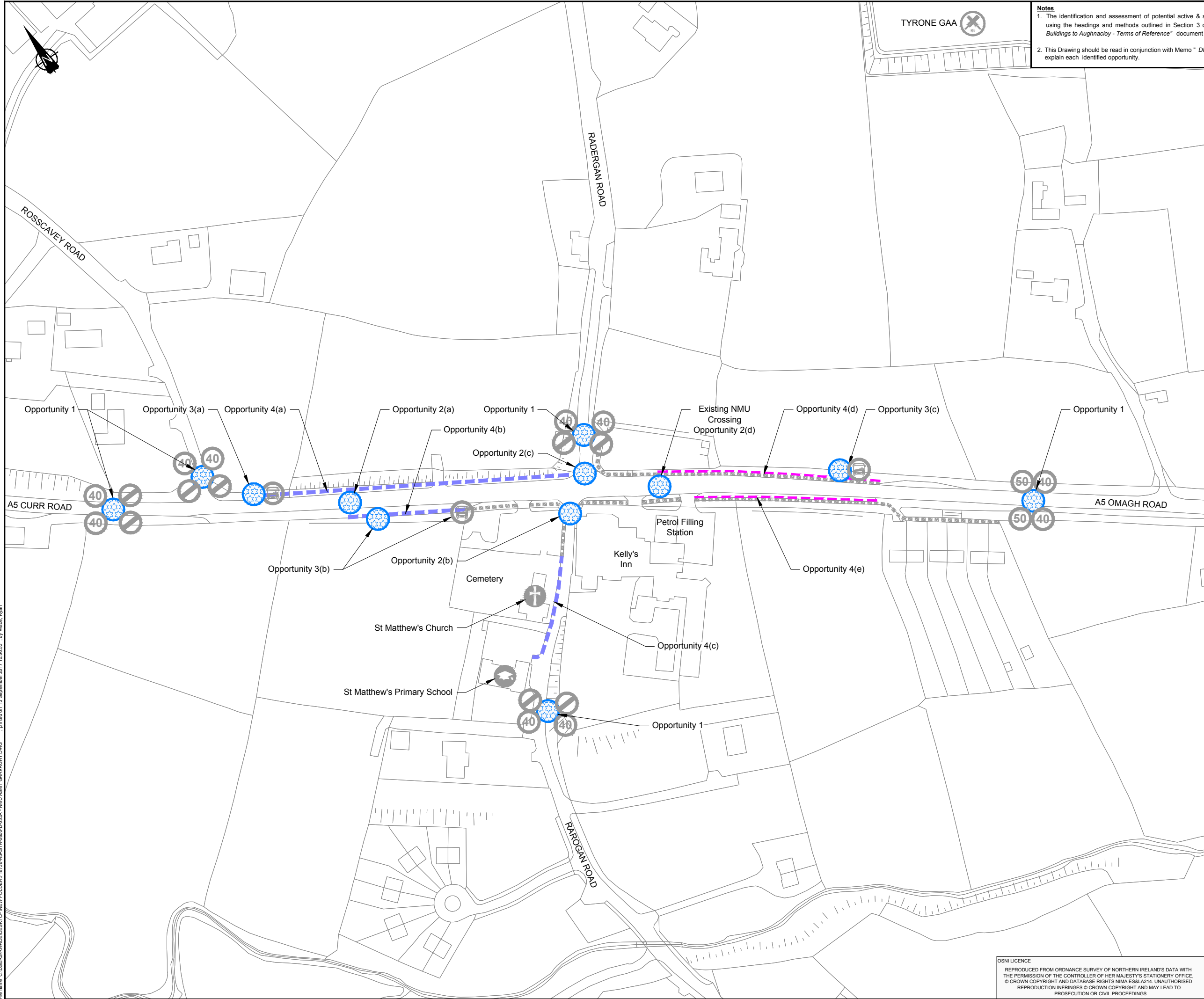
from the primary school and the church. To provide continuity, the new footway shown be positioned on the northbound side of Rarogan Road. The length next to the church is reduced in width, and to avoid having to acquire land from the church, the opposite verge could be reduced. Appropriate road marking would benefit the layout at this location.

Opportunity 4(d)

The existing footway adjacent the existing A5 southbound carriageway could potentially be widened to 3m for an approximate length of 125m. This NMU facility will link the crossing point identified in Opportunity 2(d) to the existing southbound bus stop.

Opportunity 4(e)

The existing footway adjacent the existing A5 northbound carriageway could potentially be widened to 3m for an approximate length of 105m. This NMU facility will link the petrol filling station / crossing point identified in Opportunity 2(d) to the residential properties located at the southern end of Garvaghy.



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Key

Existing Asset Features

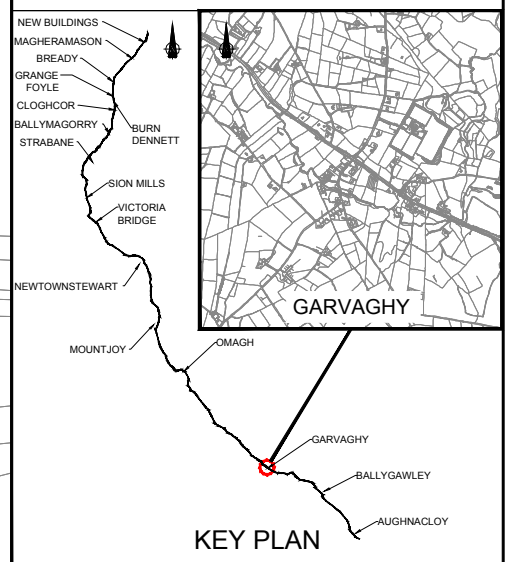
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- Signage: [Icons: 50, 40, 30]
- School: [Icon]
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Proposed Improvements

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- 3m Wide (Shared Use) NMU Facility: [Icon]
- Existing NMU Facility to be Widened: [Icon]
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- Potential Greenway Route: [Icon]
- Highway Realignment with new Shared Use NMU Facility: [Icon]
- Opportunity Described in Memo: [Icon]

Other Features

- Proposed A5WTC Scheme: [Icon]



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SITE/PROJECT: **Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy**

TITLE: **ASSESSMENTS OF URBAN AREAS LOCATION: GARVAGHY, CO. TYRONE SHEET 1 OF 1**

SCALE @ A1: 1:1000	CHECKED: MPH	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DATE: 14/08/2017

DRAWING NO: **718736-A5ASTA-0800-D-033** REV: **A**

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DM17 – A5 Active & Sustainable Transport Assessment: Ballygawley

Description

The identification and assessment of potential active & sustainable transport opportunities in Ballygawley, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “*Masterplan for Active and Sustainable Transport Opportunities: A5 – New Buildings to Aughnacloy - Terms of Reference*” document (WSP Ref. 1058654/RP/001).

This memo should be read in conjunction with Drawings: 718736-A5ASTA-0800-D-027 to 028 ‘Assessments of Urban Areas, Location: Ballygawley, Co. Tyrone’.

The [blue arrows](#) in the photos below indicate the direction of northbound traffic on the A5.

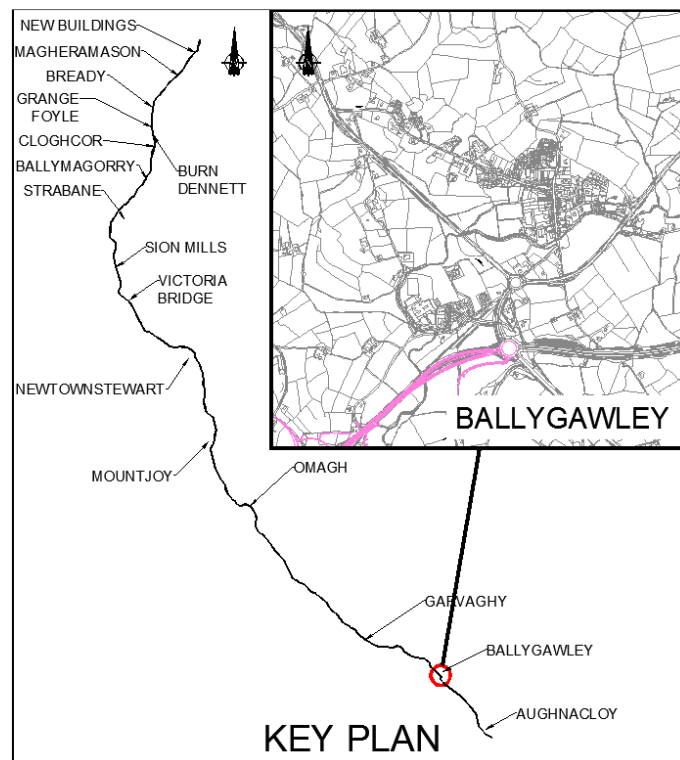


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 through Ballygawley are of a reasonable standard including discrete sections of footway adjacent to the southbound carriageway of the existing A5, as it passes Ballygawley.

Existing NMU Crossing Facilities (See Drawing D-028)

There are uncontrolled crossing facilities at the A5 Omagh Road / A5 Annaghilla Road roundabout. There are no formal crossing points in the vicinity of the Park and Ride facility on Tullybryan Road.

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Ballygawley, potential footway improvements, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5 and potential Greenway facilities that could be introduced.

Footway improvements include the widening of an existing footway north of the Old Omagh Road on the A5 to create a 3m wide shared use NMU facility, the creation of a 3m wide facility on the existing A5 between the Old Omagh Road and the A5 Omagh Road/ Grange Road and Armaghlughey Road in the existing verge, and the creation of a 3m wide facility on Remeen by widening the existing footway that will connect to the proposed greenway network.

Opportunity 1 (See drawings D-027 and D-028): Village Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds. However, the signage at both ends of Ballygawley are already to a good standard.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements; the image below illustrates the existing gateway feature.



Photograph 1: Opportunity 1 Northern Entrance to Ballygawley (See Drawing D-027)

Opportunity 2 (See Drawings D-027 and D-028): Footway Improvements

Opportunity 2(a)

Widening existing 2m wide (approx.) footway adjacent to the southbound carriageway of the A5 to 3m will create a Shared use NMU facility of length 140m.



*Photograph 2: Shared use NMU facility along A5 (Northern Entrance to Ballygawley)
(See Drawing D-027)*

Opportunity 2(b)

From the junction of the Old Omagh Road with the existing A5 on the southbound carriageway towards Ballygawley, there is an existing footway (estimated 1m-1.5m wide) for a distance of approximately 165m. It has been assessed that there is adequate verge space to allow for the existing footway to be widened. Due to the narrow width of the existing carriageway, it would not be advisable to extend the footway into the carriageway. Beyond this point there are a number of private dwellings which would result in the requirement for vesting, realignment of access and the relocation of existing lighting columns.

The footway adjacent to the existing car sales business on the southbound carriageway makes the footway continuing on this side unrealistic, due to the restrictions imposed by the ownership boundary. A crossing would be required to the northbound side where a 2m width would be achievable without vesting, which would meet the minimum requirement for the proposed NMU.

Various items of street furniture would need to be relocated, the footway from Ballygawley Parish Church would potentially be required to extend into the carriageway in order to achieve the 3m wide width.

At Fair Green junction the existing facilities through the town would not support a 3m wide shared use facility. The introduction of this would result in the requirement for full footway realignment and multiple crossings due to the narrow nature of some footways.

Opportunity 2(c)

At the A5 Omagh Road arm of northern roundabout, widening the existing 2m wide (approx.) footway across the central reservation island will provide suitable NMU crossing facilities to access footways running adjacent to the A5. The central reservation island at the Annaghilla Road arm of northern roundabout is also proposed to be widened to accommodate adequate NMU crossing facilities with the existing footway on the southern side to be widened to 2m.



Photograph 3: Shared use NMU facility – Ballygawley Roundabout – A5 Omagh Road Arm (See Drawing D-028)



Photograph 4: Shared use NMU facility – Ballygawley Roundabout - A5 Annaghilla Road Arm. (See Drawing D-028)

Opportunity 2(d)

The existing footway from the A5 Annaghilla Road towards the existing park and ride is proposed to be widened to 3m to provide a shared use NMU facility over a length of 58m, to link into existing facilities.

Opportunity 2(e)

The existing footway along the northern carriageway of Tullybryan Road is currently between 1.5 – 2m wide. It is proposed that the footway is widened to 3m from Grange Park to the entrance of the school, to provide adequate NMU facilities.

Opportunity 3 (See drawings D-027 and D-028): Pedestrian Crossing Facilities

Opportunity 3(a)

Provide an uncontrolled pedestrian crossing across the junction of Old Omagh Road and the A5, Omagh Road (i.e. the Northern entrance to Ballygawley). Drop kerbs and tactile paving could be installed along the footway's desire line. The crossing distances are approximately 4.0m and 8.0m.

Opportunity 3(b)

Provide an uncontrolled Shared use NMU crossing across the junction of the cul-de-sac and Remeen. Drop kerbs on both sides and tactile paving on one side has already been installed along the footway's desire line.

Tactile paving could be installed at the other side and the existing features could be widened to accommodate the 3m wide shared use NMU. The crossing distance is approximately 10m.

Opportunity 4 (See Drawings D-027 and D-028): NMU Routes and Greenways

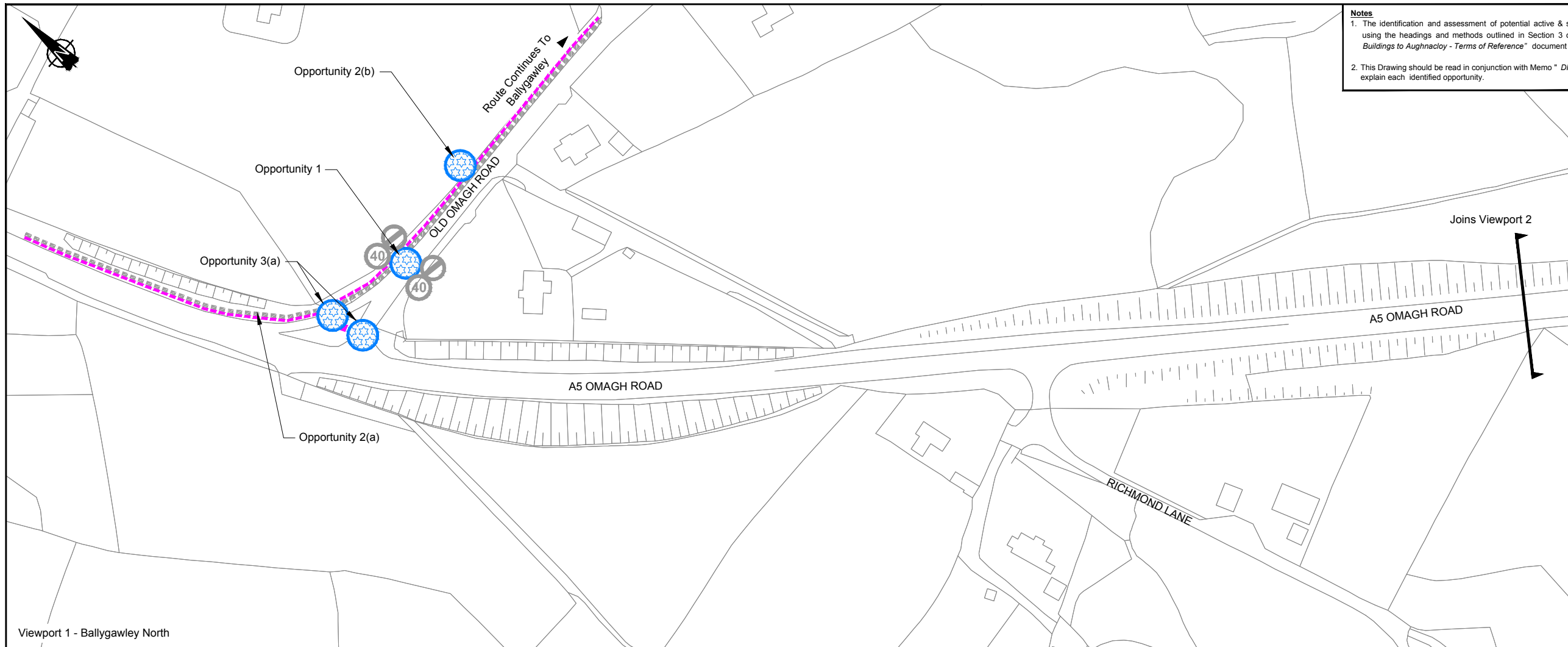
A potential greenway route has been highlighted in the area, running parallel to Tullybryan Road.

Notes regarding Park & Ride and School Access

Due to improvement work being completed in this area previously (2010), NMU access is well accounted for.

NMU access to the Park & Ride facility has been accommodated by 2.0m wide footways. Crossing points have drop kerbs and tactile pavement. However, during consultations with Translink, they indicated a desire to relocate the existing park & ride facility closer to the proposed A5WTC scheme.

NMU access to the school (St Ciaran's) has been accommodated by 2.0m wide footways. Crossing points have drop kerbs and tactile pavement.



Viewport 1 - Ballygawley North



Viewport 2 - Ballygawley

Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM17 - A5 - Active & Sustainable Transport Assessment: Ballygawley" which will explain each identified opportunity.

Key

Existing Asset Features

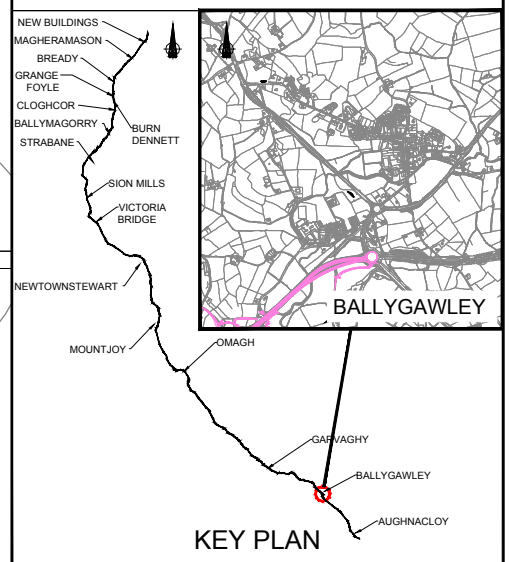
NMU Facility		Bus Stop	
Signage		Sports / Cricket Club	
School		Church	
Residential Care Homes		Health Care	

Proposed Improvements

2m Wide NMU Facility	
3m Wide (Shared Use) NMU Facility	
Existing NMU Facility to be Widened	
Existing NMU Facility to be Widened (via Potential Greenway)	
Potential Greenway Route	
Highway Realignment with new Shared Use NMU Facility	
Opportunity Described in Memo	

Other Features

Proposed A5WTC Scheme



DRAWING STATUS: FOR ISSUE

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STEP/PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy **A5WTC**

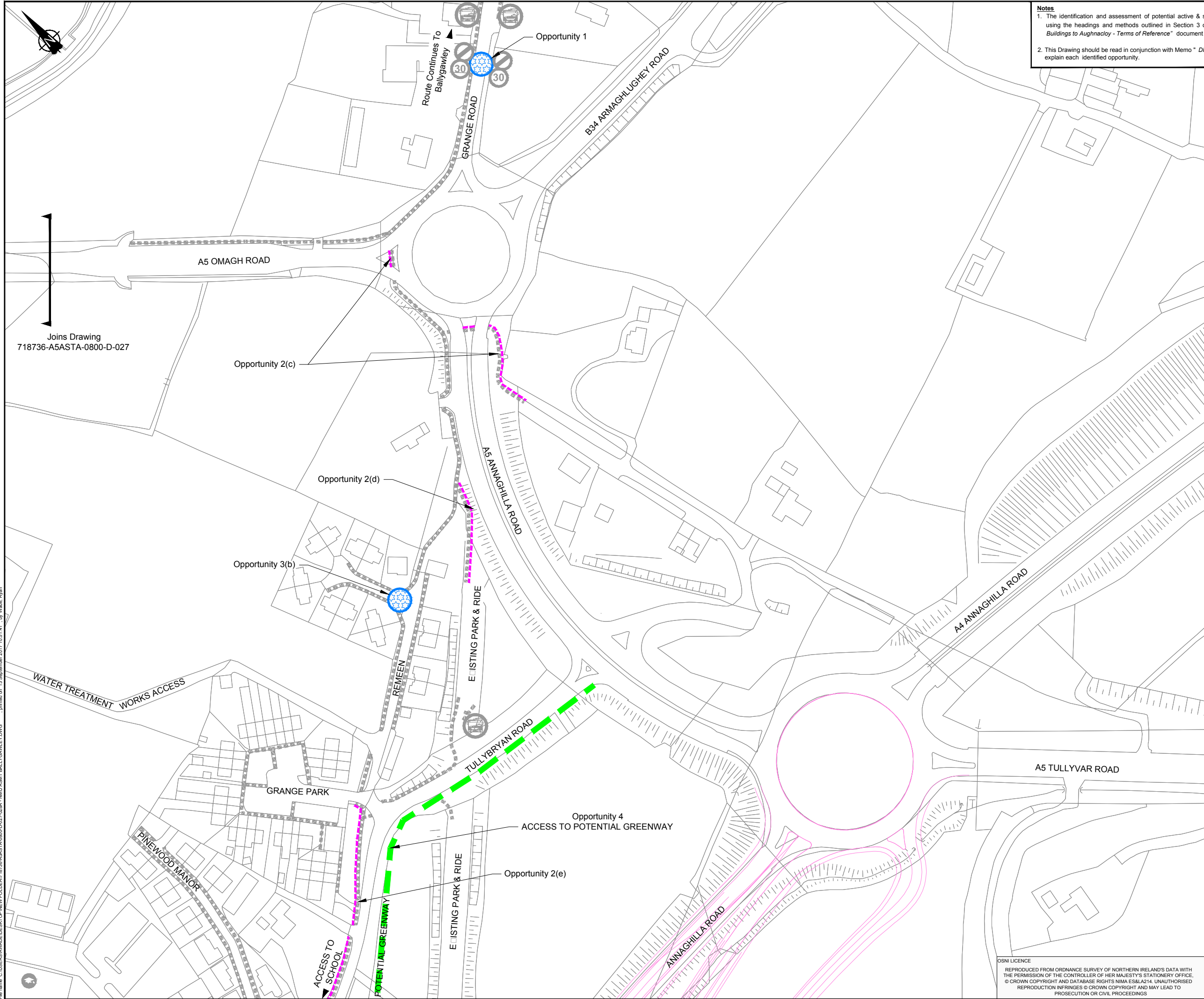
TITLE: ASSESSMENTS OF URBAN AREAS
 LOCATION: BALLYGAWLEY, CO. TYRONE
 SHEET 1 OF 2

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DRAWN: PM
		DATE: 14/08/2017

DRAWING NO: **718736-A5ASTA-0800-D-027** REV: **A**

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Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo " DM17 - A5 - Active & Sustainable Transport Assessment: Ballygawley " which will explain each identified opportunity.

Key

Existing Asset Features

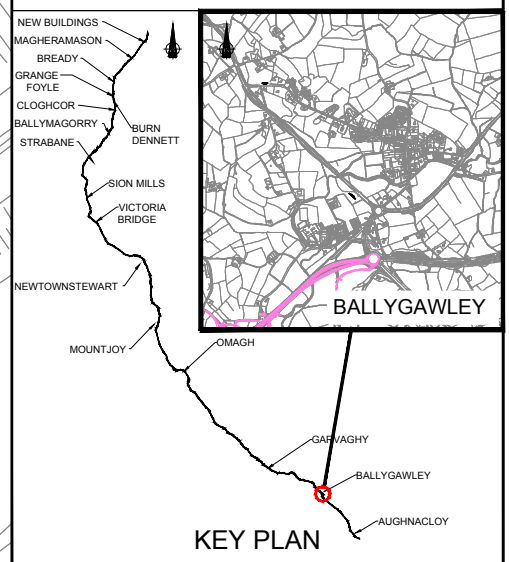
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- Signage: [50, 40, 30 symbols]
- School: [Symbol]
- Residential Care Homes (RH): [Symbol]
- Bus Stop: [Symbol]
- Sports / Cricket Club: [Symbol]
- Church: [Symbol]
- Health Care (HC): [Symbol]

Proposed Improvements

- 2m Wide NMU Facility: [Symbol]
- 3m Wide (Shared Use) NMU Facility: [Symbol]
- Existing NMU Facility to be Widened: [Symbol]
- Existing NMU Facility to be Widened (via Potential Greenway): [Symbol]
- Potential Greenway Route: [Symbol]
- Highway Realignment with new Shared Use NMU Facility: [Symbol]
- Opportunity Described in Memo: [Symbol]

Other Features

- Proposed A5WTC Scheme: [Symbol]



DRAWING STATUS: **FOR ISSUE**

wsp

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CLIENT: Department for **Infrastructure**
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STEP/PROJECT: **Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy**

TITLE: **ASSESSMENTS OF URBAN AREAS LOCATION: BALLYGAWLEY, CO. TYRONE SHEET 2 OF 2**

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: PM	DATE: 14/08/2017

DRAWING NO: 718736-A5ASTA-0800-D-028	REV: A
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DM19 – A5 Active & Sustainable Transport Assessment: Aughnacloy

Description

The identification and assessment of potential active & sustainable transport opportunities in Aughnacloy, County Tyrone. This work has been undertaken using the headings and methods outlined in Section 3 of the “Masterplan for Active and Sustainable Transport Opportunities: A5 – New Buildings to Aughnacloy - Terms of Reference” document (WSP Ref. 1058654/RP/001).

This memo could be read in conjunction with drawings: 718736-A5ASTA-0800-D-029 to 030 ‘Assessments of Urban Areas, Location: Aughnacloy, Co. Tyrone.’

The [blue arrows](#) in the photos below indicate the direction of northbound traffic on the A5.

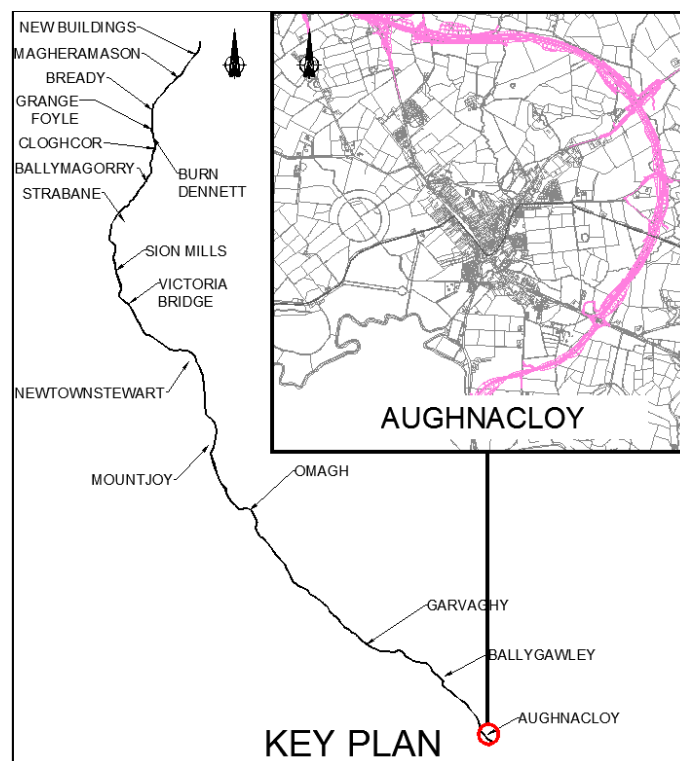


Figure 1: Indicative Location

Summary of Existing NMU Provisions

From the assessment work undertaken, it appears that the current provisions for non-motorised users (NMUs) on and around the existing A5 through Auchnacloy are of a relatively good standard. Existing NMU provisions include footways located on both sides of the existing A5, except for a limited section located to the southern end of Auchnacloy where on the southbound side there is no footway provision.

Existing Wide Footways in Auchnacloy (See Drawing D-029)

The footways within Auchnacloy are generally wide. The footway adjacent the northbound carriageway is approximately 4m wide and southbound is approximately 3m.

These footways and carriageways are separated by lay-bys, suitable for bay parking, which are up to 12m wide.

Existing Parking Facilities

There is a residential street parking provided which accommodates approx. 8 vehicles.



There is parking provided through the town centre which is adequate for use by attractors along the town centre.



Photograph 1 & 2: On-street parking provisions

Existing Pedestrian Controlled Crossing Point on A5 (See Drawing D-029)

Currently there is a pelican controlled signal crossing point on the existing A5 in Aughnacloy. Dropped kerbs and tactile paving has been provided. Crossing distance is approximately 7.5m. This crossing point serves numerous commercial properties, domestic properties, churches and more.

Summary of Potential NMU Improvement Opportunities

The opportunities described in the remaining pages of this memo discuss potential improvements to the two gateway entrances to Aughnacloy on the existing A5, potential footway improvements, the potential addition of pedestrian crossing facilities on the existing A5 and on roads that intersect the existing A5, potential greenway facilities that could be introduced and potential improvements to existing bus stop facilities.

Proposals for improved NMU facilities include a 3m wide shared use facility at the northern end of the town, adjacent to the southbound carriageway, 3m wide facility to the north of the Ravella Road junction and provision of a 3m wide facility along the A5 Monaghan Road on the northbound side of the carriageway.

Opportunity No.1 (See Drawings D-029 and D-030): Village Gateway signage, road markings, etc.

Existing gateway signage could be improved to modify driver behaviour and reduce mean speeds.

This potential improvement consists of replacing the existing gateway signage with enhanced signage where the posted speed limit is the dominant feature and colouring the carriageway surface to draw road users' attention to the posted speed limit; building out the kerb lines to narrow the carriageway at the gateway; and adding landscaping features (e.g. flowers beds) to impress upon road users that they are now entering a built up area. Refer to Page 5 of DM04 New Buildings for an example of these potential gateway improvements; the two images below illustrate the existing gateway features.



Photograph 3: Northern entrance to Aughnacloy Via A5 (See Drawing D-030)



Photograph 4: Southern entrance to Aughnacloy Via A28 (See Drawing D-030)

Opportunity 2 (See Drawings D-029 and D-030): Footway Improvements

Opportunity 2(a)

At the northern approach to Aughnacloy, there is an existing footway adjacent the southbound carriageway of the existing A5 approximately 1.6m wide. There is a grass verge to the rear of the footway which is approximately 3.5m wide. The grass verge could potentially be used for a 3.0m wide shared use NMU facility. This NMU facility could potentially link to the NI greenway route, which also partially uses this wide verge. The existing gateway signage would require relocation to ensure a 3.0m unobstructed width is achieved. Narrowing of the carriageway as per Opportunity 1, could also facilitate the inclusion of this opportunity.



Photograph 5: Potential Shared Use NMU Facility (See Drawing D-029)

Opportunity 2(b)

There are existing footways of approximate width 2.5m and 2.0m adjacent the southbound and northbound carriageways of the existing A5 respectively. The southbound footway could potentially be widened by 0.5m to create a shared use NMU facility. To facilitate a 3m wide shared use NMU facility on the southbound side, the southbound footway could potentially be widened away from the carriageway. Land ownership boundaries would have to be established to ascertain the feasibility of this potential opportunity.

Due to the constraints of domestic properties adjacent the northbound carriageway, widening of this section is not recommended.

Length of footway widening approximately 485m.

Opportunity 2(c)

Where the southbound existing footway terminates near Tramline Way, the potential shared use NMU facility highlighted in Opportunity 3(a) could continue until the footway recommences again. The potential length of this shared use NMU facility is 55m. The area identified for this new 3.0m wide shared use NMU facility is currently used as parking for the cemetery and church.

Opportunity 2(d)

There are bollards in the existing footway which may cause an obstruction to cyclists. 2no. bollards could be removed or reduced to either side of tramline way. An electricity pole and traffic lights could also be relocated to facilitate the shared use facility at this location.

Opportunity 2(e)

A 3.0m shared use NMU facility could be incorporated into the northbound footway from the signal controlled crossing (Opportunity 3(e)) to Ravella Road, where the footway is greater than 3.0m wide.



Photograph 6: Shared Use NMU Facility (See drawing D-029)

Land acquisition will not be required, as the widened footways will utilise existing lay-by areas.

Opportunity 2(f)

In line with Opportunity 5(a) to provide bus shelters, the footway at these locations could be widened to meet the edge of the A5 carriageway with a small number of parking bays on either side of the existing A5 removed to provide an unobstructed pathway for bus users to access the bus.

Opportunity 2(g)

At the southern end of Aughnacloy, there is an existing footway adjacent the northbound carriageway of the existing A5 Monaghan Road. The footway is approximately 2.0m wide.

This footway has the potential to be widened by 1m to create a shared use NMU facility, which will continue to the sports pitches adjacent the southern gateway of Aughnacloy. Footway length is approx. 455m.

It should be noted that there would be complexities associated with the implementation of a 3m wide shared use NMU facility at this location. For example there are existing walls that would need to be moved by circa 1m, comprising a total length of 175m. Moreover, there is also a row of properties at the northern end of this pot



Photograph 7: Addition of Shared NMU facility (wall to be offset 1m) (See Drawing D-030)

Opportunity 2(h)

Due to the constraints of narrow footways and domestic properties on Dungannon Street, it would not be possible to incorporate a full 3m wide shared use NMU facility. However, the footways on the B35 Carnteel Road could potentially be widened. This is also part of the Ulster Way walking route.

The footway adjacent to the Carnteel Road westbound carriageway is currently approximately 1.5m wide. The grass verge beside the footway could potentially be used to widen it by 1.5m, to create a 3m wide shared use NMU facility.



Photograph 8 : Opportunity 2(h) – Addition of Shared Use NMU Facility; Opportunity 5(c) – Addition of Bus Stop Shelter (Red Circle) (See Drawing D-030)

The length of facility would approximately be 380m. Approximately 10no. road lighting columns would need to be relocated ensure an unobstructed 3m width is achieved.

This shared use NMU facility will also provide access to the identified potential greenway route. See Opportunity 4(a).

Opportunity No.3 (See drawings D-029 & D-030): Pedestrian Crossing

Opportunity 3(a)

Provide an uncontrolled pedestrian crossing across the junction of Aghaloo Close and the Existing A5. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. This crossing point could potentially form part of the shared use NMU facility highlighted in Opportunity 2(a). The crossing distance is approximately 7.0m.

Opportunity 3(b)

An uncontrolled pedestrian crossing across the Existing A5, close to Hillcrest could be provided. A crossing point here would service the southbound bus stop. Drop kerbs and tactile paving could be installed. The crossing distance is approximately 10.0m.

There is currently a bus stop shelter at this bus stop. No improvements are suggested.

Opportunity 3(c)

Provide an uncontrolled pedestrian crossing across the junction of Tramline Way and the Existing A5. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. This crossing point could be a part of the shared use NMU facility highlighted in Opportunity 2(c). The crossing distance is approximately 6.5m.

Opportunity 3(d)

There is currently 2no. uncontrolled pedestrian crossings at the entrance and exit of the petrol filling station. The existing drop kerbs and tactile paving could be widened by approximately 1.0m to accommodate cyclists and pedestrians using the proposed shared use NMU facility.

Opportunity 3(e)

A signal pedestrian crossing across the Existing A5 (close to St Mary's Chapel) could be provided. A crossing point here would service the domestic properties and the church. Drop kerbs and tactile paving could be installed. The crossing distance is approximately 7.0m.

Opportunity 3(f)

Provide an uncontrolled pedestrian crossing across the junction of Ravella Road and the Existing A5. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 5.0m.



Photograph 9: Uncontrolled Pedestrian Crossing across Ravella Road (See Drawing D-029)

Opportunity 3(g)

Provide an uncontrolled pedestrian crossing across the entrance of St James' Church on the Existing A5. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 5.0m.



Photograph 10: Uncontrolled Pedestrian Crossing across St James' Church (See Drawing D-029)

Opportunity 3(h)

Provide an uncontrolled pedestrian crossing across the junction of Sydney Street and the Existing A5. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 7.0m.



Photograph 11: Uncontrolled Pedestrian Crossing across Sydney Street (See Drawing D-029)

Opportunity 3(i)

Provide an uncontrolled pedestrian crossing across the private access on the Existing A5. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 4.5m.



Photograph 12: Addition of Uncontrolled Pedestrian Crossing (See Drawing D-029)

Opportunity 3(j)

An uncontrolled pedestrian crossing across the Existing A5 Monaghan Road could be provided. Drop kerbs have been provided, but tactile paving could be installed along the footway's desire line. The crossing distance is approximately 9.0m.

Opportunity 3(k)

A signal pedestrian crossing across the Existing A5 (close to Coronation Park) could be provided. A crossing point here would service the domestic properties and St Mary's Primary School on Lettice Street. Drop kerbs and tactile paving could be installed. The crossing distance is approximately 7.0m.

Opportunity 3(l)



Photograph 13: Addition of Uncontrolled Pedestrian Crossing (See Drawing D-030)

An uncontrolled pedestrian crossing across the Existing A5 immediately south of the Dungannon Street junction could be provided. Drop kerbs and tactile paving could be installed along the footway's desire line. The crossing distance is approximately 9.0m.

Opportunity 3(m), 3(n) & 3(o)

Uncontrolled pedestrian crossings could be installed across an unclassified road, Chestnut Grove and Lisadavil Park. Drop kerbs have been provided but tactile paving could be installed along the footway's desire line. The crossing distances are approximately 6.0m, 7.5m and 6.0m respectively.

These crossing points will form part of the shared use NMU facility highlighted in Opportunity 2(h).

Opportunity 3(p)

An uncontrolled pedestrian crossing across the B35 Carnteel Road could be provided. Drop kerbs and tactile paving could be installed along the footway's desire line. The crossing distance is approximately 10m.

This crossing point would serve domestic properties, the 2no. bus stops and Aughnacloy Primary School.

Opportunity 4 (See Drawing D-029 and D-030): NMU Routes & Greenways

There is a greenway route highlighted to the northeast of Aughnacloy. This greenway utilises the disused railway line. Access to this section of the greenway can be provided along Tramline Way and the B35 Carnteel Road.

Opportunity No.5 (See Drawings D-029 and D-030): Bus Facilities

Opportunity 5(a)

The main bus stops in Aughnacloy are located to the north of the Sydney Street/A5 junction. Therefore, they will serve numerous commercial properties, domestic properties and churches. There is an existing signal controlled pedestrian crossing 100m from the bus stops. Bus shelters could be provided at both locations.

Opportunity 5(b)

There is also a second bus stop to the south of Sydney Street, on the southbound side of the road. A shelter could also be provided at this location. There is space for a second northbound bus stop and shelter, should it be deemed necessary.

Opportunity 5(c)

A bus stop shelter could be provided for the westbound bus stop on the B35 Carnteel Road, outside the church. This bus stop shelter would need to be located against the wall of the church to ensure it does not conflict with the unobstructed 3m width of the shared use NMU facility highlighted in Opportunity 2(h).

Opportunity 5(d)

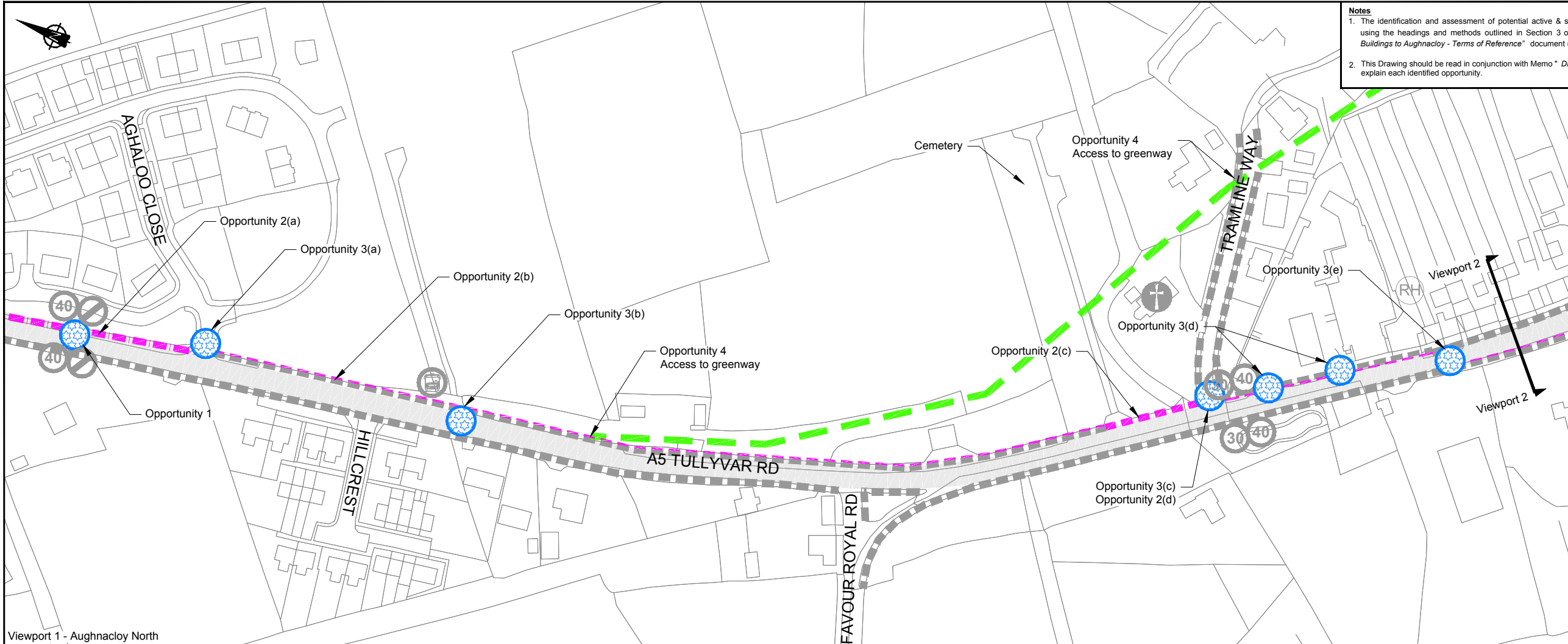


Photograph 14: Opportunity 5(d) – Addition of Bus Stop Shelter (Red Circle) Opportunity 3(p) – Uncontrolled Pedestrian Crossing (Red Rectangle) (See Drawing D-030)

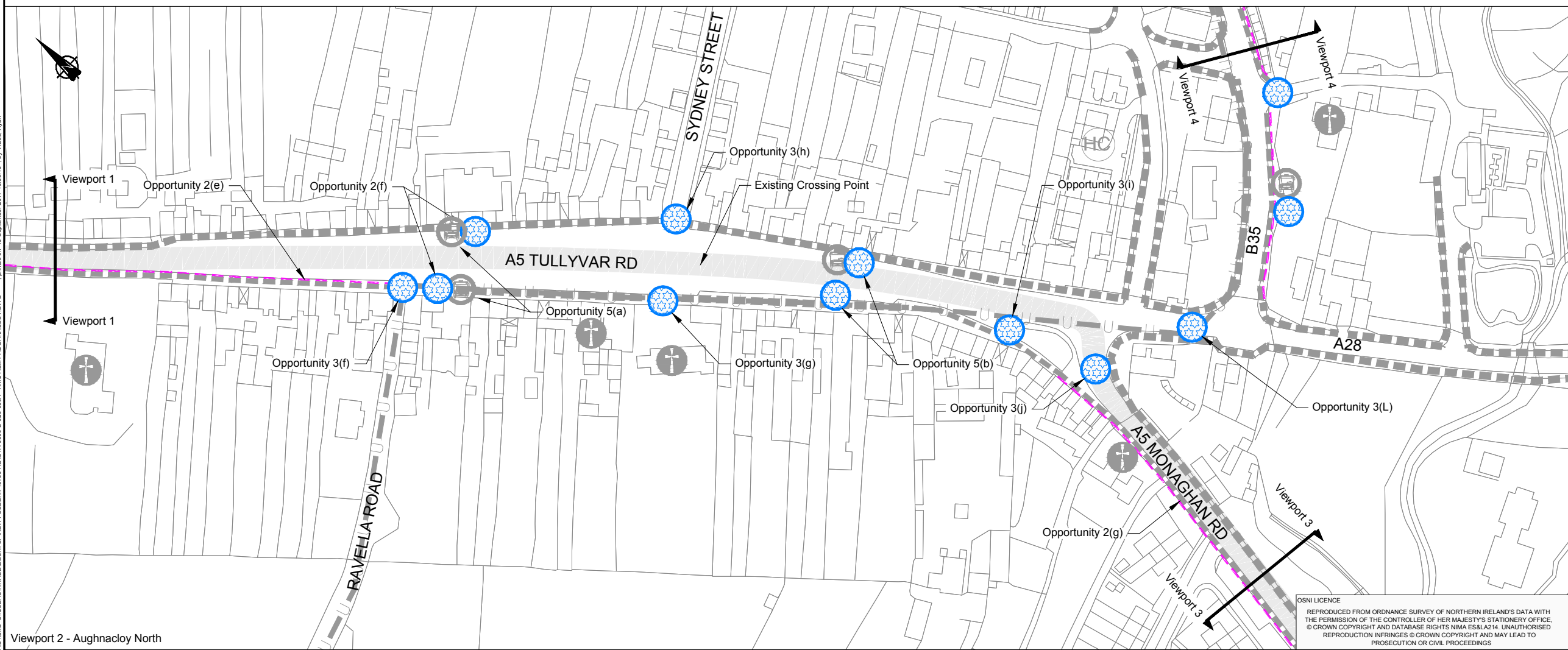
A bus stop shelter could be provided for the westbound bus stop on the B35 Carnteel Road to match the eastbound bus stop.

Park and Ride Facilities

During the course of this assessment, Translink has been consulted and have not identified Aughnacloy as a desirable location for a Park and Ride facility.



Viewport 1 - Aughnacloy North



Viewport 2 - Aughnacloy North

Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM19 - A5 - Active & Sustainable Transport Assessment : Aughnacloy" which will explain each identified opportunity.

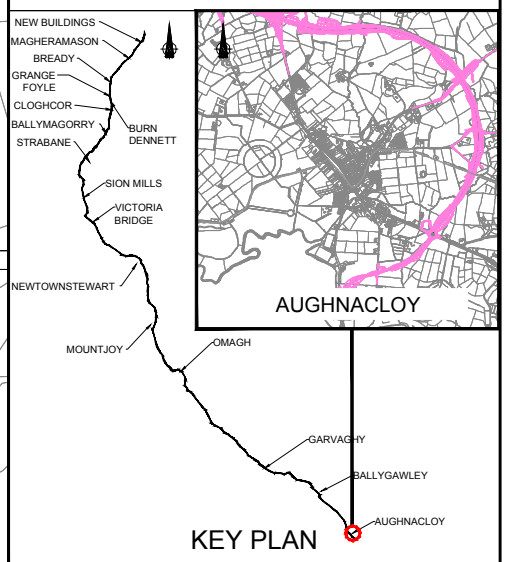
Key

Existing Asset Features

- NMU Facility
- Signage (50, 40, 30)
- School
- Residential Care Homes (RH)
- Ulster Way
- Proposed Improvements
- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)
- Proposed A5WTC Scheme



DRAWING STATUS: FOR ISSUE

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SITE PROJECT: Masterplan for Active & Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy **A5WTC**

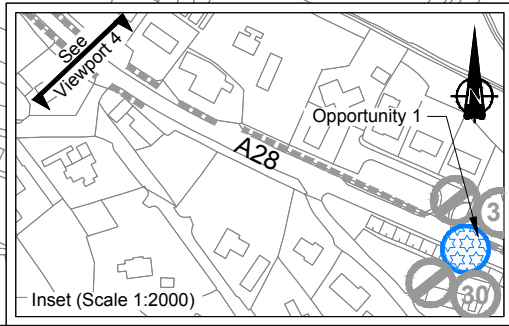
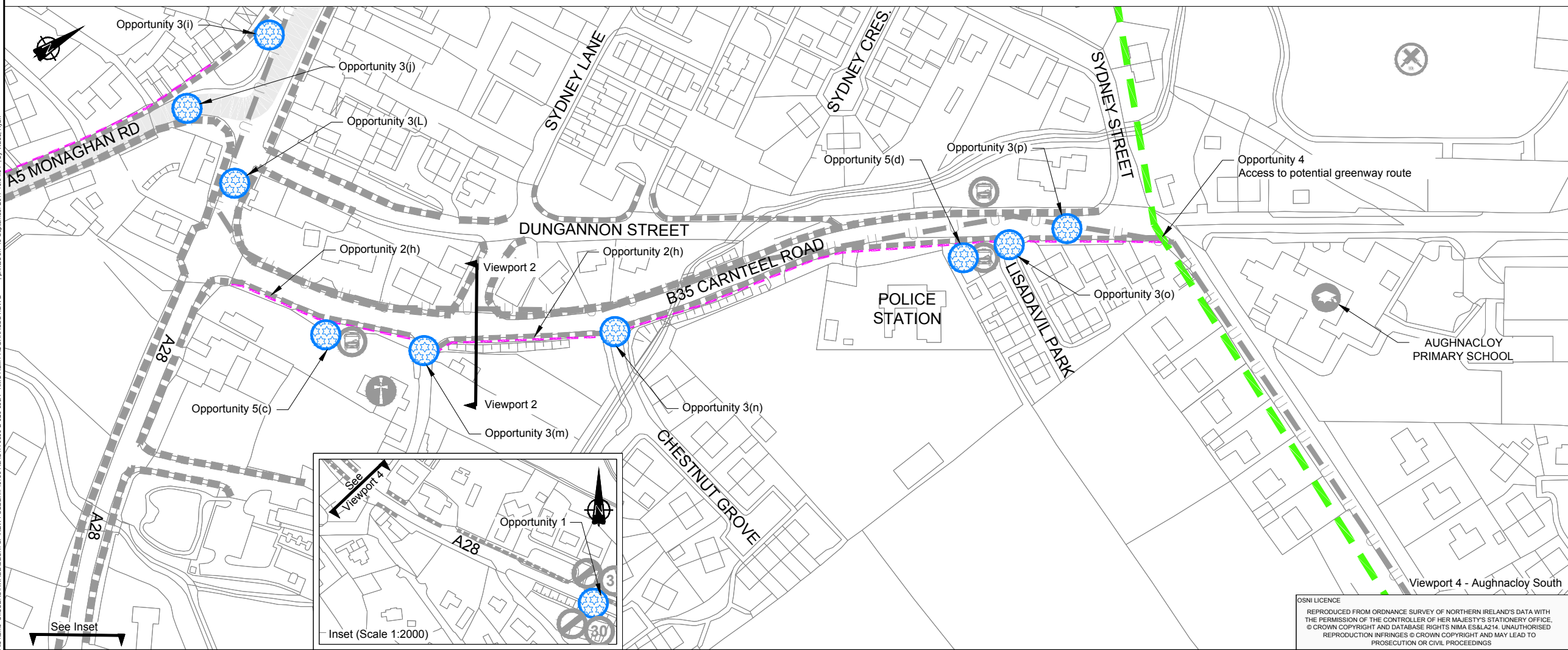
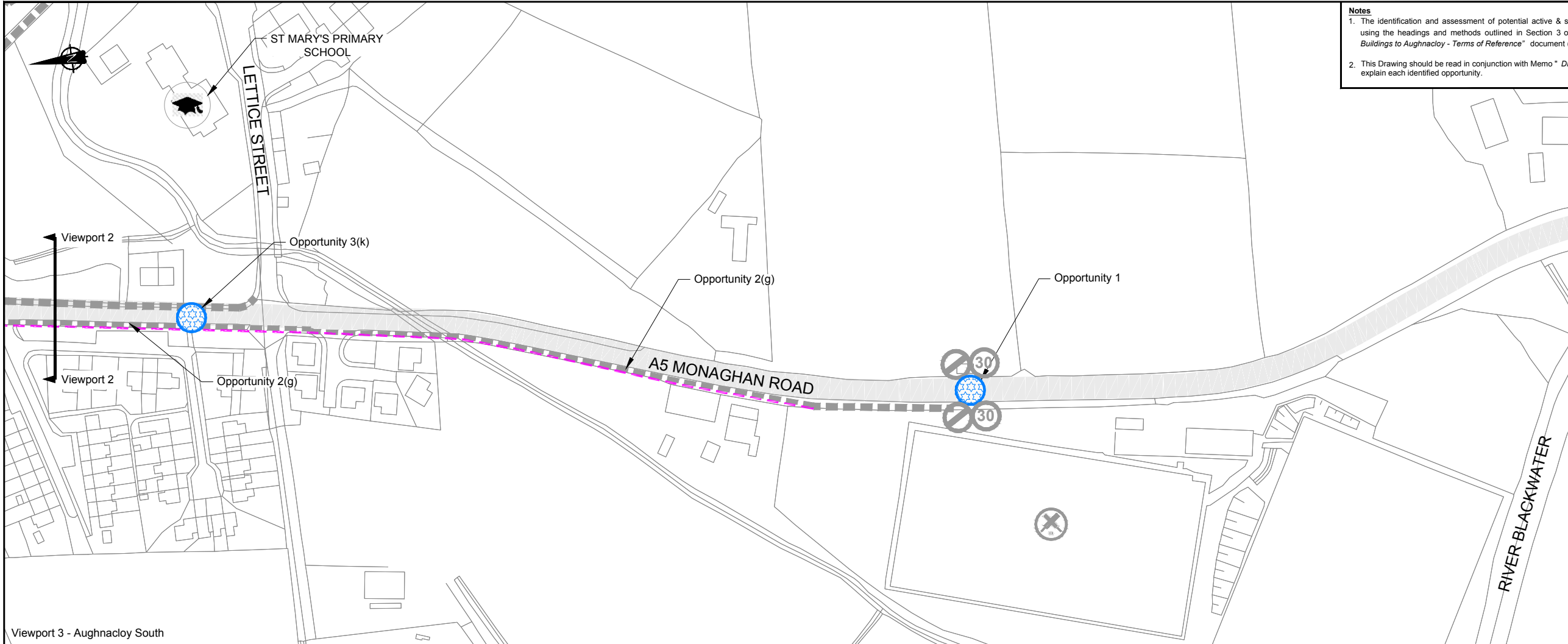
TITLE: ASSESSMENTS OF URBAN AREAS
 LOCATION: AUGHNACLOY, CO. TYRONE
 SHEET 1 OF 2

SCALE @ A1: 1:1000	CHECKED: PD	APPROVED: PGE
PROJECT NO: 718736	DESIGNED: OFK	DATE: 14/08/2017

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Notes
 1. The identification and assessment of potential active & sustainable transport opportunities along the A5. This work has been undertaken using the headings and methods outlined in Section 3 of the "Masterplan for Active and Sustainable Transport Opportunities: A5 - New Buildings to Aughnacloy - Terms of Reference" document (WSP Ref. 1058654/RP/001).
 2. This Drawing should be read in conjunction with Memo "DM19 - A5 - Active & Sustainable Transport Assessment : Aughnacloy" which will explain each identified opportunity.

Key

Existing Asset Features

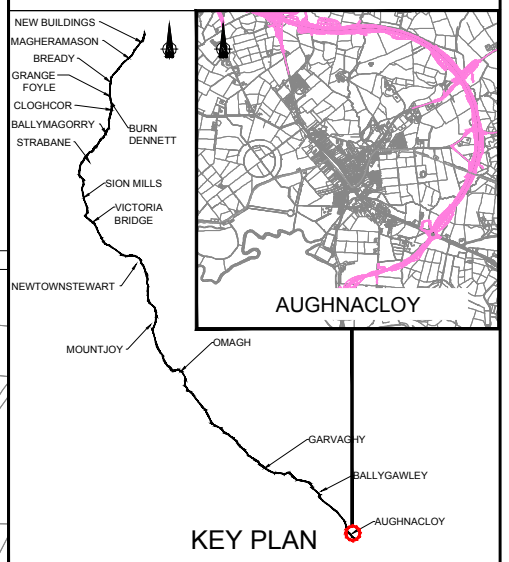
- NMU Facility
- Signage
- School
- Residential Care Homes (RH)
- Ulster Way
- Bus Stop
- Sports / Cricket Club
- Church
- Health Care (HC)

Proposed Improvements

- 2m Wide NMU Facility
- 3m Wide (Shared Use) NMU Facility
- Existing NMU Facility to be Widened
- Existing NMU Facility to be Widened (via Potential Greenway)
- Potential Greenway Route
- Highway Realignment with new Shared Use NMU Facility
- Opportunity Described in Memo

Other Features

Proposed A5WTC Scheme



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TITLE: ASSESSMENTS OF URBAN AREAS
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