

URS

York Street Interchange

Strategic Advisory Group

Summary Report

November 2015

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STRATEGIC ADVISORY GROUP:
SUMMARY REPORT

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1. INTRODUCTION

1.1 York Street Interchange

The Department for Regional Development (DRD) TransportNI proposes to construct the York Street Interchange (‘the Proposed Scheme’) as a long-term strategic road improvement to improve links between the Westlink and the M2 and M3 motorways in Belfast.

The existing at-grade signalised York Street junction currently links the Westlink to M2 and M3 through a complex arrangement of traffic signals that interface with the local road network, which includes York Street, York Link, Great Georges Street and Nelson Street. The overall signalised “box” created by these four signalised junctions is known as the York Street junction gyratory system in which road users currently experience delays and congestion, particularly at peak periods.

The Proposed Scheme would provide a fully grade-separated interchange to replace the existing signalised gyratory junction. Interchange links between the Westlink, M2 and M3 would be provided in underpasses aligned beneath new bridge structures at York Street and under the existing Dargan and Lagan bridges.

1.2 Strategic Advisory Group

During the development of the Proposed Scheme in 2014 it was decided to convene a group who would offer a wider perspective view on the project. TransportNI contacted representatives from a number of external parties requesting representation. Table 1.2.1 presents the agreed membership of the Strategic Advisory Group (SAG). The SAG was convened on six occasions between January and September 2015.

Table 1.2.1: SAG membership

| Name | Organisation |
|---|--|
| Roy Spiers | TransportNI |
| Colin Pentland | TransportNI |
| Mark O’Donnell | Department For Social Development (DSD) |
| Ann Doherty | Belfast City Council (BCC) |
| Dermot O’Kane | PlanningNI/BCC* |
| Paul Spray | University of Ulster – Greater Belfast Development (GBD) |
| Roisin McDonough | Arts Council for Northern Ireland |
| Mark Hackett | Forum for Alternative Belfast (FAB) |
| Michael Megarry/ Una Somerville/ Catherine Adams/Christina Todd | URS** |
| *Following the changes introduced by RPA in April 2015, the responsibilities for planning were transferred to BCC | |
| **URS in attendance to facilitate the work of SAG | |

1.3 Purpose and Terms of Reference of SAG

During the inaugural meeting held on the 16th January 2015 the remit of the SAG was raised. Following discussion the purpose of the Group was identified:

- *to provide strategic guidance to facilitate the integration of York Street Interchange with other Government and private initiatives in the setting of the project, in order to maximize opportunities for investment across all the sectors with the aim of enhancing the end product; and*
- *to review scheme aesthetics and enhance user appreciation.*

The wording was ratified at the meeting held on the 13th March 2015. The terms of reference under which the Group would fulfil its purpose were also identified:

“In attempting to achieve the purpose set, it should be appreciated that TransportNI are constrained by:

- *the statutory processes required to be followed in order to deliver a major road improvement; and*
- *the budgetary limitations incumbent upon them as a business unit of DRD.”*

1.4 Proposed Scheme information received by SAG

Prior to the various meetings held SAG members received a range of information in connection with the Proposed Scheme. This has included:

- DMRB Stage 3 Proposed Scheme Report Part 1 - Environmental Statement
- DMRB Stage 3 Proposed Scheme Report Part 2 - Engineering, Traffic and Economic Assessment Report
- fixed view digital animation of the Proposed Scheme; and
- copies of visualisations of the Proposed Scheme

Presentations were given by URS during each of the SAG meetings to facilitate discussion.

The representatives of each of the organisations attending gave an overview presentation of their agencies function during the SAG meetings.

2. AREAS OF THE PROPOSED SCHEME FOR REVIEW

2.1 Three strands for consideration

During the inaugural meeting it was noted that future consideration by the SAG should focus on three strands:

- *Linkage and connection* – considering how the movement of people by various modes was impacted by the Proposed Scheme.
- *Aesthetics and finishes* – considering how the appearance of the Proposed Scheme could be enhanced both in terms of thematic design and specification of materials.
- *Setting and future land use* – considering the Proposed Scheme with respect to existing & known committed developments and also longer term planning for the surrounding area.

Whilst the SAG considered each of these areas it was noted that at this early stage specific agreement of details of every aspect would not be possible. It was acknowledged that in due course a smaller focus group could be drawn together to consider ‘constraints and restraints’ associated with the project. Detail design will be integrated with future Belfast planning strategies and guidelines and also proposed nearby projects, such as Streets Ahead Phase 3.

2.2 Linkage and connection

During the course of discussions the Group focussed on the following areas relating to linkage and connection.

2.2.1 *Pedestrian provision*

The SAG acknowledged that the level of pedestrian facilities provided by the Proposed Scheme was critical in ensuring the safe movement of people throughout the area but also to create a positive pedestrian experience. The Group noted that the Proposed Scheme provided new controlled crossing facilities at a number of locations as shown in Figures 2.2.1 and 2.2.2.

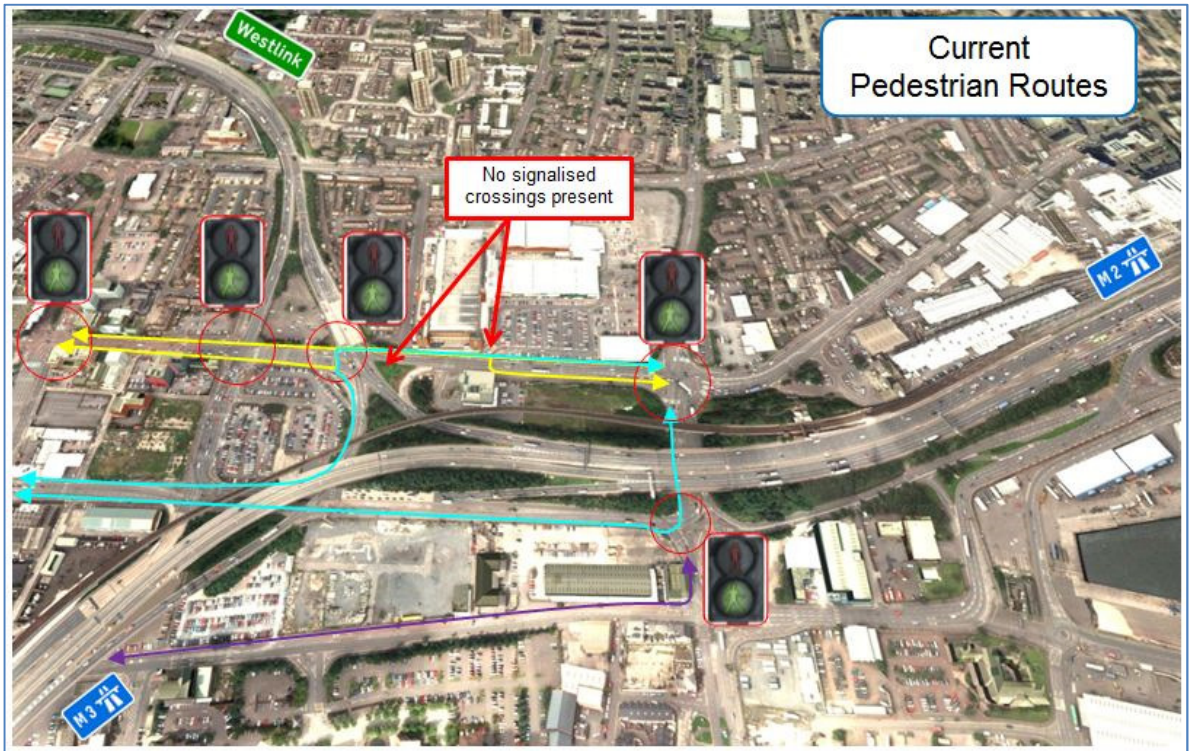


Figure 2.2.1 Existing pedestrian facilities along York Street



Figure 2.2.2 Proposed pedestrian routes along York Street

The SAG acknowledged that the existing York Street area was not a pleasant environment for pedestrian to use. It was accepted that by separating the strategic and local traffic flows by means of grade separation, pedestrians would experience a less intimidating walking environment. Figures 2.2.3 and 2.2.4 illustrate this point at the York Street/Great Georges Street junction.

The SAG noted that the extent of proposed footway at Great Georges Street should be revisited to allow the provision of street trees within the boundaries of the pedestrian area.



Figure 2.2.3 – Existing crossing, Great Georges Street



Figure 2.2.4 – Proposed Scheme crossing, Great Georges Street

2.2.2 *Cycling provision*

It was acknowledged to the SAG that TransportNI had received a significant number of letters during the consultation period on the draft Statutory Orders, commenting on the proposed provision for cyclists. The Proposed Scheme provides a mandatory northbound cycle lane along York Street between its junction with Frederick Street and Dock Street. In the southbound direction a mandatory cycle lane along York Street is proposed between its junction with Dock Street and the M2, with a shared use bus lane proposed from this point until the junction with Frederick Street. The proposed provision is shown in Figure 2.2.5.

The SAG were advised that the issues identified following the statutory consultation were being given due consideration and that TransportNI was in the process of discussing the provision of cycling facilities with the relevant parties i.e. DRD Cycling Unit and Sustrans.

The SAG was supportive that the needs of cyclists are addressed within the Proposed Scheme and that high quality cycling infrastructure be provided. In addition it was noted that the potential existed in the future for the new Belfast bike share scheme to be extended to the York Street area.

2.2.3 *Henry Street Interface*

Within correspondence received by TransportNI during the consultation period on the scheme draft Statutory Orders, an identified local community interface area was highlighted in the vicinity of Henry Street. Henry Street was closed to vehicular traffic from York Street following removal in the late 2000's of a vehicular barrier erected during the Troubles and the construction of a community designed screening boundary. The current arrangement allows the movement of pedestrians from Henry Street onto York Street. Under the Proposed Scheme the movement of pedestrians between both streets was maintained albeit that pedestrians heading towards the city centre had an increased journey due to the fact that the proposed York Street footway was elevated. Figures 2.2.6 and 2.2.7 present both the existing and proposed arrangements.

The SAG discussed the merits of the provision of steps/ramp at the end of Henry Street to allow direct connection for pedestrians wishing to travel towards the city centre via York Street. The SAG noted that discussion with the local community was required and that engagement with the Department of Justice should also take place.

URS/TransportNI subsequently undertook consultation with local community representatives, the Department of Justice and community policing staff from PSNI with regard the interface. The matter will be highlighted during detailed design to ensure that appropriate consideration is given to the area.

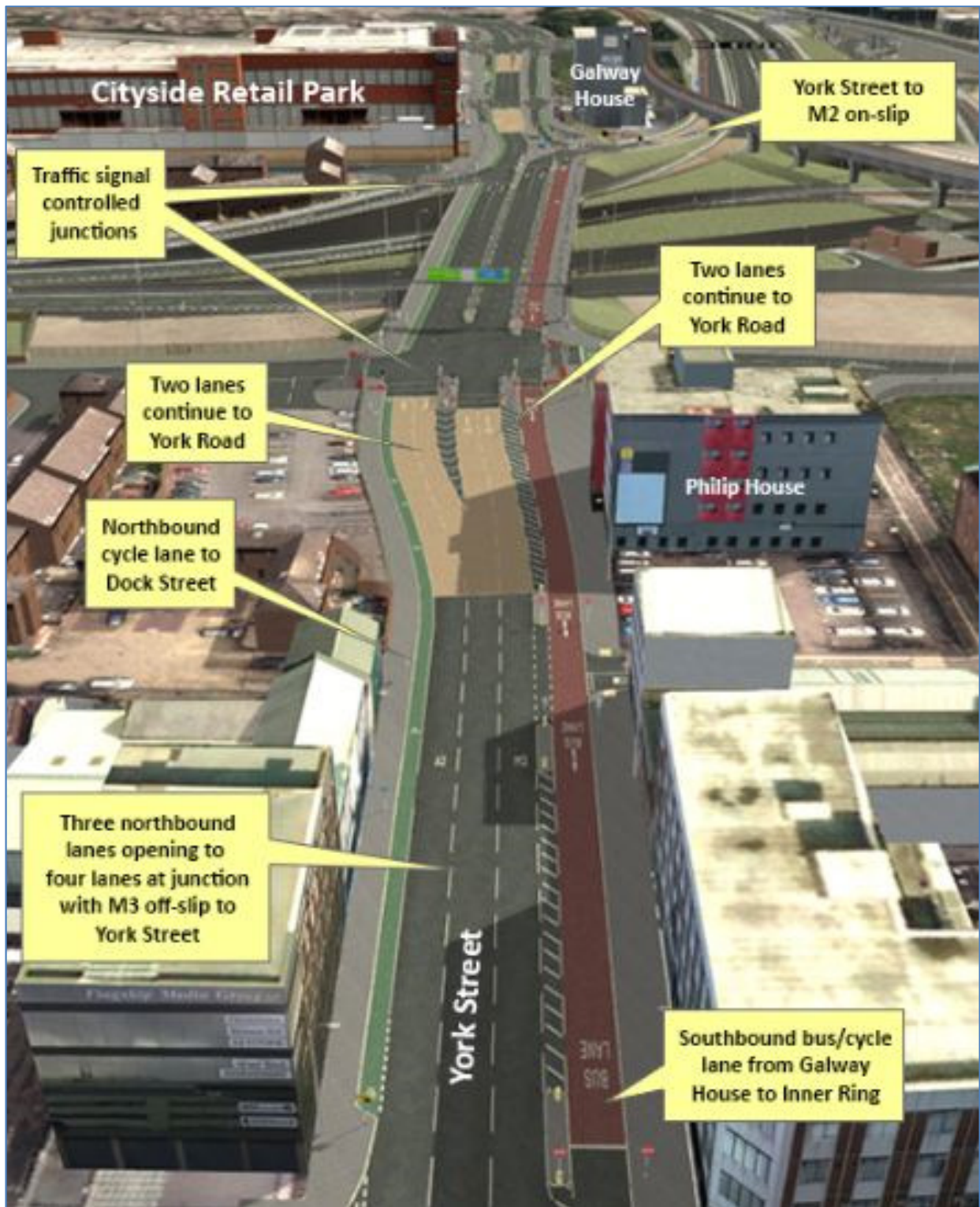


Figure 2.2.5 – Proposed Scheme looking north along York Street including cycling provision



Figure 2.2.6 – Existing Henry Street/York Street area



Figure 2.2.7 – Proposed Henry Street/York Street area including raised York Street footway

2.2.4 Gamble Street Rail Halt

The SAG queried with TransportNI the impact of the Proposed Scheme upon the Gamble Street rail halt referred to in the Belfast Metropolitan Transport Plan . It was confirmed the Proposed Scheme had no direct impact upon the Gamble Street site. The SAG was informed that Northern Ireland Railways was considering the viability of dualling the existing single line stretch of the Dargan Rail Bridge. Potential strengthening works to future-proof the

substructure of the existing bridge were under consideration and may form part of the construction works for the Interchange scheme.

2.3 **Aesthetic and finishes**

The SAG acknowledged that the quality of streetscape can enhance the experience of all users including vehicular drivers, pedestrians and cyclists. Careful consideration of the following elements would be required:

- materials selected for footway finish;
- street furniture;
- soft landscaping;
- lighting, both feature and conventional street lighting; and
- selected use of public art

In considering the design finishes for the Proposed Scheme, TransportNI outlined to the SAG the approach adopted during the M1 Westlink project (2005-09). For this scheme, an aesthetic review panel had been formed to assist in the preparation of the aesthetic requirements included with the contract documentation. Tenderer's proposals for the aesthetic of the scheme, based on these requirements, were also assessed as part of the quality submission included with returned tenders. The designs of Grosvenor Road overbridge and the Broadway Roundabout were referenced as examples where the approach had been successfully adopted.

2.3.1 **Hierarchy of streetscape**

The SAG considered the finishes to various DSD and TransportNI projects completed in recent years in Belfast. When considering the Proposed Scheme it was identified that recorded and envisaged pedestrian usage meant that a hierarchy of streetscape could be adopted as outlined below and shown in Figure 2.3.1:

- Priority 1 - York Street (between Frederick Street & Brougham Street);
- Priority 2 - the realigned section of Great Georges Street (between York Street. and Nelson Street) and the revised 2-way section of Nelson Street; and
- Priority 3 - Dock Street (between York Street. and Corporation Street) and Corporation Street.

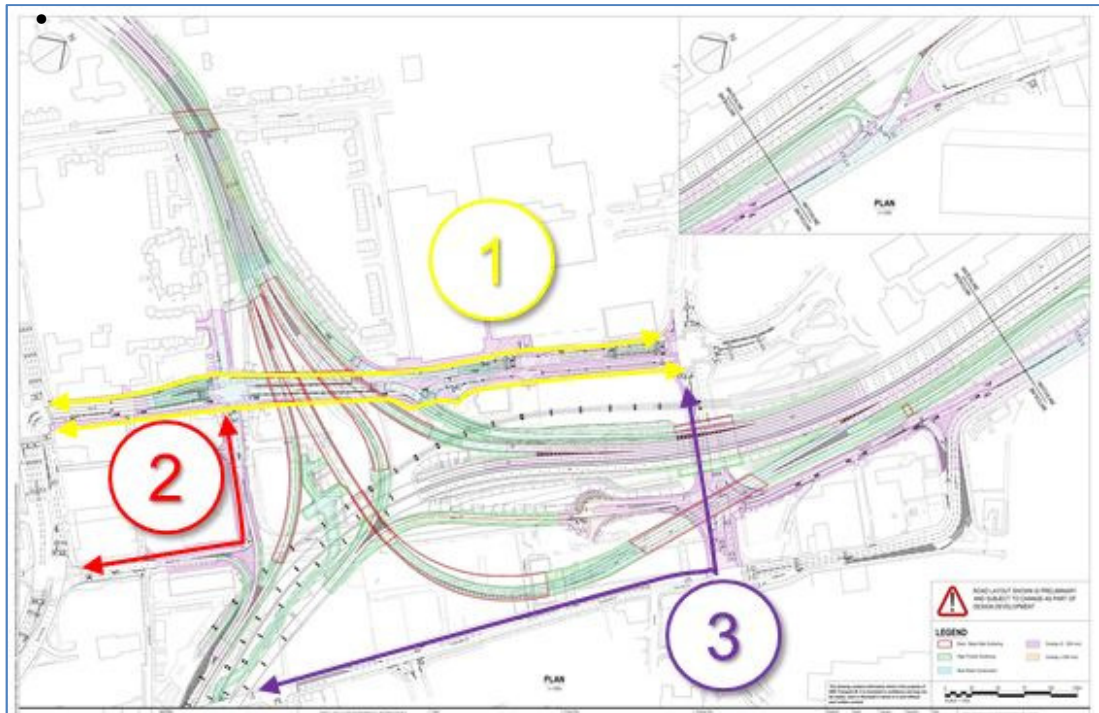


Figure 2.3.1 – Proposed hierarchy of streetscape

Within the resources available the priority given to each of the identified routes would inform the design and specification of the streetscape envisaged. The proximity of the Greater Belfast Development (GBD) for the University of Ulster and the DSD Street Ahead Phase 3 project were acknowledged by the SAG. The priority given to the streetscape along the York Street corridor should seek to assist to the sense of arrival at the GBD.

2.3.2 Footway finishes

Whilst the SAG identified the importance the quality of streetscape can have, the financial implications of its provision were noted. As the Proposed Scheme is being delivered by TransportNI it is apparent that they as a business unit of the Department for Regional Development (DRD) will be responsible for the preparation of the economic business case that will detail the required expenditure. TransportNI have over many years successfully delivered strategic road improvement projects in both urban and rural environments. In doing so decisions have been made with respect to the selection of suitable materials for the finish of various construction elements including footways. Based on consideration of a range of factors including initial cost and service life durability, TransportNI have generally selected the use of asphaltic surface course materials and precast concrete kerbing for footways. Where specific areas have been identified within programmes for urban regeneration or retail/economic development, enhanced levels of material specification for footways, typically granite paving and kerbs are selected. Schemes of this nature have generally been funded by Departments other than DRD.

These matters were discussed extensively with the SAG. Based on the provision of enhanced materials for the priority 1 and 2 areas referred to under section 2.3.1, an estimated additional capital cost of £850k would be required (excluding costs for street furniture or decorative street lighting). TransportNI advised the SAG that such additional funding would not be available through their allocations.

As the development of an aesthetic theme for the Proposed Scheme progressed (see section 2.3.3) it was determined that selective use of enhanced footway materials could be incorporated within the future requirements of the scheme. These included the use of granite blocks and kerbs in identified areas along the priority 1 and 2 footway corridors. Examples of their use are shown in Figure 2.3.2.

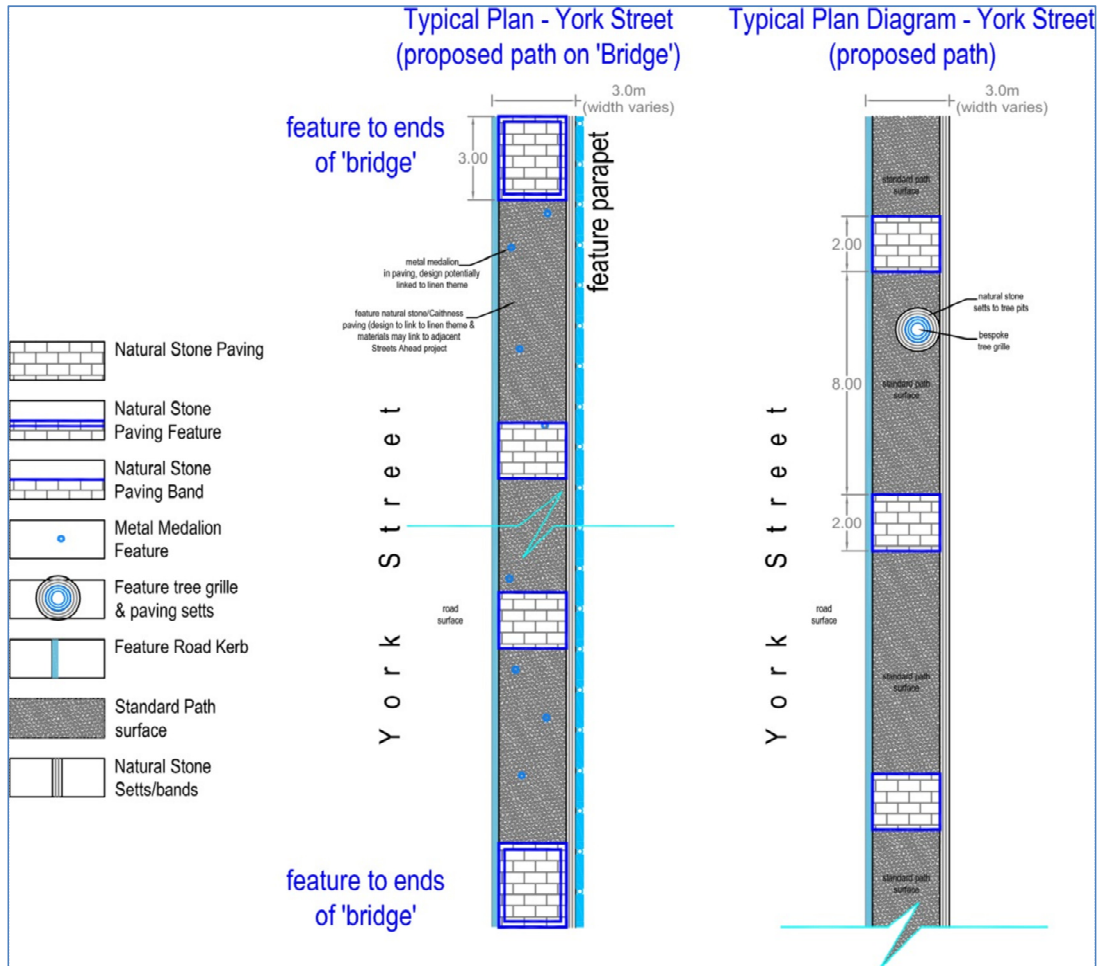


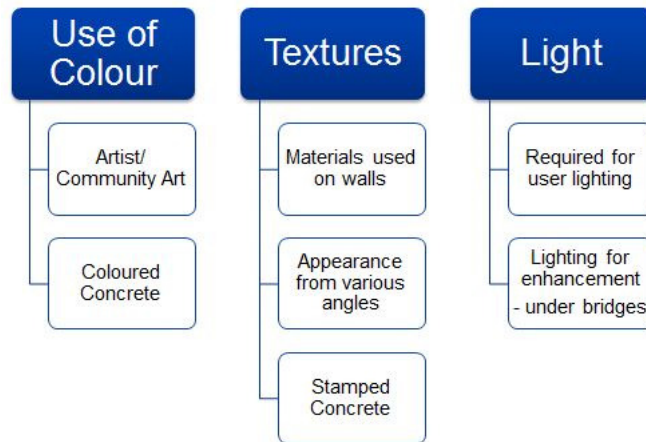
Figure 2.3.2 – Selective use of enhanced footway materials

2.3.3 **Aesthetic Theme**

In considering the aesthetics of the Proposed Scheme the SAG were required to appreciate the various forms of structure necessary to provide grade separation between the strategic and local road links. These forms included:

- underpasses;
- retaining walls;
- overbridges; and
- existing bridge undercrofts.

Depending upon the nature of each structural form the following aspects could be considered:



It was identified that the majority of these structural forms included areas that could be utilised within a unified theme throughout the Proposed Scheme. The level to which an agreed theme could be incorporated would depend upon:

- who will see the element – vehicular users only and/or pedestrians;
- the overall scale of the element – height of wall etc.;
- safety of motorised road users, with distraction of drivers considered unacceptable;
- safety of pedestrians; and
- cost.

Particular attention was directed towards opportunities with respect to pedestrians and in particular the proposed and existing overbridges. These included the:

- York Street overbridges (2No. 2-span structures)
- new Dock Street overbridge;
- existing Dock Street overbridge; and
- existing North Queen Street overbridge.

Given the strategic nature of the roads carried by each of the overbridges listed, the presence or absence of pedestrians and the requirement to provide suitable levels of vehicular containment, it was identified that the York Street overbridges offered the most significant potential for themed treatment.

2.3.4 Unified Aesthetic Theme – Linenopolis

Following consideration by URS’ landscape designers, a unified aesthetic theme incorporating the heritage of the York Street area in connection with the linen industry in Belfast was proposed termed “Linenopolis”. The Linenopolis theme would inspire the colour palette, textures and design details including:

- wall finishes – bespoke form liner/textured walls, lighting, block work representing woven fabric, colour, cladding;
- acoustic barriers – timber/Perspex/steel/eco/feature barriers;
- streetscapes on priority streets – feature lighting, footpath features, parapets with linen design theme;
- planting – coloured planting, distinctive trees on individual routes, feature planting;
- underpasses – well lit and welcoming; and
- temporary use of lands – ‘pop-up’ community gardens/wildflower meadows/planters.

During the course of the SAG meetings held between March and June 2015 the Linenopolis theme was developed in varying areas. Appendix A includes selected images which illustrate the identified theme.

The SAG was supportive of the Linenopolis theme and considered it respectful of the history and character of Belfast with a contemporary edge.

It was noted that as the Linenopolis theme was developed, control of its implementation would remain with the client e.g. development of a comprehensive landscape design would not be the responsibility of a future design & build contractor.

2.3.5 North Queen Street

The SAG noted the impact that the Proposed Scheme had upon the existing North Queen Street Bridge. The carriageway cross-section proposed for the Westlink requires that the bridge be widened by 2.7m on the southern elevation and 4.5m on the northern elevation. Consequently the extent to which pedestrians will enter beneath an area dependent upon artificial lighting will increase. Furthermore the SAG noted that the existing pedestrian environs would benefit from improvement. Figure 2.3.3 and 2.3.4 presents the current setting.

It was noted that North Queen Street Bridge is situated on the site of the former McGurk’s bar, which was destroyed in a terrorist bombing attack in 1971. A number of memorials to the victims have been erected over the years at the bridge structure including a false façade erected in December 2011 depicting the original bar’s appearance, fixed to the south east wingwall of the bridge. Given the proposed widening of the structure the SAG acknowledged the sensitive nature of the treatment of the existing memorials.. The Group accepted that the existing memorial required removal due to the works and should be carefully taken down and returned to the appropriate parties.

In order to enliven the pedestrian environs beneath the structure a combination of improvements were proposed. These included the provision of high level feature lighting to the undercroft area, use of Aluminium Composite Material (ACM) graphics panels on the existing abutments designed with local community input and decorative acoustic barriers along the edge of the structure on both elevations on the Westlink. The proposed options are shown in Figures 2.3.5 and 2.3.6. It was noted that agreement with regard future maintenance of any measures was required.



Figure 2.3.3 Southern elevation of North Queen Street Bridge



Figure 2.3.4 Northern elevation of North Queen Street Bridge

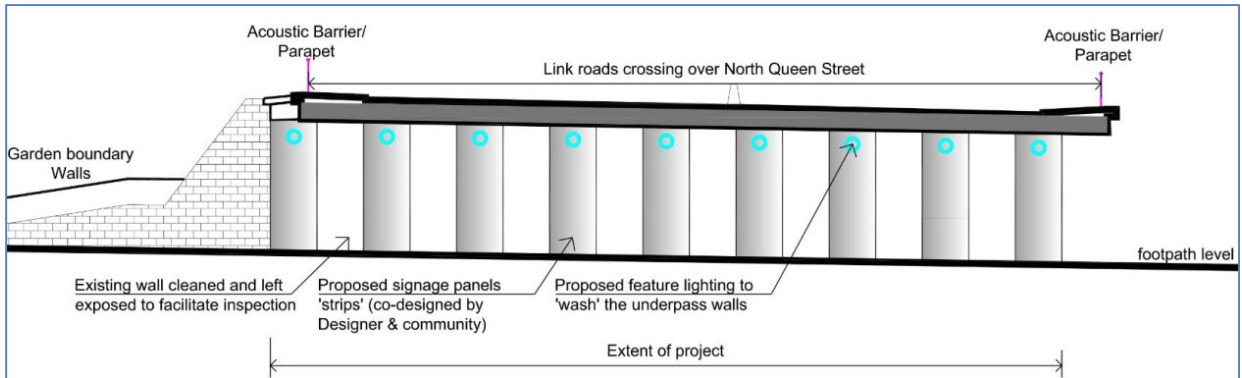


Figure 2.3.5 Proposed treatment to widened North Queen Street Bridge abutment

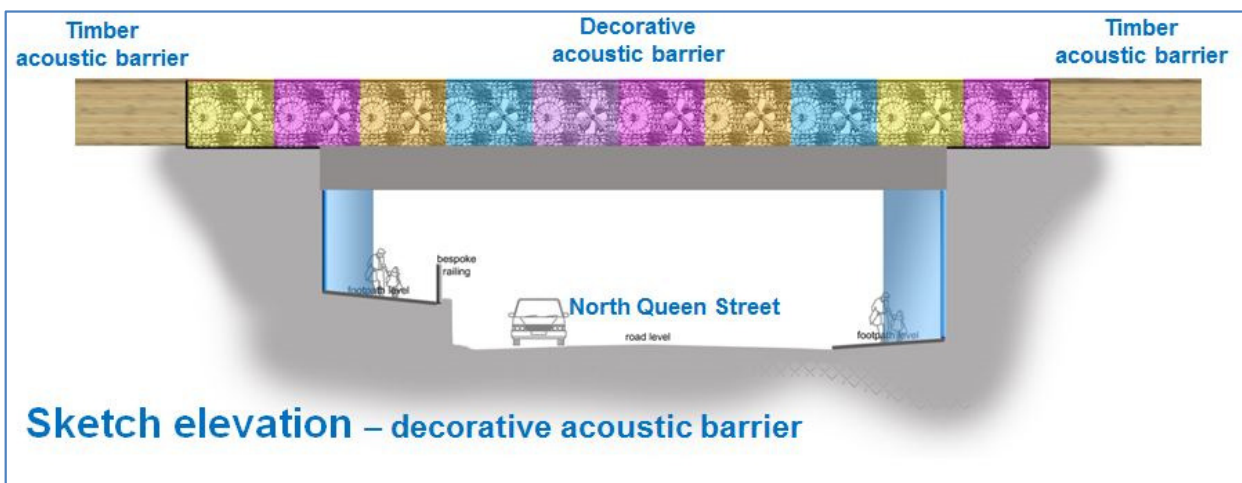


Figure 2.3.6 Southern elevation of proposed widened North Queen Street Bridge

2.3.6 York Street overbridges

The proposed separation of strategic and local traffic requires the construction of two overbridges to carry a realigned section York Street between its junctions with Great Georges Street and the proposed Westlink off-slip. It was identified that of the existing pedestrian routes within the footprint of the Proposed Scheme the York Street corridor experienced the most significant volume of pedestrians. Accordingly it was acknowledged by the SAG that these structures offered the greatest opportunity for aesthetic enhancement in line with the Linenopolis theme.

The requirement to ensure a suitable level of vehicular containment was noted within the consideration of the SAG. URS proposed that the necessary containment could be provided by a reinforced concrete stem which would be integral with the bridge deck. This form of construction allowed for the use of bespoke form liners based on the Linenopolis theme.

In addition to the use of textured wall finishes it was proposed that the edge treatment of the overbridges incorporate a non-structural parapet. This element would contribute towards ensuring that a level of deterrent was provided to pedestrians attempted to scale the edge of the overbridges and/or throw materials onto the road links beneath. The proposed feature also allowed the incorporation of the Linenopolis theme within a larger area of the structure.

The use of feature lighting to the York Street overbridges was also proposed. The SAG noted the work of Vicki Scuri with respect to urban infrastructure design. Figures 2.3.7, 2.3.8 and 2.3.9 illustrate proposals for future consideration.

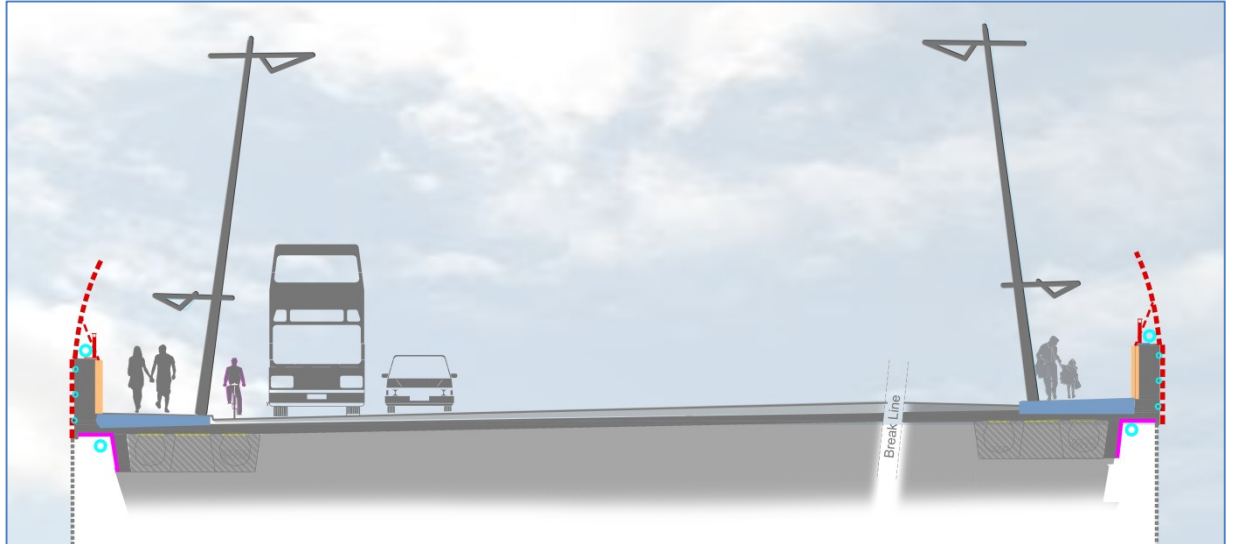


Figure 2.3.7 Typical section through York Street overbridge(s).

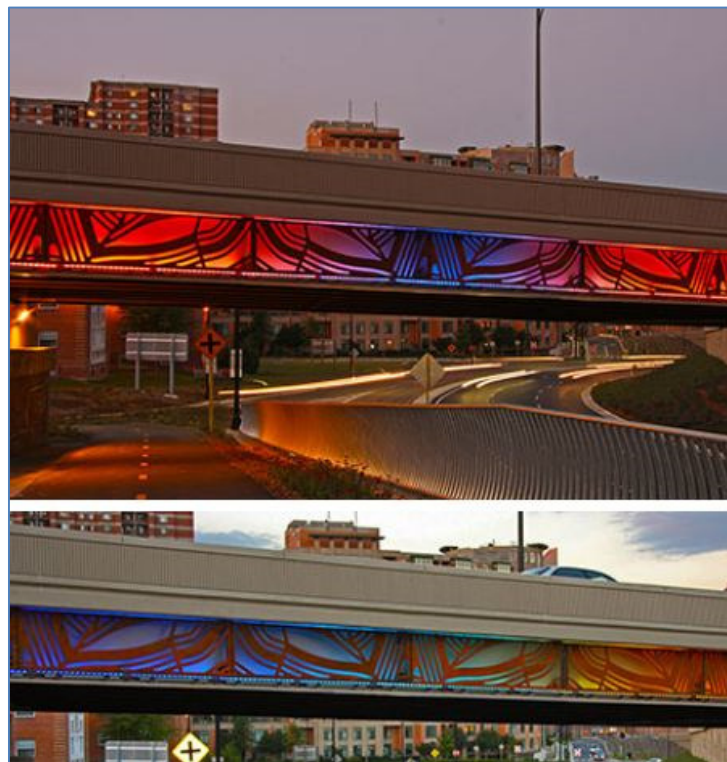


Figure 2.3.8 Examples of feature lighting used on the Arlington Boulevard, Highway 50, USA. Images are reproduced from the Vicki Scuri website: <http://www.vickiscuri.com/project-arlington.html>

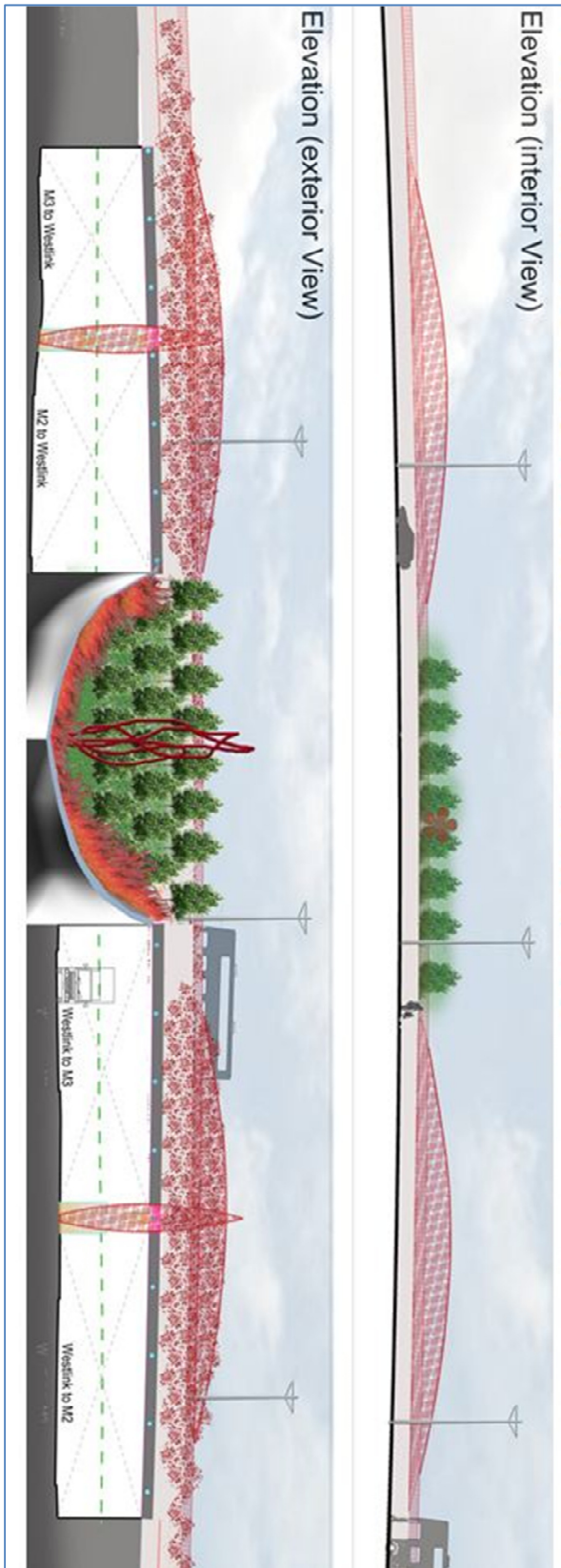


Figure 2.3.8 Elevations of York Street overbridges

2.3.7 **Public Art/Gateway feature**

The SAG discussed the incorporation of public art within the Proposed Scheme. The example of “The Rise” completed at Broadway Roundabout was discussed. It was considered that the current economic climate within local Government may preclude the development of a similar scale of gateway feature for the Proposed Scheme. However a number of locations at the start and end of certain strategic corridors were identified for consideration of features denoting the commencement of the Linenopolis theme.

The SAG representative from the Arts Council noted that in conjunction with the Arts Council for Ireland a commission was proposed to design a piece of public art to commemorate Seamus Heaney. It was suggested that the piece could be sited within the area of the Proposed Scheme and that the identification of possible sites would be facilitated by the Department.

The SAG were supportive of the proposal and it was agreed that potential locations should be identified for consideration.

2.3.8 **Costs of Aesthetic Proposals**

Section pending.

2.4 **Land Use and Setting**

The SAG considered the Proposed Scheme with respect to existing and known committed developments and also longer term planning for the surrounding area. The URS planning team facilitated the SAG by researching and investigating the potential opportunities for the treatment and development of lands surplus to the Interchange scheme. Figure 2.3.9 presents the contextual setting of the Proposed Scheme and illustrates its physical, planning and policy context.

2.4.1 **Surplus land**

Two presentations were made regarding surplus lands, which considered 5 individual parcels that will be available once the Proposed Scheme is completed. These surplus land parcels are shown in Figure 2.3.10. It was noted that under the Proposed Scheme these lands would be secured via suitable boundary fencing but that no specific short term treatment (other than being cleared of construction materials) was proposed.

The strategic context of the surplus lands was acknowledged by the SAG both in regard to their physical location and relationship to other city wide initiatives. In this regard the new role of the Belfast City Council to take on regeneration responsibilities (from April 2016) and prepare Development Plans, following Local Government Reform was acknowledged.

York Street Interchange - Contextual Setting (Physical, Planning and Policy)

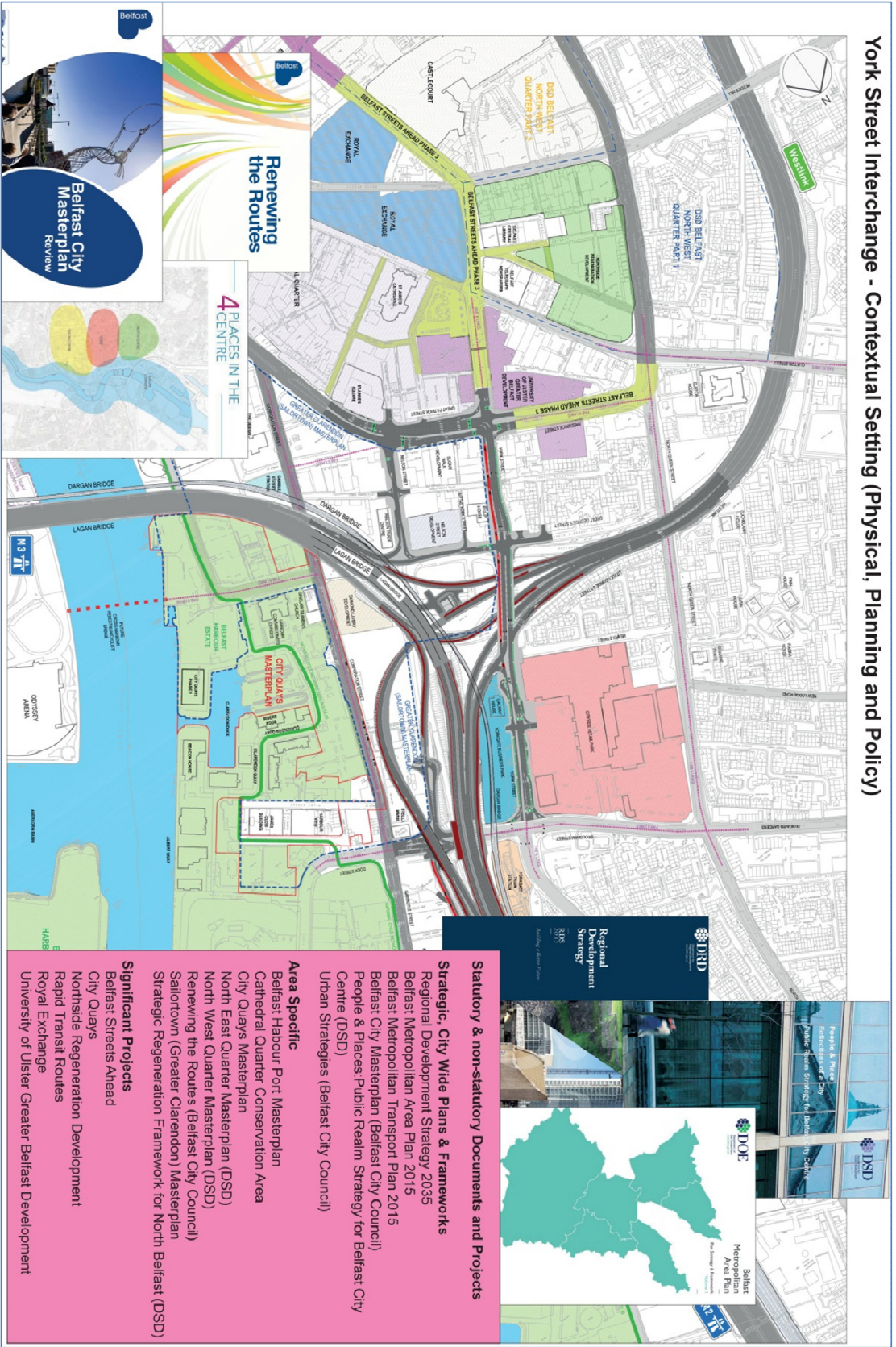
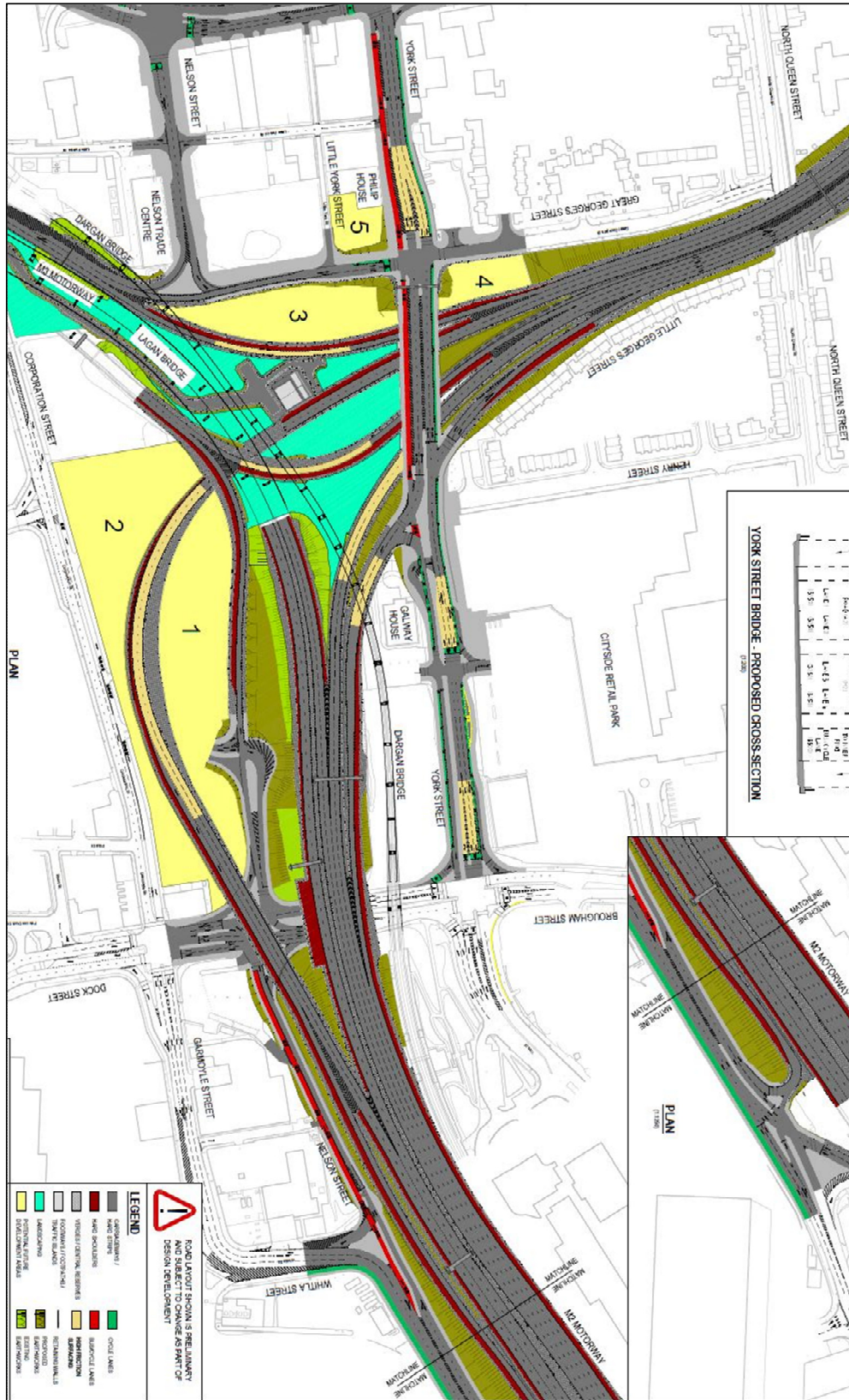


Figure 2.3.11 York Street Interchange setting

Figure 2.3.9 Surplus land parcels (5No)



A rationale for examining the sites was concurred with:

- clear direction/ vision to avoid an ad hoc approach that results in piecemeal and unrelated development contributing little to the overall identity and quality of the area; and
- design-led planning initiative to stimulate and facilitate development that demands design quality response in the short and long term.

Options for use of these surplus lands were considered by reference to a number of case studies. 'Do Nothing', 'Short-Medium Term', and 'Long Term' development options were examined. In response to these options, the following comments were noted by the SAG:

- 'Do Nothing' option (the treatment proposed under the current Proposed Scheme) – this presents a serious security liability and a loss of opportunity, as well as creating a negative gateway image to the city. Mitigation measures such as screening, enhanced boundary fencing and wild flower planting could be required. It was therefore agreed that a "Do-Nothing" option be ruled out.
- Short-Medium term – options included allotments and pop up events, parking and screening and planting. It was agreed that there was merit in creating a vibrant interim use but further detail would be required on roles and responsibilities.
- Long term – options included skate and play parks, outdoor gyms and sports cages, flexible business spaces, small scale social housing, gateway art and light installations, outdoor cinemas. The role of parking was also reviewed but thoughts were expressed by the SAG that a more positive longer term use should be more fully investigated.

The study emphasised the need for interim projects/meanwhile uses to be implemented on the surplus sites, as well as a responsibility upon the DRD to ensure that the sites do not become a lost opportunity. Access to the surplus lands and safe linkages between them were also emphasised.

As a result of the presentations, it was agreed by the Department that the surplus lands would not be treated under the 'Do Nothing' option as identified within the Proposed Scheme. Further consideration needs to be given to the other options discussed but it was accepted that the provision of wildflower planting to the surplus lands should be included as a minimum.

2.4.2 Existing Building Lines

The SAG discussed the setting of the Proposed Scheme with regard to the existing built environment. It was acknowledged that where possible the extent of footway should be such that continuity is provided with existing building lines. Given that proposals with respect to the future use of surplus lands parcels was unknown, it was felt the Greater Clarendon masterplan was the appropriate document to consider the extent of future building development.

It was agreed that the overall extent of footways, particularly those adjacent to the identified surplus lands, be reviewed and rationalised to, where possible, integrate with future potential development.

2.4.3 Greater Clarendon/Sailortown Masterplan

Whilst consultation between the respective teams had been held in connection with the York Street Interchange project and the Greater Clarendon Masterplan, the latter had remained in

draft until further detail of the Proposed Scheme was known. Through the course of the work of the SAG the representative from DSD noted that the draft Masterplan would be reviewed and finalised to more fully reflect the Proposed Scheme. TransportNI representation on the Masterplan steering and working group was noted.

It was agreed that the extent of the Masterplan should incorporate consideration of the five areas of surplus land identified within section 2.4.1 of this report. The Masterplan is expected to be completed and published towards the end of 2015.

2.4.4 ***Rights to Light***

The SAG noted the issue of 'Right to Light' raised within a series of objections received in connection with the Proposed Scheme. The issue related to the widening of the existing Westlink embankment in the vicinity of residential housing along Little Georges Street. The Proposed Scheme required that the existing embankment be revised to accommodate a change in carriageway cross-section resulting in an increase in height (circa 0.65m) and the repositioning of running lanes horizontally closer (circa 3.5m) to the existing boundary with the properties.

The SAG recognised the significance of the matter both in terms of the potential impacts on the residents and upon the project.

3. CONCLUSIONS

3.1 Summary of Aesthetic Theme

The following principles summarise the aesthetic theme discussed with the SAG. These will be adopted within the future development of the Proposed Scheme.

A themed approach inspired by ‘Linenopolis’

- theme predominantly introduced via use of bespoke wall treatments;
- within underpasses treatments will reflect a variety of linen elements/images which overall will relay the story of linen;
- varying level of treatment will be provided on each of the underpasses (M2 – Westlink corridor receiving the greatest level of enhanced finish); and
- ends of certain underpass corridors to incorporate bespoke themed markers.

‘Linenopolis’ theme to be adopted on York Street Bridges to enhance pedestrian experience:

- bespoke wall treatments embedded within structural parapets;
- themed bespoke metal façade/parapet will provide aesthetic form to the bridges and create landmark;
- feature lighting to both external and internal elevations of bridges.

North Queen Street/Henry Street interface:

- undercroft of North Queen Street bridge enlivened with Aluminium Composite Material (ACM) graphics panels;
- community involvement in its development led by BCC/AECOM;
- feature lighting to bridge undercroft;
- acoustic barriers adjacent to the Westlink to incorporate where possible matching ACM graphics panels;
- Henry Street interface detailed to meet concerns of the local community.

Dock Street corridor:

- undercroft of bridge to receive enhanced treatment with ACM graphics panels;
- ‘Linenopolis’ theme to be incorporated with potential connection to Sailortown; and
- feature lighting to be provided on bridge undercroft.

Footway Finishes:

- priority for the York Street corridor with treatment of Great Georges Street/Nelson Street next priority (dependant on funding);

- enhanced finish by selective use of higher specification materials – granite kerbs/insets/banding to offer relief to footway surfaces; and
- decorative street lighting columns to be used on York Street.

Planting:

- retain direct control of landscape design;
- increased number of street trees where feasible; and
- planting blocks designed for seasonal colour and dramatic effect.

Appendix A Linenopolis Theme images

Linenopolis Design Theme

mills
social history
exports
workers
street names



York Street Mill

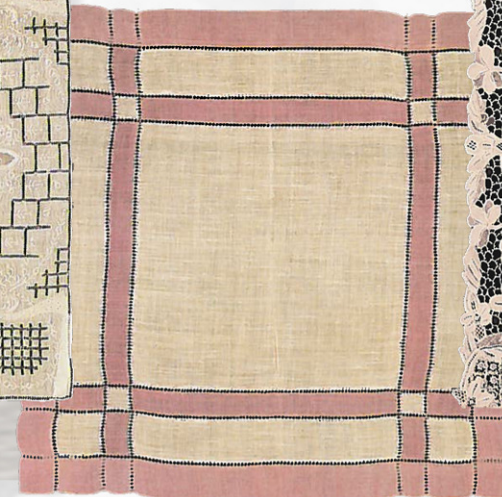
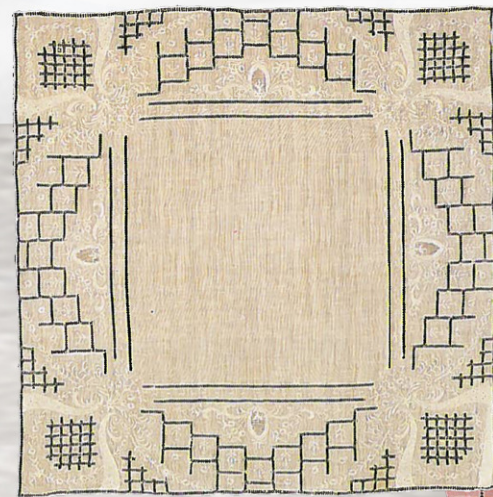
Reference: <http://www.culturenorthernireland.org/features/heritage/industrial-heritage-york-street>



Industrial Heritage Map

Reference: <http://belfasthills.org/history-culture/time-periods/present-day/mills/>

memories
built heritage
songs
architecture



linen patterns

linen designs

Belfast Linen Designer - H. R. Lilley

Reference: H. R. Lilley, artist & designer, 1886-1970
by H.J. Bruce (Belfast Community Relations Council, Cultural Traditions Group (1997))

industrial heritage

Linenopolis Design Theme

Heritage Lace Sample



Reference - <http://www.louisewestlacedesign.co.uk/gallery/nottingham-contemporary>

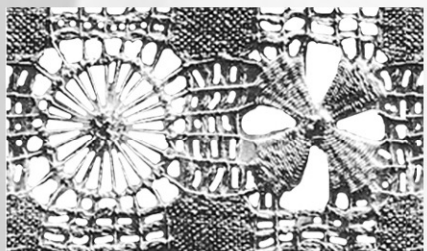
Heritage Lace Design on the Gallery Building Facade



Lace design created in concrete formliner

Precedent Project
'Nottingham Contemporary' Gallery Building

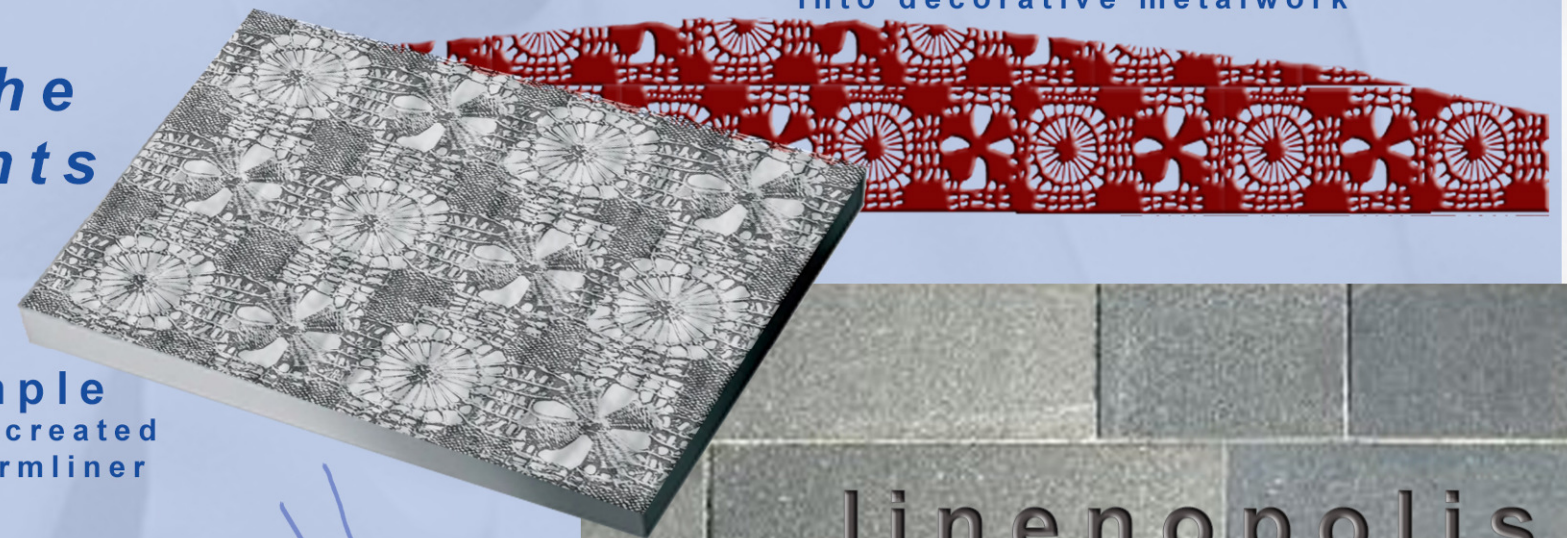
Heritage Linen
(potentially sourced from the Ulster Folk Museum Collection, subject to any copyright requirements)



Sample - Woven Linen

Linen patterns & themes embedded in the project elements

Sketch Example
of a linen pattern created into decorative metalwork



Sketch Example
of a linen pattern created into a concrete formliner

Sketch Example
of the theme engraved in the paving

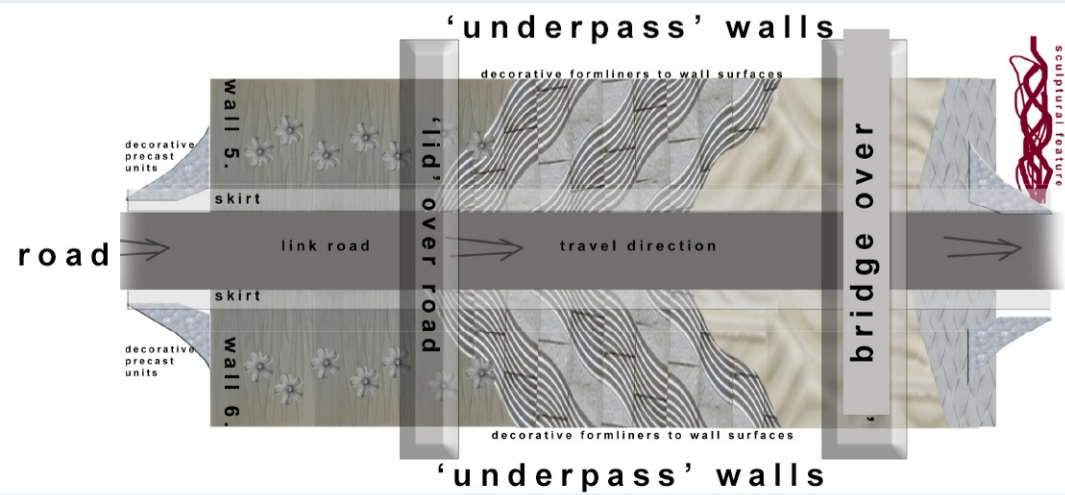


Sketch Example
Underpass wall panels decorated with themed formliners

Feature Underpass Walls - Sketch Diagrams

'Story of Linen' Journey

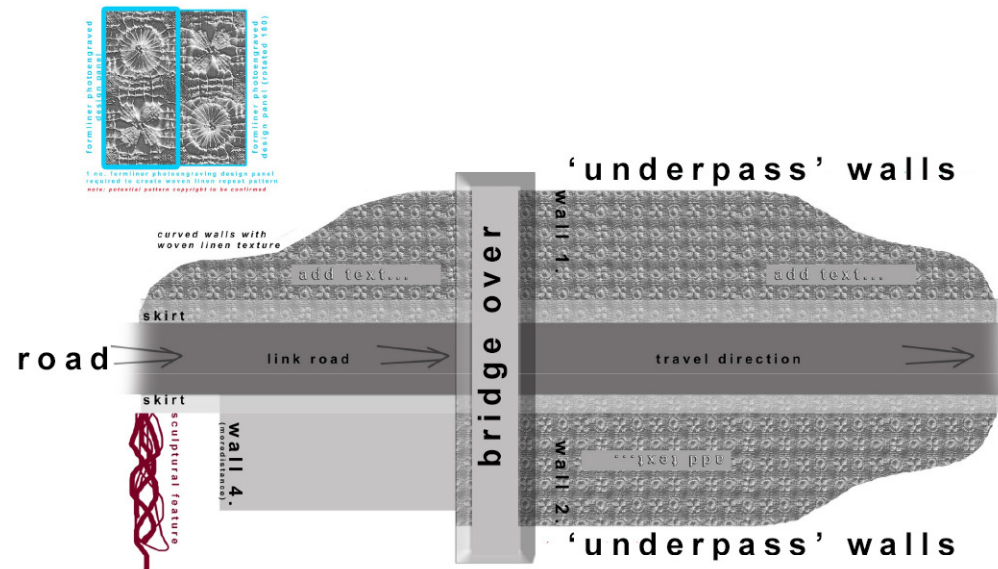
- comprising several bespoke 3D formliners



Precedent Projects (USA) Feature Underpass Walls

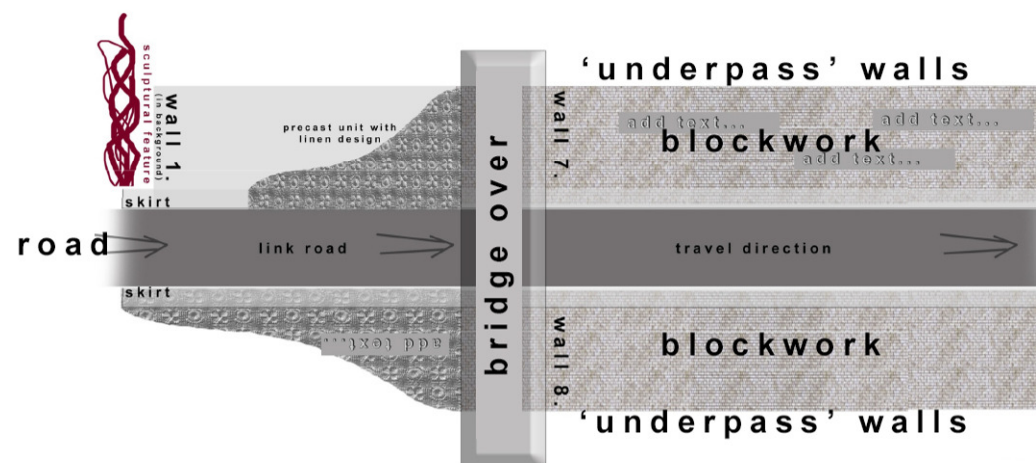


'Woven Linen' Journeys - single bespoke 3D formliner



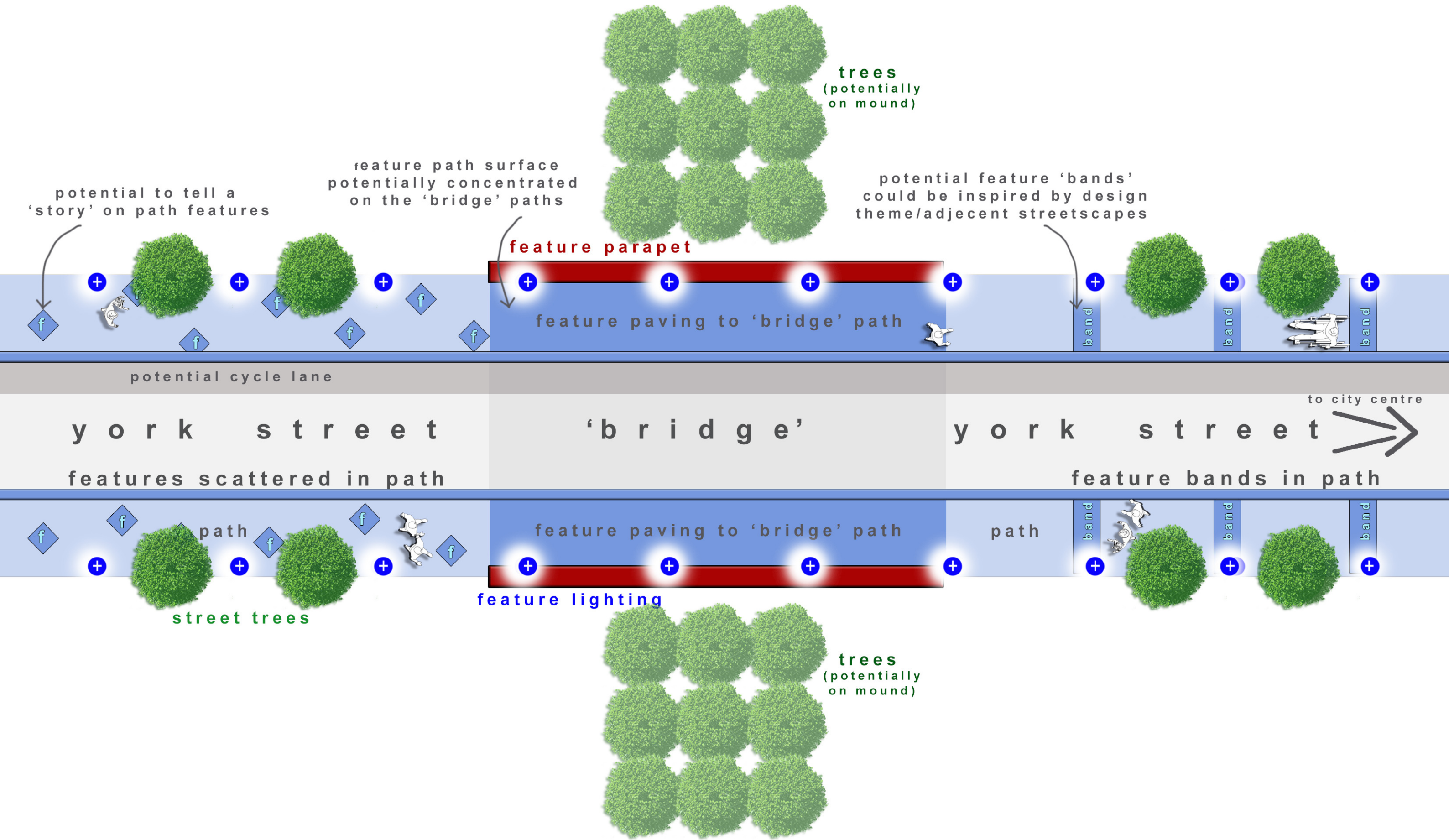
reference <http://creativeformliners.com/gallery/bridges/us19-sr60-intexchange-clearwater-florida?me>

'Wall end' Features



Feature Underpass Walls - Photosketch





York Street - concept Plan Diagram