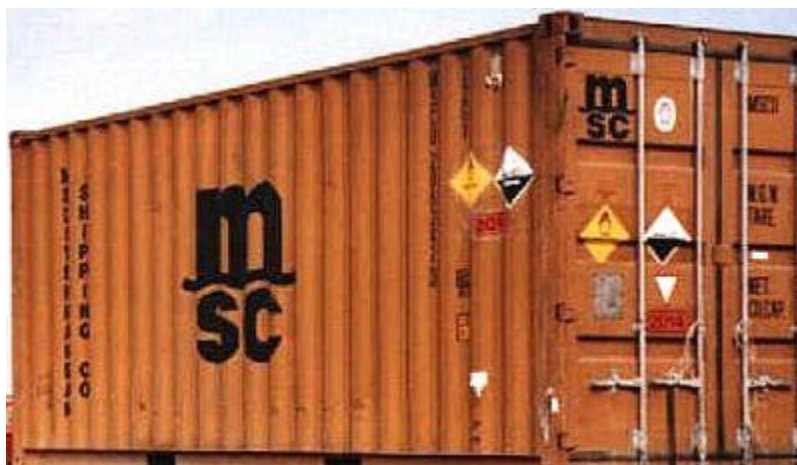




# WARRENPOINT PORT

## DANGEROUS GOODS POLICY ( V1) 2018



## **Document and Distribution Control**

This document is maintained, controlled and formally reviewed by Port Operations

This procedure is maintained electronically and the latest issue will be posted on the WHA website. Printed copies shall be treated as uncontrolled documents. Hard copies are available on request from Port Operations

Port Operations should be notified of any change of circumstances that may warrant a change to this procedure. Comments or queries relating to the contents of this document should be referred to:

Harbour Master  
Warrenpoint Harbour Authority  
The Docks  
Warrenpoint  
BT34 3JR

Tel: 028 417 73381  
Fax: 028 41773962  
Email: cdowling@warrenpointharbour.co.uk

## **Document Status**

### Record of Review

<b>ISSUE NO</b>	<b>DATE</b>	<b>NAME</b>
1	30/01/2018	Catriona Dowling
2		
3		
4		
5		

# CONTENTS

- 1. Introduction**
  - i. Scope
  - ii. Responsibilities
- 2. Legislative Requirements**
  - i. Dangerous Substances in Harbour Areas
  - ii. Explosives in Harbour Areas
  - iii. IMDG Code
- 3. Information Required by Warrenpoint Harbour Authority**
- 4. Management of Dangerous Cargo**
  - i. Packages
  - ii. Liquid Bulk cargoes (including LPG)
  - iii. Solid Bulk Dangerous Cargoes
  - iv. Class 1 Cargoes
  - v. Class 7 Cargoes
- 5. Fumigation**
- 6. Bunkering**
- 7. Transit Storage Policy**
- 8. Security Provisions**
- 9. Training and Awareness Requirements.**
- 10. Vessel Traffic Movement Restrictions**
- 11. Emergency Requirements**
- 12. Audit and Review Process**

# 1. Introduction

## I) Scope

This Policy is prepared to advise all parties of the requirements of Warrenpoint Harbour Authority to manage Dangerous Goods and ensure the safe transportation of these Goods through the Port Jurisdiction as defined by the Warrenpoint Harbour Authority Order (Northern Ireland) 1971. It is based on legislative requirements and IMO Codes and Recommendations.

The Policy covers all Dangerous Goods in the transport chain, including oils, noxious liquid chemicals and gases carried in bulk, solid bulk materials possessing chemical hazards, solid bulk materials hazardous only in bulk, harmful substances in packaged form (covered by Annex III of MARPOL 73/78) as well as dangerous goods in packaging form (covered by the IMDG Code). The Policy also covers the movement of empty and uncleaned tanks that have contained Dangerous Goods.

The Policy does not apply to Dangerous Substances used in the Port Area, on board vessels or for storage within the Harbour Estate as these are controlled by other legislation. The following table summarises the geographic scope of this policy

<b>Within scope of Policy</b>	<b>Outside scope of Policy</b>
Vessels navigating within Port limits	Fixed Storage locations
Vessels alongside Operational Quays	Fixed pipework from berths to fixed storage locations
Transit storage of Dangerous Goods on Quays	Use of Dangerous Goods by WHA, Tenants and other Harbour users
Bunkering Operations on Quays	Carriage of Dangerous Goods on Harbour Roads (enforced by HSENI)
Fumigation Operations on Quays	Dangerous Goods used on board on vessels
	Storage of Class 1 explosives held in MSER licensed sites (enforced by Firearms and Explosives Branch, Dept. of Justice)

## ii) Responsibility

The Responsibility for Policy implementation rests with the Harbour Master in accordance with his/her duties as defined by

- a) Warrenpoint Harbour Authority Order (Northern Ireland) 1971
- b) Dangerous Substances in Harbour Areas Regulations (N.I.) 1991
- c) Explosives in Harbour Areas Regulations (N.I.) 1995

## **2. Legislative Requirements**

### **i) Dangerous Substances in Harbour Areas Regulations (N.I.) 1991**

The Dangerous Substances in Harbour Areas Regulations (DSHAR) is the controlling legislation for the carriage, loading, unloading and storage in harbour and harbour areas.

The regulations provide the Harbour Master with the power to prohibit or remove Dangerous Goods if in his/her opinion the condition of the substance or the vehicle or vessel carrying it creates a risk to health or safety

In summary, it requires

- A consignor or forwarding agent who intends to bring a dangerous substance into a harbour area to give the Harbour Master at least 24 hours' notice of his intention
- Vessels within Port limits to display a red flag during daylight hours and an all-round red light during the hours of darkness
- All persons involved in the handling of dangerous substances to be properly trained and take necessary precautions to avoid fire or explosion
- Vessels carrying substances in bulk to hold a suitable certificate of fitness
- Vessels carrying or recently carried liquid dangerous substances in bulk to be prohibited from cleaning or ventilating tank spaces, discharging contaminated ballast and slops unless the Harbour Master or Authorised Assistant Harbour Master has given permission
- Freight containers for export to be accompanied with a container packing certificate as part of the transport documentation.
- Tanks and receptacles used for packaged Dangerous Substances should be suitable for purpose and marked and labelled in accordance with the IMDG Code
- The Harbour Authority to prepare an emergency plan for dealing with emergencies involving Dangerous Substances.
- Vessels carrying Dangerous Substances to immediately inform the Harbour Authority
- Transfer of Dangerous Substances via pipeline between vessels to be permitted only with permission by the Harbour Master in writing.

## **ii) Explosives in Harbour Areas Regulations (N.I.) 2005**

The Explosives in Harbour Areas Regulations supplement DSHAR and impose the following additional controls

- Operations involving Class 1.1, 1.2, 1.3 and 1.5 substances to be only undertaken in accordance with an Explosives Licence granted by the Firearms and Explosives Branch of the Department of Justice. The Licence impose additional safety and security requirements for these operations as well as a requirement to keep appropriate records.
  
- Appointment of an Explosives Security Officer to
  - oversee the movement.
  - ensure the licence requirements are met.

## **iii) IMDG Code**

The provisions outlined in the Code are now mandatory under SOLAS and outlines the requirements for the carriage of packaged Dangerous Substances by sea. The IMDG Code is based on an internationally agreed system which

- Classifies dangerous goods together based on the hazards they present on transport
- Specifies packaging/tanks which are of appropriate strength and which will prevent the goods escaping
- Specifies hazard warning labels, placards and other marks to identify dangerous goods in transport.
- Requires standard documentation to be provided when dangerous goods are being transported
- Lays down principles for ensuring that incompatible dangerous goods are kept apart
- Lays down principles for where to place dangerous goods on board ship to ensure safe transport
- Provides emergency response advice for dangerous goods involved in a fire or spillage on board ship

The Segregation rules outline in Chapter 7.2 of the Code can be followed for transit quayside storage.

### 3. Information Required by Warrenpoint Harbour Authority

#### i) Advanced Notification for Ship Arrivals

Advanced notification for arriving vessels with Dangerous Goods should be received by Port Operations not less than 24hrs before arrival. For vessels with passage time < 24hrs from departure port, the notification should be received by Port Operations at the time of Departure from the Last Port <sup>1</sup>. Failure to notify may result in the vessel's refused / delayed entry into the Harbour. The following information outlined in the tables below must be received by the port in writing. Information from the ferry operators should be sent via the following email addresses:

[cdowling@warrenpointharbour.co.uk](mailto:cdowling@warrenpointharbour.co.uk)

[ccunningham@warrnepointharbour.co.uk](mailto:ccunningham@warrnepointharbour.co.uk)

[itaylor@warrenpointharbour.co.uk](mailto:itaylor@warrenpointharbour.co.uk)

[bleicester@warrenpointharbour.co.uk](mailto:bleicester@warrenpointharbour.co.uk)

[akielty@warrenpointharbour.co.uk](mailto:akielty@warrenpointharbour.co.uk)

[pmconville@warrenpointharbour.co.uk](mailto:pmconville@warrenpointharbour.co.uk)

[security@warrenpointharbour.co.uk](mailto:security@warrenpointharbour.co.uk)

#### Packaged Dangerous Goods (including tank containers)

Information	Format
Name and IMO number of Vessel Agent ETA to Fairway Buoy	Accepted via email
Manifest List itemising <ul style="list-style-type: none"> <li>• Proper Shipping Name</li> <li>• UN number</li> <li>• Class</li> <li>• Packing Group</li> <li>• Quantity</li> <li>• Number and type of packages</li> <li>• Flashpoint (°C) – if applicable</li> <li>• Marine Pollutant Status</li> <li>• Container or Vehicle Number</li> <li>• Supplementary information outlined in IMDG 5.4.1.4.3</li> </ul> <p>-----</p> In addition, for Class 1 and Class 7 <ul style="list-style-type: none"> <li>• Class Division and Compatibility Group ((Class 1)</li> <li>• Net Explosive mass in kg. (Class 1)</li> <li>• Name and Max. Activity of Radionuclide (Class 7)</li> <li>• Package type &amp; Transport Index (Class 7)</li> </ul>	Completed inbound hazmat worksheet on CERS3 workbook  (Ferry operations – hazardous manifest)  -----  SITPRO Dangerous Goods Note with <ul style="list-style-type: none"> <li>• Packing Declaration</li> <li>• Name and contact details for consigner and consignee</li> </ul>
Precise Stowage of goods on board	Completed inbound hazmat worksheet on CERS3 workbook (Ferry operations – hazardous manifest)
Condition of Dangerous Cargoes if any undue hazard is likely to arise Vessel Defects	Report to LPS "Warrenpoint Harbour Radio" (Channel 12 or 028 41773381)

- 1 Notification of Class 1.4 and 7 goods should be received no less than 24hours from arrival.  
Notification of all other Class 1 goods should be received no less than 48 hours from arrival**

## Bulk Dangerous Goods (Solids, Liquids and Liquefied Gases)

Information	Format
Name and IMO number of Vessel Agent ETA to Fairway Buoy	Accepted via email
List of products carried in Bulk <ul style="list-style-type: none"> <li>• Proper Shipping Name</li> <li>• UN number</li> <li>• Class</li> <li>• Packing Group</li> <li>• Quantity</li> <li>• Flashpoint (°C) – if applicable</li> <li>• Marine Pollutant Status</li> </ul>	Completed inbound hazmat worksheet on CERS3 workbook Vessel's stowage / tank plan
Precise location of goods on board (indicating those to be unloaded and those to be left on board)	Vessel's stowage / tank plan
Condition of Dangerous Cargoes if any undue hazard is likely to arise Vessel Defects	Report to LPS "Warrenpoint Harbour Radio" (Channel 12 or 028 41773381)



## ii) Advanced Notification for Ship Departures

Advanced notification for Departing vessels with Dangerous Goods should be received by Port Operations not less than 2 hours before departure. Failure to notify will result in the vessel's refused / delayed sailing out of the Harbour.

The following information outlined in the tables below must be sent via email. Information should be sent via the following email addresses:

[cdowling@warrenpointharbour.co.uk](mailto:cdowling@warrenpointharbour.co.uk)

[ccunningham@warrnepointharbour.co.uk](mailto:ccunningham@warrnepointharbour.co.uk)

[itaylor@warrenpointharbour.co.uk](mailto:itaylor@warrenpointharbour.co.uk)

[bleicester@warrenpointharbour.co.uk](mailto:bleicester@warrenpointharbour.co.uk)

[akielty@warrenpointharbour.co.uk](mailto:akielty@warrenpointharbour.co.uk)

[pmconville@warrenpointharbour.co.uk](mailto:pmconville@warrenpointharbour.co.uk)

[security@warrenpointharbour.co.uk](mailto:security@warrenpointharbour.co.uk)

### Packaged Dangerous Goods (including tank containers)

Information	Format
Name and IMO number of Vessel Agent ETD	Accepted via email
Manifest List itemising <ul style="list-style-type: none"> <li>• Proper Shipping Name</li> <li>• UN number</li> <li>• Class</li> <li>• Packing Group</li> <li>• Quantity</li> <li>• Number and type of packages</li> <li>• Flashpoint (°C) – if applicable</li> <li>• Marine Pollutant Status</li> <li>• Container Number</li> <li>• Supplementary information outlined in IMDG 5.4.1.4.3</li> </ul>	Completed outbound hazmat worksheet on CERS3 workbook  (Ferry operations – hazardous manifest)
----- In addition, for Class 1 and Class 7 <ul style="list-style-type: none"> <li>• Class Division and Compatibility Group (Class 1)</li> <li>• Net Explosive mass in kg. (Class 1)</li> <li>• Name and Max. Activity of Radionuclide (Class 7)</li> <li>• Package type &amp; Transport Index (Class 7)</li> </ul>	SITPRO Dangerous Goods Note with <ul style="list-style-type: none"> <li>• Packing Declaration</li> <li>• Name and contact details for consigner and consignee</li> </ul>
Precise Stowage of goods on board (indicating those to be loaded and those left on board)	Completed outbound hazmat worksheet on CERS3 workbook (Ferry operations – hazardous manifest)
Condition of Dangerous Cargoes if any undue hazard is likely to arise Vessel Defects	Report to Port Control "Warrenpoint Harbour Radio" (Channel 12 or 028 41773381)

- 1 Notification of Class 1.4 and 7 goods should be received no less than 24hours from departure.  
Notification of all other Class 1 goods should be received no less than 48 hours from departure.**

## Bulk Dangerous Goods (Solids, Liquids and Liquefied Gases)

Information	Format
Name and IMO number of Vessel Agent ETD	Accepted via email
List of products carried in Bulk <ul style="list-style-type: none"> <li>• Proper Shipping Name</li> <li>• UN number</li> <li>• Class</li> <li>• Packing Group</li> <li>• Quantity</li> <li>• Flashpoint (°C) – if applicable</li> <li>• Marine Pollutant Status</li> </ul>	Completed outbound hazmat worksheet on CERS3 workbook Vessel's stowage / tank plan
Precise location of goods on board (indicating those to loaded and those to be left on board)	Vessel's stowage / tank plan
Condition of Dangerous Cargoes if any undue hazard is likely to arise Vessel Defects Defects in the Cargo Containment and Handling system Confirmation of valid Certificates <ul style="list-style-type: none"> <li>• Certificate of Fitness</li> <li>• Document of Compliance</li> <li>• International Oil Pollution Prevention Certificate (if applicable) or</li> <li>• International Pollution Prevention Certificate for the Carriage of Noxious Substances in Bulk (NLS Certificate)</li> </ul>	Report to Port Control "Warrenpoint Harbour Radio" VHF CH12 OR 02841773381 if status different to Arrival

### iii) Hazardous/ Dangerous Goods Transit Storage lists

Ro-Ro and Lo-Lo will record and forward a list to Port Operations preferably in Microsoft Excel format prior to import/ export of Dangerous Goods to the following e-mail addresses

[cdowling@warrenpointharbour.co.uk](mailto:cdowling@warrenpointharbour.co.uk)

[ccunningham@warrnepointharbour.co.uk](mailto:ccunningham@warrnepointharbour.co.uk)

[itaylor@warrenpointharbour.co.uk](mailto:itaylor@warrenpointharbour.co.uk)

[bleicester@warrenpointharbour.co.uk](mailto:bleicester@warrenpointharbour.co.uk)

[akielty@warrenpointharbour.co.uk](mailto:akielty@warrenpointharbour.co.uk)

[pmconville@warrenpointharbour.co.uk](mailto:pmconville@warrenpointharbour.co.uk)

[security@warrenpointharbour.co.uk](mailto:security@warrenpointharbour.co.uk)

During the standard opening hours of the port, these lists should include the following information for each container / cargo transport unit transporting Dangerous Goods within the Harbour Areas.

- Container Number / Mark Reference No
- Date of Arrival
- Expected date of departure
- Net weight
- Proper Shipping Name
- Class
- Subsidiary risk
- Packing Group
- Subsidiary Risk(s)
- Flashpoint (°C) – if applicable
- Marine Pollutant Mass (kg) Gross/Net
- Stowage position on board

**Operators will ensure that Dangerous Goods are not kept within the Harbour restricted area for periods longer than specified by the Transit storage policy in Section 7.**

Current storage lists will be maintained by Ro-Ro and Lo-Lo operators and be made available to the Port Authority or relevant Emergency Services on request. The information should be made readily available on site in the case of an Emergency.

## 4. Movement of Dangerous Goods

### i) **Packaged Goods (including tank containers)**

Operators will monitor the condition and movement of packaged goods and will ensure Dangerous Goods notes (DGNs) are passed to and from each vessel.

**DGNs for Transit goods should be held centrally where access to them can be assured in the case of an emergency.**

Packaged Goods moved within the scope of this policy should be labelled and placarded in accordance with the IMDG Code.

**Packaged Goods (including tank containers) can only be stored in accordance with the Transit storage policy outlined in Section 7.**

Packaged goods under fumigation or planned to be fumigated should be stored in a designated area. Entry into such areas should be restricted and appropriate warning signs displayed

Storage areas on site should be marked on site plans. Copies of these plans should be made available to Warrenpoint Harbour Authority and the NI Fire and Rescue Service.

Sites used for storage, where possible, should be located where management and security may keep them under frequent inspection and supervision. Ideally containers should be stored at ground level with doors unobstructed so that access may be obtained in an emergency.

External Storage and Handling areas used for Explosive and Flammable substances must be designated as areas where smoking and other sources of ignition are prohibited and where only electrical equipment of a type safe for use in a flammable or explosive atmosphere. Hot work and the use of any equipment or activity which may lead to a fire or explosion hazard within 2 metres of these areas will be prohibited unless authorised by permit by the Port Engineer.

**Any Incident involving Dangerous Goods (including damaged packages and tanks) MUST be immediately reported to the Port Operators and Port Control (Channel 12 or 41773381)**

Warrenpoint Harbour authority will undertake routine and random audits of the facilities provided and may inspect DG documentation for goods stored on site.

Any non-conformances found as a result of these inspections will be recorded as per WHA's Non-conformance Procedure under the ports Safety Management System.

## ii) **Liquid Bulk Cargoes (including Liquefied Gas)**

Ship to Ship transfers of Liquid Bulk Cargoes (with the exception of bunkering operations) are not permitted within Harbour limits. Ship to ship bunkering operations require prior notification and the authorisation of the Harbour Master prior to commencement.

Flexible pipes or loading arms should be checked for damage before use.

An ISGOTT ship/shore safety checklist must be completed before Liquid Bulk cargo operations commence.

Frequent checks should be made by shore based staff and ship's crew to ensure that agreed pumping rates are not exceeded.

Warning notices should be placed at all entrances and approaches to the berth during handling operations.

Spill containment equipment should be available and in a "ready to use" condition

Hot work and the use of any equipment or activity which may lead to a fire or explosion hazard within these areas will be prohibited unless authorised by permit by the Port Engineer.

**Any Incident involving Liquid Bulk Cargoes MUST be immediately reported to Port Control. (Channel 12 or 028 417 73381)**

Warrenpoint Harbour Authority will undertake routine and random audits of the facilities provided and may inspect documentation used to manage cargo transfer arrangements.

Any non-conformances found as a result of these inspections will be recorded under WHA's Non-conformance procedure and is reportable under the Safety Management System.

## iii) **Solid Bulk Dangerous Cargoes**

Berth Operators / Stevedores and the vessel concerned should ensure that loading and unloading operations take place in accordance with the International Maritime Solid Bulk Cargo (IMSBC) Code. Spills of material should be cleaned up immediately and if unsalvageable, disposed as hazardous waste.

Oxidising (Class 5.1) substances should be transported, handled and stowed in a manner that prevents in so far as reasonably practicable, contamination with combustible or carbonaceous materials. Oxidising substances should be kept away from any source of heat or ignition. Vessels carrying Ammonium Nitrate based fertiliser in bulk (classified as Class 5.1 or not) will be prohibited to bunker.

All necessary practicable precautions should be taken to prevent and minimise the emission of harmful dusts and to protect persons and the environment from them.

All reasonable steps should be taken to limit ignition sources in the cargo transfer area if there is a potential risk of a dust explosion.

Warrenpoint Harbour Authority will undertake routine and random audits of the facilities provided and may inspect documentation used to manage cargo transfer arrangements. Any non-conformances found as a result of these inspections will be recorded as per WHA's Non-conformance procedure under the Safety Management System

#### iv) **Class 1 Cargoes**

Requests for Explosives movements (detailing Class, Division and NEM) will be made to Port Operations. Requests should be made at least 48 hours (24 hours for Class 1.4) prior to the movement.

Port Operations will check if the movement is permitted and confirm what working berth / ships position will be used in accordance with the Explosives license. Port Operations will nominate an Explosives Security Officer to co-ordinate the movement. Notification of each movement will be made to the Firearms and Explosives Branch of the Department of Justice (DoJ)

The Explosives Security Officer (ESO) will liaise with the Consignor/ee, Carrier, Berth Operator and will agree the following

- a. Detailed movement plan (timings for vessels, transport, escorts etc.)
- b. Identification of safeguarding distances on the quay
- c. Arrangements for clearing and policing the safeguarding distances
- d. Nominated person (with relevant contact detail) to advise on condition of the Explosive cargo if required.

Explosives in compatibility group L will not be handled unless the Nominated person outlined can demonstrate in writing that the special risk that the material may exhibit had been effectively isolated.

A PSNI escort is required for all movements where the NEM of Class 1.1 material exceeds 50kg. Escorts can also be arranged privately by the consignor/ee

Imported Explosives **MUST** be transported off-site when landed. At all possible explosives should be unloaded as soon as reasonably practicable following vessel's arrival.

Explosives for export **MUST** arrive on the quay at a time close to vessel departure (agreed by the nominated ESO) and be loaded immediately onto the vessel.

The ESO will co-ordinate the movement and will be responsible for the maintenance of the Column 2,3 and 4 distances as outlined in the relevant Explosives Licence.

**Temporary storage of Class 1 goods will not be permitted on Warrenpoint Harbour Operational Quays and Wharfs. Storage of Class 1 material may only be permitted in other areas of the Harbour Estate if held under a licence granted by the Firearms and Explosives Branch of the Department of Justice in accordance with the Manufacture and Storage of Explosives Regulations.**

**Cargo Handling areas used for Explosive substances must be designated as areas where smoking and other sources of ignition are prohibited and where only electrical equipment of a type safe for use in an explosive atmosphere. Radar and radio transmitters should not be used in close proximity to Explosives during the handling process**

Vessels involved in the handling of explosives will not be permitted to take bunkers.

In an Emergency situation, the PSNI will be contacted by the ESO if PSNI officers are not present. The Warrenpoint Harbour Authority Major Incident / Emergency Plan (an agreed multi-agency plan) provides guidance for responders to Harbour incidents.

v) **Class 7 Cargoes**

Requests for Radioactive movements will be made to Port Operations. Requests should be made AT LEAST 24 hours prior to the movement.

Notification of each movement will be made to NIEA's Industrial Pollution and Radiochemical Inspectorate.

Port Operations will liaise with the Consignor/ee, Carrier, Berth Operator and will agree the following: Detailed movement plan (timings for vessels, transport, escorts etc.)  
Nominated Radiological Protection Advisor (with relevant contact detail) to advise on condition of the cargo if required.

Imported Radioactive Material MUST be transported off-site when landed. At all possible explosives should be unloaded as soon as reasonably practicable following vessel's arrival.

Radioactive Material or export MUST arrive on the quay at a time close to vessel departure and be loaded immediately onto the vessel

Persons involved during Class 7 operations should limit their working time and distance from the container or vehicle containing the package. Working at twice the distance away will reduce the potential dose received by a factor of 4.

In an Emergency situation, the PSNI and NIFRS will be contacted by Warrenpoint Harbour Master. The Warrenpoint Harbour Authority Major Incident / Emergency Plan (an agreed multi-agency plan) provides guidance for responders to Harbour incidents.

## 5. Fumigation

Port Operations must be notified in advance of impending arrival of Cargo Transport Units (CTUs) or bulk cargoes under fumigation. The notification should be received not less than 24hrs before arrival. For vessels with passage time < 24hrs from departure port, the notification should be received by Port Operations at the time of Departure from the Last Port. Failure to notify may result in the vessel's refused / delayed entry into the Harbour.

The notification including a copy of the Fumigation certificate should contain the following

- Cargo or the material fumigated
- Fumigant used
- Application method
- Quantity and concentration of fumigant
- Date of application of the fumigant
- Name and contact of the fumigator-in-charge for clearance
- A copy of Fumigation Risk Assessment

Fumigated Import Bulk cargo vessels will proceed to anchor to ascertain their fumigation condition by a competent person. Vessels will not pass in until relevant Gas free clearance has been given for all holds to be discharged

Clearance certificates should be issued by a competent and responsible person when a CTU or cargo space has been sufficiently ventilated. Entry is NOT permitted in these areas unless a clearance certificate has been raised

Export Bulk Cargoes may be fumigated subject to the prior notification given above. The vessel will be required to shift to a suitable isolated berth (away from public or other working areas) at the vessel's cost.

Ships under fumigation will ensure that entry is prohibited in cargo spaces. Care should also be taken if the vessels accommodation is located next to fumigated cargo spaces

Port areas used for fumigation purposes must be clearly identified and fenced off. Guidance will be provided by the fumigator- in charge.

Fumigation warning signs MUST be posted

- at port areas used for fumigation
- on fumigated CTUs
- outside cargo spaces containing fumigated cargoes on board ship



## 6. Bunkering

“Bunkering” is taken to mean the transfer of liquid hydrocarbons, intended for the main propulsion and/or operation of the auxiliary machinery of a vessel or a liquid intended for lubricating the vessels engine or her other machinery including slops and bilge waters.

The Agent / Master of a vessel intending to Bunker, shall give advanced Notification in writing seek permission from the Harbour Master, not less than 24 Hours in advance of the intention to Bunker. If Bunkers are received on a regular basis, advance notice can be given on a weekly basis for the week ahead, indicating the name of the vessel, location of the bunkering operation, time of bunkering and means of transfer (barge or road tanker). The type and Quantity of bunkers to be transferred should also be indicated.

Organizations' wishing to bunker must provide Port Operations with a copy of their current Bunkering Risk Assessment and all tanker drivers must have completed for induction training to gain access to port. Port Operations will assess the proposed Bunkering Operation.

The agent will ensure the ship cordons off the working area during bunkering operations. A Bunkering Safety Checklist is to be completed prior to the commencement of Bunkering by a Competent Person from the Vessel and the Road Tanker Driver or Bunker Barge Master. This Safety Checklist should meet the requirements as indicated by The International Safety Guide for Oil Tankers & Terminals (ISGOTT) Bunkering Safety Checklist. Bunkering cannot commence until this Checklist is completed and signed by both parties.

The Checklist should include but is not limited to the following:

- There are adequate NO SMOKING signs positioned and being observed
- There is adequate Fire Fighting Appliances available
- Scuppers are firmly closed
- Vessel is securely moored
- Bunkering Hoses are properly maintained and in good condition
- Bunkering Pipes which are not in use are effectively blanked.
- Bunkering Hoses have sufficient play and are adequately supported
- Bunkering connection has been provided with a good seal.
- There is a well tightened bolt in every bolt hole of the bunker hose joining flanges and the vessels manifold.
- The tank has sufficient Ullage to receive bunkers
- The pipeline and valves to the tanks / pumps are correctly set
- There is a sufficient overflow container under the hose connections
- Cargo Handling or other operations in progress will not cause a hazard to the bunker operations or vice-versa?
- There is an agreed communications system established between the vessel being bunkered and the supply vehicle.

## Ship & Road Tanker Transfer Safety List

- Transfer operations must be notified to WHA Operations in advance of Road Tanker arriving.
- Transfer Operation are only to take place during Daylight Hours.

<b>Ship:</b>	<b>Road Tanker:</b>	
<b>Date:</b>	<b>Time:</b>	<b>Berth:</b>

**Instructions**

All questions must be answered affirmatively to ensure safety of operations. If an affirmative answer is not possible the reason is to be given and agreement reached between the road tanker and ship on appropriate precautions. Where a question is not applicable a note to that effect is to be entered under 'remarks'. **If at any time safety is endangered by any action on the part of the ship or road tanker or any equipment under their control, the operation should cease immediately.**

Product	Quantity	Tank Capacity	Free Space	Max. rate
			<b>Ship's Officer</b>	<b>Road Vehicle</b>
1	Is the ship securely moored?			
2	Is there safe access between shore and ship?			
3	Are sufficient personnel assigned both on ship and ashore to ensure a safe operation?			
4	What are the ship/shore communications?			<b>STOP</b> ship / shore
5	Have emergency shutdown procedures been agreed?			
6	Are all ship scuppers closed/plugged?			
7	Are unused connections blanked and valves closed?			
8	Is road vehicle and equipment in a suitable location?			
9	Are naked light requirements observed?			
10	Are 'no smoking' and 'bunkering in progress' signs available and displayed?			
11	Is fire fighting equipment deployed on board and ashore and ready for use?			
12	Is a sufficient 'save-all / drip tray' located under the hose connections?			
13	Are hoses in good condition?			
14	Are all hoses and joints secured and supported correctly?			
15	Have all persons involved sighted and understood an agreed contingency plan?			

Declaration: We have checked, jointly where appropriate, the items on this checklist and have satisfied ourselves that the entries we have made are to the best of our knowledge. Arrangements have been made to carry out repetitive checks as necessary.



<b>Ship's Officer</b>	<b>Road Vehicle</b>
Name:	Name:
Signature:	Signature:
Date:	Date:
Time:	Time:

## Ship to Ship Transfer Safety List

- **Transfer operations must be notified to WHA Operations in advance of barge arriving.**

Ship:	Bunker Barge:
-------	---------------

Date:	Time:	Berth:
-------	-------	--------

- **Transfer Operation are only to take place during Daylight Hours.**

**Instructions**

All questions must be answered affirmatively to ensure safety of operations. If an affirmative answer is not possible the reason is to be given an agreement reached between the barge and ship on appropriate precautions. Where a question is not applicable a note to that effect is to be entered under 'remarks'. **If at any time safety is endangered by any action on the part of the ship or barge or any equipment under their control, the operation should cease immediately.**

Product	Quantity	Tank Capacity	Free Space	Max. rate
			<b>Ship's Officer</b>	<b>Bunker Barge</b>
1	Barge has obtained permission to go alongside			<b>Remarks</b>
2	Barge Fenders have been checked, are in good order and no possibility of metal to metal contact			
3	Adequate electrical insulation between both vessels			
4	The vessels are both securely moored			
5	There is a safe means of access between the vessels			
6	Effective communications have been set up and backup communications are available.			
7	Have emergency shutdown procedures been agreed? And an effective watch on board both vessels			
8	Are all ship scuppers closed/plugged?			
9	Are unused connections blanked and valves closed?			
10	Are naked light requirements observed?			
11	Are 'no smoking' and 'bunkering in progress' signs available and displayed?			
12	Is fire fighting equipment deployed on board both vessels and in a state of readiness			
13	Is a sufficient 'save-all / drip tray' located under the hose connections?			
14	Are all hoses and joints secured and supported correctly?			
15	Have all persons involved sighted and understood an agreed contingency plan?			
16	Overboard valves connected to the cargo system, engine bilges and bunker lines closed and sealed			
17	All hatch lids are closed			

18	Bunker tanks contents will be monitored at regular intervals			
19	Absorbents available for immediate use			
20	Main aerials are earthed and radars are switched off			
21	Smoking restrictions in place on both vessels			
22	Whilst bunkering all doors to accommodation to remain closed and secured			
23	Material Safety Data sheets are available when requested			
24	Hazards have been considered and appropriate Risk assessments have been carried out taking into account prevailing weather conditions etc.			

Declaration: We have checked, jointly where appropriate, the items on this checklist and have satisfied ourselves that the entries we have made are to the best of our knowledge. Arrangements have been made to carry out repetitive checks as necessary.

<b>Ship's Officer</b>		<b>Road Vehicle</b>	
Name:		Name:	
Signature:		Signature:	
Date:	Time:	Date:	Time:

## 7. Transit Storage Policy for RO-RO and LO-LO

### RO-RO Hazardous/ Dangerous Goods Transit Storage Policy

Import /export dangerous goods which are received within Warrenpoint Harbour/ Seatruck Area

- Placards are checked to ensure that they comply with the requirements of the IMDG Code and units are then taken to the designated DG storage area demarcated as per site map below.
- **Good should not be stored for longer than 48 hours prior to shipment**
- Notification of DG's stored within the Harbour Area should be sent to Warrenpoint Harbour Authority as per Section 3 (iii) of Warrenpoint Port Dangerous Good Policy 2018.
- Dangerous Goods loads are stowed in designated areas in the Seatruck Terminal as agreed with the Port Authorities. Hazardous storage bays will be identified / marked with a red triangle (as below).



### Exports

- Booking: The Freight office requests when accepting Dangerous Goods bookings both UN numbers and classes so we can determine if we can accept the DG load for shipment or if there are any segregation issues. All Dangerous Goods loads (export/import) are accompanied by DG notes, supplying UN numbers & classes prior to arrival at port.
- Arrival: All units are required to have completed DGN's and IMDG placarding displayed on the vehicle prior to entering the port. They are then directed to the designated area to await shipment.
- Awaiting shipment: Dangerous Goods notes for each load on the terminal are held in the Freight Office and are recorded on our in-house booking system. This provides accurate up to date information on hazardous cargo currently on site. Before arrival the ship is advised of any hazardous cargo booked for the sailing.
- Loading: Prior to loading Dangerous Goods the vessels first officer is advised by the shift Terminal Controller of hazardous cargoes so that we can establish the required stowage location on board the vessel. Stevedores are also made aware of any hazardous cargo on their loading lists.
- Sailing: When each vessel sails a Hazardous manifest is sent to the distant port which contains DG notes, UN numbers, classes and stowage location on board vessel. The original completed DGN's travel in the ships document bag. All dangerous shipments are highlighted on manifests and can be accessed quickly.

### Imports

- Manifests are received from the distant port and hazardous cargoes noted.
- **Discharge / storage:** On discharge the hazardous cargoes are stored in the designated area with all relevant DGN's held in the freight office for reference in case of emergency. These documents are given to the driver on collection.

## LO-LO Hazardous/ Dangerous Goods Transit Storage Policy

- Dangerous goods are not permitted into the Harbour for Shipment prior to the export vessels' arrival.
- Dangerous goods which are imported are not permitted to be stored within the Harbour and onward transport is required to remove dangerous goods immediately on discharge from the vessel.
- In case of an emergency where temporary storage is required for Dangerous Goods within Warrenpoint Harbour this can **Only been done with the prior permission of the Harbour Master and taken to the agreed position in the designated DG storage area demarcated as per site map below.**
- Notification of DG's stored should be sent to Warrenpoint Harbour Authority as per Section 3 (iii) of Warrenpoint Port Dangerous Good Policy 2018.

**Explosive Goods:** The following tables summarises the Maximum Quantity and storage positions for Explosive goods in transit through Warrenpoint Harbour.

### Berth 1

COLUMN 1				COLUMN 2	COLUMN 3	COLUMN 4
Maximum aggregate quantity (in kilograms) of explosives of each Division of Class 1 allowed to be present if distance limitations are met.				Distance from berth within which buildings may only be occupied by persons essential to the handling operation.	Limiting distance to a passenger vessel at berth or anchorage.	Limiting distance to persons in the open or other explosives.
1.1	1.2	1.3	1.5	metres	metres	metres
50	50	50	50	16	16	5
100	100	130	100	25	25	8
150	150	300	150	32	32	11
200	200	500	200	39	39	13
300	300	1200	300	51	51	17
400	400	2100	400	62	62	21
500	500	3000	500	72	72	24
600	600	5000	600	81	81	27
700	700	6000	700	89	89	30
800	800	8000	800	97	97	32
900	900	10000	900	105	105	35
1000	1000	13000	1000	113	113	38

## Berth 2

COLUMN 1				COLUMN 2	COLUMN 3	COLUMN 4
Maximum aggregate quantity (in kilograms) of explosives of each Division of Class 1 allowed to be present if distance limitations are met.				Distance from berth within which buildings may only be occupied by persons essential to the handling operation.	Limiting distance to a passenger vessel at berth or anchorage.	Limiting distance to persons in the open or other explosives.
1.1	1.2	1.3	1.5	metres	metres	metres
50	50	50	50	16	16	5
100	100	130	100	25	25	8
150	150	300	150	32	32	11
200	200	500	200	39	39	13
300	300	1200	300	51	51	17
400	400	2100	400	62	62	21
500	500	3000	500	72	72	24
600	600	5000	600	81	81	27
700	700	6000	700	89	89	30
800	800	8000	800	97	97	32
1000	1000	13000	1000	105	105	35
1200	1200	18000	1200	126	126	42
1400	1400	24000	1400	139	139	46
1600	1600	30000	1600	151	151	50
1700	1700	34000	1700	156	156	52

## Berth 3

COLUMN 1				COLUMN 2	COLUMN 3	COLUMN 4
Maximum aggregate quantity (in kilograms) of explosives of each Division of Class 1 allowed to be present if distance limitations are met.				Distance from berth within which buildings may only be occupied by persons essential to the handling operation.	Limiting distance to a passenger vessel at berth or anchorage.	Limiting distance to persons in the open or other explosives.
1.1	1.2	1.3	1.5	metres	metres	metres
50	50	50	50	16	16	5
100	100	130	100	25	25	8
150	150	300	150	32	32	11
200	200	500	200	39	39	13
300	300	1200	300	51	51	17
400	400	2100	400	62	62	21
500	500	3000	500	72	72	24
600	600	5000	600	81	81	27
700	700	6000	700	89	89	30
800	800	8000	800	97	97	32
1000	1000	13000	1000	105	105	35
1200	1200	18000	1200	126	126	42
1400	1400	24000	1400	139	139	46
1600	1600	30000	1600	151	151	50
1750	1750	36000	1750	159	159	53

### Berth 4

COLUMN 1				COLUMN 2	COLUMN 3	COLUMN 4
Maximum aggregate quantity (in kilograms) of explosives of each Division of Class 1 allowed to be present if distance limitations are met.				Distance from berth within which buildings may only be occupied by persons essential to the handling operation.	Limiting distance to a passenger vessel at berth or anchorage.	Limiting distance to persons in the open or other explosives.
1.1	1.2	1.3	1.5	metres	metres	metres
50	50	50	50	16	16	5
100	100	130	100	25	25	8
150	150	300	150	32	32	11
200	200	500	200	39	39	13
300	300	1200	300	51	51	17
400	400	2100	400	62	62	21
500	500	3000	500	72	72	24
600	600	5000	600	81	81	27
700	700	6000	700	89	89	30
800	800	8000	800	97	97	32
1000	1000	13000	1000	105	105	35
1200	1200	18000	1200	126	126	42
1400	1400	24000	1400	139	139	46
1600	1600	30000	1600	151	151	50
1800	1800	37000	1800	161	161	54
1900	1900	41000	1900	167	167	56

### Berth 5

COLUMN 1				COLUMN 2	COLUMN 3	COLUMN 4
Maximum aggregate quantity (in kilograms) of explosives of each Division of Class 1 allowed to be present if distance limitations are met.				Distance from berth within which buildings may only be occupied by persons essential to the handling operation.	Limiting distance to a passenger vessel at berth or anchorage.	Limiting distance to persons in the open or other explosives.
1.1	1.2	1.3	1.5	metres	metres	metres
50	50	50	50	16	16	5
100	100	130	100	25	25	8
150	150	300	150	32	32	11
200	200	500	200	39	39	13
300	300	1200	300	51	51	17
400	400	2100	400	62	62	21
500	500	3000	500	72	72	24
600	600	5000	600	81	81	27
700	700	6000	700	89	89	30
800	800	8000	800	97	97	32
1000	1000	13000	1000	105	105	35
1200	1200	18000	1200	126	126	42
1500	1500	27000	1500	145	145	48
2000	2000	45000	2000	172	172	57
2500	2500	65000	2500	194	194	65
2800	2800	78000	2800	206	206	69



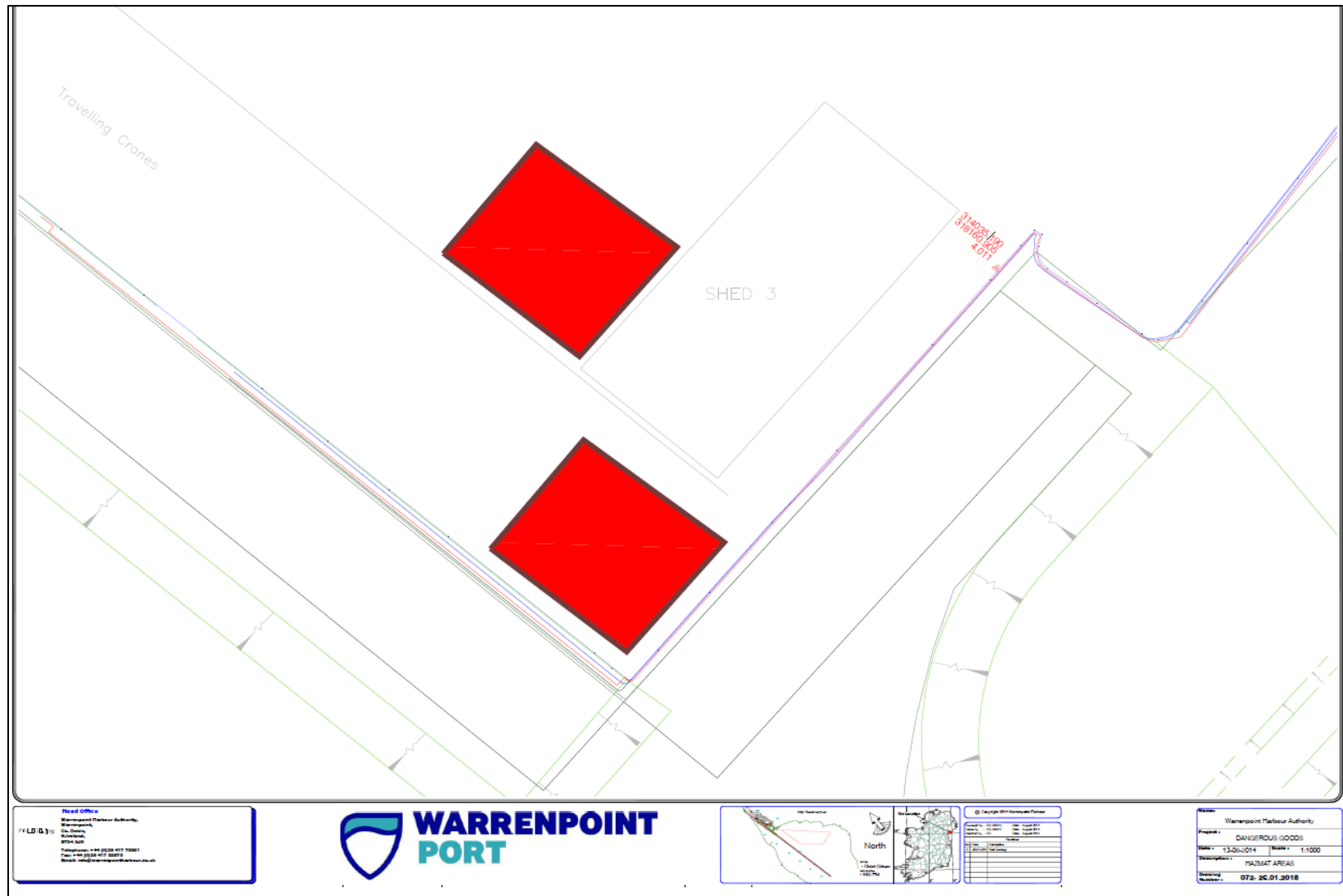
## Berth 6

COLUMN 1				COLUMN 2	COLUMN 3	COLUMN 4
Maximum aggregate quantity (in kilograms) of explosives of each Division of Class 1 allowed to be present if distance limitations are met.				Distance from berth within which buildings may only be occupied by persons essential to the handling operation.	Limiting distance to a passenger vessel at berth or anchorage.	Limiting distance to persons in the open or other explosives.
1.1	1.2	1.3	1.5	metres	metres	metres
50	50	50	50	16	16	5
100	100	130	100	25	25	8
150	150	300	150	32	32	11
200	200	500	200	39	39	13
300	300	1200	300	51	51	17
400	400	2100	400	62	62	21
500	500	3000	500	72	72	24
600	600	5000	600	81	81	27
700	700	6000	700	89	89	30
800	800	8000	800	97	97	32

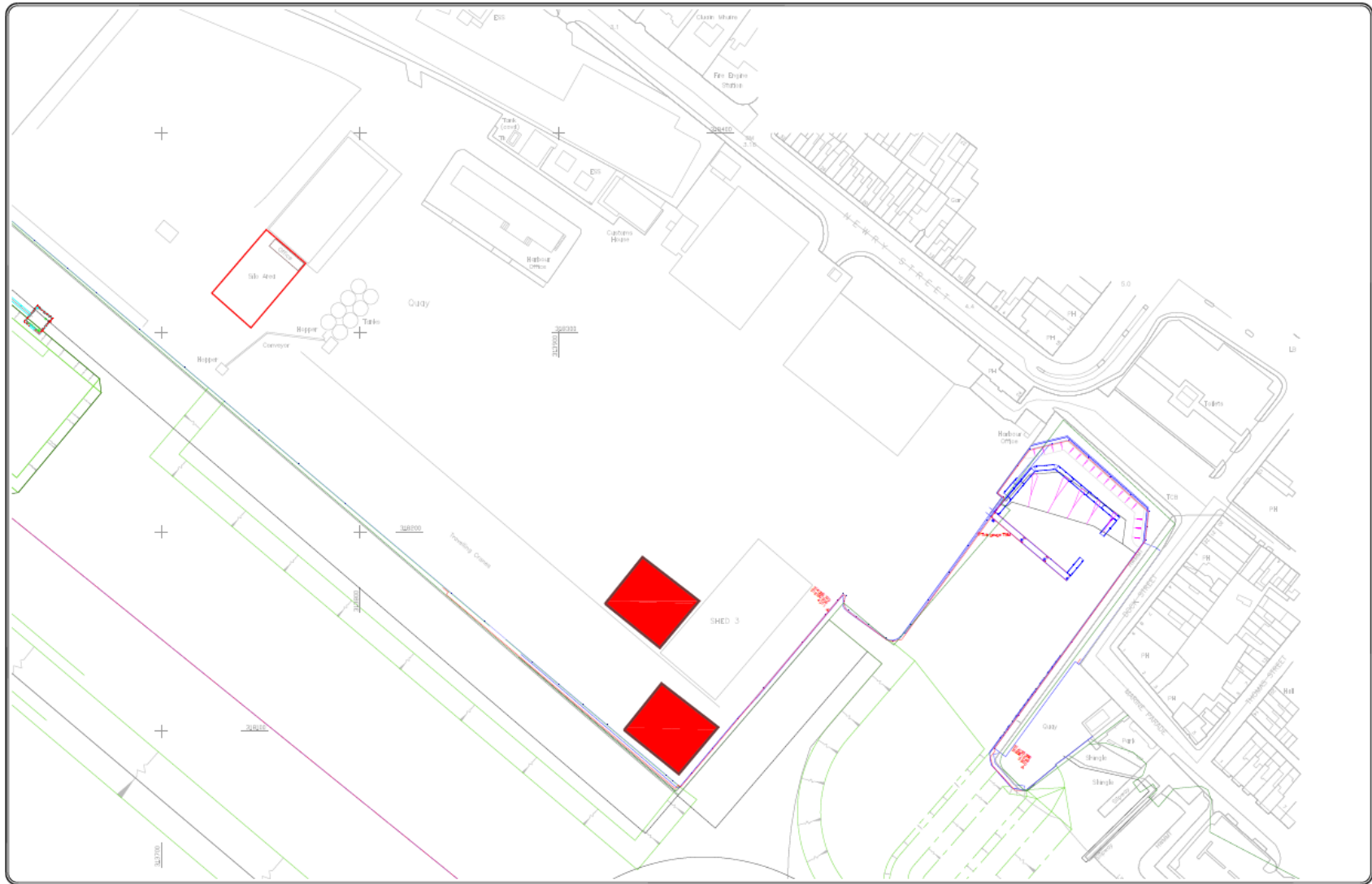
## Berth 7 / Ro-Ro Berth

COLUMN 1				COLUMN 2	COLUMN 3	COLUMN 4
Maximum aggregate quantity (in kilograms) of explosives of each Division of Class 1 allowed to be present if distance limitations are met.				Distance from berth within which buildings may only be occupied by persons essential to the handling operation.	Limiting distance to a passenger vessel at berth or anchorage.	Limiting distance to persons in the open or other explosives.
1.1	1.2	1.3	1.5	metres	metres	metres
50	50	50	50	16	16	5
100	100	130	100	25	25	8
150	150	300	150	32	32	11
200	200	500	200	39	39	13
300	300	1200	300	51	51	17
400	400	2100	400	62	62	21
500	500	3000	500	72	72	24
600	600	5000	600	81	81	27
700	700	6000	700	89	89	30
800	800	8000	800	97	97	32
900	900	10000	900	105	105	35
1000	1000	13000	1000	113	113	38
1100	1200	18000	1200	126	126	42

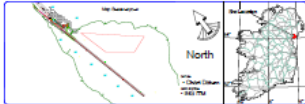
# Site Map: Designated DG Storage Areas for Transit Storage of Dangerous Good / RO-RO & LO-LO



# Site Map: Designated DG Storage Areas for Transit Storage of Dangerous Good / RO-RO & LO-LO



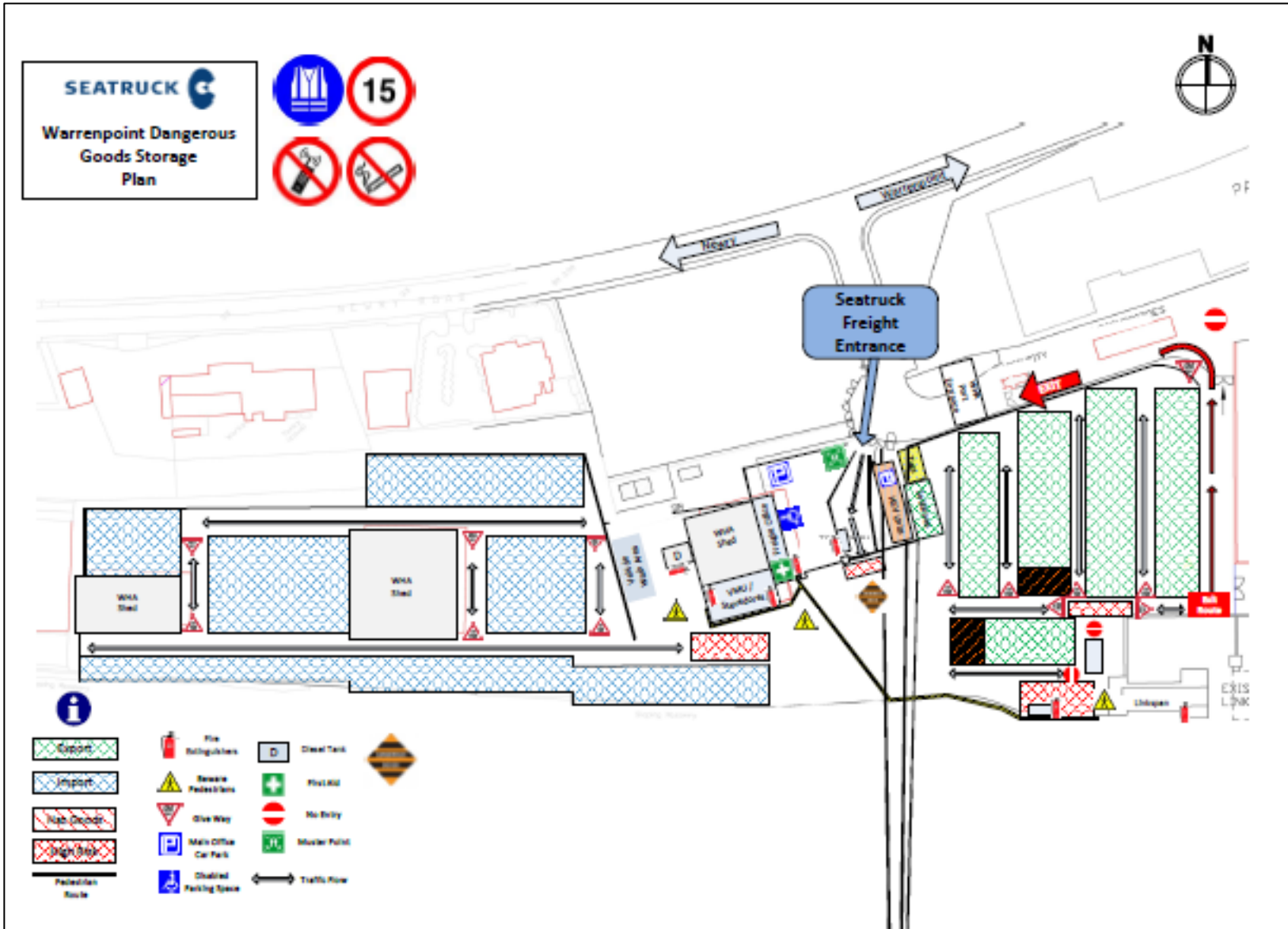
**Head Office**  
 Warrenpoint Harbour Authority,  
 Warrenpoint,  
 Co. Down,  
 BT24 5AB  
 Telephone: +44 (0)28 417 1001  
 Fax: +44 (0)28 417 0010  
 Email: info@warrenpointharbour.co.uk



© Copyright 2018 Warrenpoint Harbour Authority	
Project Name	Designated DG Storage Areas
Project No.	072-20.01.2018
Scale	1:1000
Date	13-06-2014
Author	
Checker	
Approver	

Warrenpoint Harbour Authority	
Project Name	Designated DG Storage Areas
Project No.	072-20.01.2018
Scale	1:1000
Date	13-06-2014
Author	
Checker	
Approver	

# Site Map: Designated DG Storage Areas for Transit Storage of Dangerous Good / RO-RO & LO-LO



## 8. Security Provisions

Security provisions in place for Dangerous Goods movements are covered by the Harbour's International Ship and Port Security obligations and are addressed in detail in the Port Facility Security Plan. Consigners should ensure that the above provisions should form part of their overall security planning for multi-modal movements of High Consequence Dangerous Goods (see IMDG 1.4)

Security arrangements for High Consequence Dangerous Goods (excluding Class 1) that are transported on vessels less than 500 GT (which is outside of the ISPS scope) will be security assessed by the Vessel Master and the Port Facility Security Officer (PFSO).

Security arrangements for Class 1 goods that are transported on vessels less than 500 GT (which is outside of the ISPS scope) will be security assessed by the vessel master, PFSO and the Explosives Security Officer in the normal way

## 9. Training and Awareness Requirements

All persons involved in the transport and handling of Dangerous Cargoes should undertake General Awareness / Familiarisation training in Dangerous goods.

Such training should include the following topics

- description of the types and classes of Dangerous Goods
- marking, labelling and placarding
- packing; segregation and compatibility requirements
- transport documentation
- emergency response arrangements
- the use of Personal Protection Equipment if appropriate.

Function specific training should be provided of all staff who directly handle Dangerous Goods within Harbour Limits. These include operations such as

- Loading / Unloading Dangerous Goods into /from ships
- Inerting, washing or sampling from cargo tanks
- Preparing DG loading and stowage plans

Records of all training undertaken should be kept for inspection during audits.

## 10. Vessel Traffic Movement Restrictions

In general, there are no significant Vessel Movement restrictions for vessels carrying dangerous goods.

However, the following restrictions are in place as outlined in the Harbour's LPS procedures

- A "slow pass" must be undertaken by ALL vessels when passing a Ship to Ship bunkering operation.

Pilotage arrangements for vessels carrying Dangerous Goods are Pilot to remain onboard during bunkering operations and Pilot is required to be onboard for any movement of vessels carrying dangerous goods in-bound, out-bound and/ or for movements between berths within the port.

## 11. Emergency Requirements

Emergency arrangements for incidents are detailed in the Harbour's Major Incident / Emergency Plan

**Port Control and must also be informed the police of any incident involving Dangerous Goods.**

The Northern Ireland Fire and Rescue Service will be recognised as the lead responder for Dangerous Goods / Hazmat incidents

Harbour responses are detailed in Emergency Action plans which are held senior managers

Current storage lists will be and the information should be made readily available in the case of an Emergency

## 12. Audit and Review Process

Warrenpoint Harbour Authority will undertake an inspection and audit process to measure compliance with this Policy and the associated legislation.

Audits will take place in conjunction with agents and vessels. An annual audit programme will be established to cover the areas outlined in the table below.

Location	Audit topic
Vessels	Certification checks Stowage planning and checking Training and Awareness Bunkering arrangements Emergency Arrangements
Quays	Transit storage arrangements Condition checks of packaging and tanks Placarding and Labelling Training and Awareness Emergency Arrangements Class 1 and 7 movements Documentation check
Desktop	Advanced Notification DG Document completion Management of Dangerous Goods Records

Any non-conformances found as a result of these inspections will be recorded as part of WHA's Non-conformance procedure under the Safety Management System.

A review of Audit findings will be completed on an annual basis by the Harbour Master.